

**Village of Somers
7511 12th Street
Somers, WI 53171**

**Village Board Meeting
Agenda
Tuesday, November 12th, 2024
5:30 p.m.**

Village Board Meeting:	
Item #	
1	Call to order
2	Pledge of Allegiance
3	Consent and Approval of Minutes of Regular meetings on October 22 nd , 2024, Vouchers dated October 24 th , 2024 and October 31 st , 2024, September 2024 Investment Report
4	Correspondence:
5	Citizens Comments
6	President and Trustee Comments
7	Action on Resolution 2024-011, A Resolution of the Village Board of Trustees of the Village of Somers to Recognize the Efforts of the Village and Town of Somers Election Poll Workers
8	<p>Action on Plan Commission Recommendation (Spaay & Frederick)</p> <p>a. Request by John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)</p> <p>b. Request by John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create</p>

	two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)
9	<p>Action on Plan Commission Recommendation (Shoreland):</p> <p>a. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. (For information use only, the property is located at 9026 12th Street.)</p> <p>b. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. (For information use only, the property is located at 9026 12th Street.)</p>
10	Action on Operator’s Licenses: Sandra Parise, Denise Maegaard, Dormilee Craig, Alexis Tennyson, and Megan Hornbaker
11	Adjourn

I hereby certify that as the designee of the chief elected official of the Village of Somers, I posted this notice of the November 12th, 2024 Village Board Meeting & Agenda in 1 public place & on the Village website.

Dated this 8th day of November 2024

Wendy Burnette, Clerk-Treasurer

Requests from person with disabilities who need assistance to participate in this meeting should be made to the Clerk’s Office at 262-859-2822 with as much notice as possible. **Notice is hereby given that members of the Village Board may participate telephonically. Notice is hereby given that members of the Town Board may be in attendance for the sole purpose of gathering information. A quorum may be present. However, no Board action will be taken.**

**Village of Somers
Proceedings from the Regular Board Meeting October 22, 2024**

President Stoner called the meeting to order at 5:43 p.m.

President Stoner led the Pledge of Allegiance.

Present: President George Stoner, Trustees Karl Ostby, Ben Harbach, Jackie Nelson, Scott Fredrick, and Joe Smith. Trustee Jack Aupperle was excused. Also present: Administrator Jason Peters, Clerk/Treasurer Wendy Burnette, Assistant to the Administrator Kevin Poirier, and Attorney Jeff Davison.

Consent and Approval of Minutes of Regular meetings on October 8th, 2024, Vouchers dated October 10th, 2024, and October 17th, 2024

Trustee Smith moved to approve the Minutes of Regular meetings on October 8th, 2024, Vouchers dated October 10th, 2024, and October 17th, 2024

Seconded by Trustee Harbach.

Motion carried. 6-0 vote.

Correspondence:

None

Citizens Comments

None

President and Trustee Comments

Trustee Nelsen thanked Administrator Peters for highlighting each staff member in budget proposal.

President Stoner thanked the Fire Department for such a great job with the incidents on Sheridan Road and Interstate 94. He also mentioned that everything was moving along with Golden Oil. Once the Development Agreement is signed, they are ready to move ahead.

2025 General Fund Budget Presentation by Administrator Peters

Public Hearing on Final Resolution 2024-010, A Resolution to Levy Special Charges to the 2024 tax roll

President Stoner opened the public hearing at 6:07 p.m.

Kathleen Sadowski, 8903 20th Ave, Kenosha, WI 43143 mentioned she did not know of this charge as the place that received the citation is a rental property.

President Stoner closed the public hearing at 6:09 p.m.

Motion to approve Final Resolution 2024-010, A Resolution to Levy Special Charges to the 2024 tax roll

Trustee Ostby moved to approve Final Resolution 2024-010, A Resolution to Levy Special Charges to the 2024 tax roll.

Seconded by Trustee Smith.

Motion carried. 6-0 vote.

Action on proposed ordinance 2024-009, an ordinance to repeal Section 21.15(B) of the code of ordinances of the Village of Somers regarding street numbers

Trustee Nelson moved to approve proposed ordinance 2024-009, an ordinance to repeal Section 21.15(B) of the code of ordinances of the Village of Somers regarding street numbers.

Seconded by Trustee Fredrick.

Motion carried. 6-0 vote.

Discussion and possible action on request for final payment from Bukacek in the amount of \$21,318.96 for the auditorium remodeling project

Trustee Ostby moved to approve final payment from Bukacek in the amount of \$21,318.96 for the auditorium remodeling project

Seconded by Trustee Nelson.

Trustee Nelson asked if this payment was held back as a contingency for last minute items that needed to be done.

Administrator Peters mentioned we just received the payment request.

Motion carried. 6-0 vote.

Action on Transfer of Agent for Kwik Trip #597 - Located at 11350 28th Street

Trustee Ostby moved to approve the Transfer of Agent for Kwik Trip #597 - Located at 11350 28th Street

Seconded by Trustee Fredrick

Trustee Smith asked if all her paperwork checked out. Clerk Burnette mentioned her background check was good, and she formally had agent experience in Burlington, Wi.

President Stoner asked what the reasoning was for transferring Agent. Clerk Burnette mentioned it was probably due to the resignation of the former agent.

Motioned carried. 6-0 vote

Action on Operator's Licenses:

None

Adjourn

Trustee Harbach moved to adjourn at 6:12 p.m.

Seconded by Trustee Nelson.

Motion carried. 6-0 vote.

Drafted this 4th day of November by Wendy Burnette Clerk/Treasurer
These minutes are not official until approved by the Village Board.

Check Date	Bank	Check	Vendor	Vendor Name	Amount
Bank POOL POOLED CASH					
10/24/2024	POOL	225 (E)	HINCKLEY	HINCKLEY SPRINGS	94.91
10/24/2024	POOL	226 (E)	KWIKTRIP	KWIK TRIP INC	5,354.22
10/24/2024	POOL	227 (E)	SAMSMC	SAM'S CLUB MC/SYNCB	1,274.55
10/24/2024	POOL	228 (E)	WID002	WI DEPT OF JUSTICE	35.00
10/24/2024	POOL	229 (E)	YUTKA	YUTKA FENCE	2,042.23
10/24/2024	POOL	63946	HOP001	ANDREA & ORENDORFF LLP	7,204.40
10/24/2024	POOL	63947	AJG001	ARTHUR J GALLAGHER RISK MGMT	3,190.00
10/24/2024	POOL	63948	BAXTER	BAXTER & WOODMAN	6,465.00
10/24/2024	POOL	63949	BUKACEK	BUKACEK CONSTRUCTION GROUP INC	21,318.96
10/24/2024	POOL	63950	CFS001	CONSERV FS, INC	356.05
10/24/2024	POOL	63951	CORE	CORE & MAIN LP	61,980.00
10/24/2024	POOL	63952	EMC	EMC-INSURANCE-COMPANIES	1,679.20
10/24/2024	POOL	63953	FAS001	FASTENAL COMPANY	34.68
10/24/2024	POOL	63954	UHS001	FROEDTERT SOUTH INC	732.25
10/24/2024	POOL	63955	HEARTLAND	HEARTLAND BUSINESS SYSTEMS LLC	1,200.70
10/24/2024	POOL	63956	HLK001	HOERNEL LOCK & KEY INC	109.50
10/24/2024	POOL	63957	HOTSY	HOTSY CLEANING SYSTEM	542.80
10/24/2024	POOL	63958	JCLICHT	JC LICHT LLC	55.94
10/24/2024	POOL	63959	KCSHERIF	KENOSHA CO SHERIFF DEPT	73,305.68
10/24/2024	POOL	63960	KTC001	KENOSHA TROPHY COMPANY	24.00
10/24/2024	POOL	63961	KWU001	KENOSHA WATER UTILITY	1,163.51
10/24/2024	POOL	63962	MCDEVITTS	MCDEVITTS TOWING & REPAIR	711.55
10/24/2024	POOL	63963	MEN001	MENARDS - RACINE	196.25
10/24/2024	POOL	63964	NAP001	NAPA AUTO PARTS	180.26
10/24/2024	POOL	63965	PARAGON	PARAGON DEVELOPMENT SYSTEMS	4,022.00
10/24/2024	POOL	63966	PAY001	PAYNE & DOLAN INC	208.73
10/24/2024	POOL	63967	QUADIENTLE	QUADIENT LEASING USA INC	214.68
10/24/2024	POOL	63968	R&RINS	R & R INSURANCE SERVICES INC	41,167.50
10/24/2024	POOL	63969	RWU001	RACINE WATER & WASTEWATER UTLY	361.20
10/24/2024	POOL	63970	HAMM	ROBERT HAMM	237.36
10/24/2024	POOL	63971	ROLAND	ROLAND MACHINERY COMPANY	1,432.98
10/24/2024	POOL	63972	T0000239	RYAN SMITH	23.26
10/24/2024	POOL	63973	SCHIND	SCHINDLER ELEVATOR CORPORATION	1,905.59
10/24/2024	POOL	63974	STAPLEAD	STAPLES	646.62
10/24/2024	POOL	63975	EALY	TANYA EALY	149.80
10/24/2024	POOL	63976	TKITZ	TIMOTHY KITZMAN	110.00
10/24/2024	POOL	63977	TITAN	TITAN PUBLIC SAFETY SOLUTIONS	300.00
10/24/2024	POOL	63978	TRCO001	TRUCK COUNTRY OF WISC	342.14
10/24/2024	POOL	63979	ULINE	ULINE	520.50
10/24/2024	POOL	63980	VPLPR	VILLAGE OF PLEASANT PRAIRIE	4,341.60
10/24/2024	POOL	63981	WEE001	WE ENERGIES	135.38
10/24/2024	POOL	63982	WEE001	WE ENERGIES	6,241.35
10/24/2024	POOL	63983	WES001	WESTERN CULVERT & SUPPLY INC	69.60
10/24/2024	POOL	63984	WDR001	WI DEPT OF REVENUE	284.13

POOL TOTALS:

Total of 44 Checks:	251,966.06
Less 0 Void Checks:	0.00
Total of 44 Disbursements:	251,966.06

Check Date	Bank	Check	Vendor	Vendor Name	Amount
Bank POOL POOLED CASH					
11/07/2024	POOL	64016	AT&T001	AT&T	281.94
11/07/2024	POOL	64017	AT&T001	AT&T	115.44
11/07/2024	POOL	64018	AT&TMOB	AT&T MOBILITY	393.12
11/07/2024	POOL	64019	BTM001	BOUND TREE MEDICAL LLC	1,784.44
11/07/2024	POOL	64020	DAV001	DAVISON LAW OFFICE, LTD	4,728.34
11/07/2024	POOL	64021	EHLERS	EHLERS	11,500.00
11/07/2024	POOL	64022	TRI-VISA	ELAN FINANCIAL SERVICES	4,879.78
11/07/2024	POOL	64023	FAS001	FASTENAL COMPANY	32.94
11/07/2024	POOL	64024	GALLS	GALLS LLC	234.17
11/07/2024	POOL	64025	KEN002	KENOSHA COUNTY TREASURER	1,496.80
11/07/2024	POOL	64026	KWU001	KENOSHA WATER UTILITY	58,161.85
11/07/2024	POOL	64027	KWU002	KENOSHA WATER-UTILITY	58,242.27
11/07/2024	POOL	64028	LAWEVANS	LAW OFFICE OF TIMOTHY R EVANS	1,706.25
11/07/2024	POOL	64029	MATRUCK	M A TRUCK PARTS INC	456.72
11/07/2024	POOL	64030	MEN001	MENARDS - RACINE	144.64
11/07/2024	POOL	64031	MATC	MILWAUKEE AREA TECH COLLEGE	6,742.25
11/07/2024	POOL	64032	NAP001	NAPA AUTO PARTS	55.89
11/07/2024	POOL	64033	PARAGON	PARAGON DEVELOPMENT SYSTEMS	262.95
11/07/2024	POOL	64034	RICOHUSA	RICOH USA, INC	505.30
11/07/2024	POOL	64035	SKYDIO	SKYDIO INC	320.72
11/07/2024	POOL	64036	PEREZT	TERESA PEREZ	400.00
11/07/2024	POOL	64037	ULINE	ULINE	1,403.25
11/07/2024	POOL	64038	WEE001	WE ENERGIES	101.59
11/07/2024	POOL	64039	WEE002	WE ENERGIES	85.79
11/07/2024	POOL	64040	WES001	WESTERN CULVERT & SUPPLY INC	4,748.19
11/07/2024	POOL	64041	WICOURT	WI COURT FINES & SURCHARGES	4,674.10

POOL TOTALS:

Total of 26 Checks:	163,458.73
Less 0 Void Checks:	0.00
Total of 26 Disbursements:	163,458.73

2024 VILLAGE OF SOMERS MONTHLY INVESTMENT INCOME

SEPTEMBER	STATE BANK	SCHWAB	PERSHING-BORROWING
INTEREST	\$ 16,893.27	\$ 3,446.01	\$ 4,030.47
MARKET CHGE	0	\$ 15,430.43	\$ (141.58)
TOTAL	\$ 16,893.27	\$ 18,876.44	\$ 3,888.89

ENDING BALANCES

SEPTEMBER	STATE BANK	SCHWAB	PERSHING-BORROWING
	\$ 3,781,340.80	\$ 4,655,163.64	\$ 1,106,656.15

Extended T-Bill MD6
for \$526,000 to Oct 2025

ENDING BALANCES

GF INTEREST PORTION	STATE BANK	SCHWAB	PERSHING-BORROWING
Year to date	\$ 12,140.64	\$ 41,004.23	\$ -
GF INTEREST PERCENTAGE	38.70%	38.70%	

FEES

LGIP-V	LGIP-T	TRI-CITY	TOTALS
\$ 36,200.69	\$ 2,242.40	\$ 37,927.41	\$ 100,740.25
			\$ 15,288.85
\$ 36,200.69	\$ 2,242.40	\$ 37,927.41	\$ 116,029.10

SEPTEMBER

Accrued Interest

LGIP-V	LGIP-T	TRI-CITY	TOTALS
\$ 8,476,886.52	\$ 525,087.96	\$ 8,352,053.82	\$ 26,897,188.89

SEPTEMBER

LGIP-V	LGIP-T	TRI-CITY	TOTALS
Included in Tri-City Interest	\$ 19,493.28 100%	\$ 398,870.89 50%	\$ 471,509.04

PERSHING- BORROWING	STATE BANK
\$ (247.27)	(30.35) This is being waived
\$ (247.27)	(30.35)

PERSHING- BORROWING
\$ 78.85
\$ 78.85

**VILLAGE OF SOMERS
VILLAGE BOARD
VILLAGE BOARD ITEM MEMORANDUM**

MEETING DATE: November 12th, 2024

TO: Village President Stoner and Village Trustees

FROM: Jason J. Peters, Administrator

AGENDA ITEM: #7 Action on Resolution 2024-011, A Resolution of the Village Board of Trustees to Recognize the Efforts of the Village and Town of Somers Election Poll Workers

BACKGROUND:

The Village and Town of Somers relies on over 30 poll workers to ensure our citizens can exercise their right to vote. During the 2024 election cycle our election workers prepared and managed polls in February, April, August, and November. As you are aware our November 5th Election was a Presidential Election that brought a historic number of citizens to our polls. Administrator Peters has prepared the attached Resolution to show our thanks to all our election poll workers.

PRIOR ACTION TAKEN:

Administrator Peters informed that Board at our November 6th Work Session that the attached Resolution was being prepared and would be brought to our November 12th Board Meeting.

SUGGESTED ACTION/ACTION REQUESTED/COMMENTS:

Administrator Peters would recommend the approve of Resolution 2024-011. In the event that the Town Board agrees with the suggested action, a suggested motion to approve would be as follows:

“Motion to approve Resolution 2024-011, A Resolution of the Village Board of Trustees to Recognize the Efforts of the Village and Town of Somers Election Poll Workers”

ATTACHMENTS:

Proposed Resolution 2024-011

VILLAGE OF SOMERS

RESOLUTION 2024-011

A RESOLUTION OF THE VILLAGE BOARD OF TRUSTEES TO RECOGNIZE THE EFFORTS OF THE VILLAGE AND TOWN OF SOMERS ELECTION POLL WORKERS

WHEREAS, the right to vote is essential to our Democracy; and

WHEREAS, the Village and Town of Somers relies on Election Poll Works to ensure that our citizens can carry out their right to vote; and

WHEREAS, the Village and Town of Somers has in excess of thirty individuals that offer their time and talents to work our elections; and

WHEREAS, Village and Town of Somers Election Poll Workers spend countless hours receiving training on how to administer elections, registering citizens to vote, handling absentee ballots, counting tallies, reporting results and managing polling locations; and

WHEREAS, many of these Village and Town of Somers Election Poll Workers have worked numerous past elections and have passed their institutional knowledge onto those new poll workers who have chosen to be involved; and

WHEREAS, Village and Town of Somers Election Poll Workers work hand in hand with Village and Town Staff to prepare for election day; and

WHEREAS, in 2024 the Village and Town of Somers Election Poll Workers election cycle included dates in February, April, August and November; and

WHEREAS, the November 5th, 2024 election saw historic numbers of citizens in the Village and Town who exercised their right to vote for County, State and Federal Offices; and

NOW THEREFORE, BE IT RESOLVED, that the Village Board of Trustees recognizes the hard work and effort of the Village and Town of Somers Election Poll Workers; and

FURTHER RESOLVED, the Village Board of Trustees wishes to congratulate the Village and Town of Somers Election Poll Workers on another successful election cycle; and

FINALLY RESOLVED, that the Village Board of Trustees wishes to formally document their thanks and gratitude to each and every Village and Town of Somers Election Poll Workers.

Dated at Somers, Wisconsin, this 12th day of November, 2024.

VILLAGE OF SOMERS

By: _____

George Stoner, President

Attest: _____

Wendy Burnette, Clerk/Treasurer

**VILLAGE OF SOMERS
VILLAGE BOARD
MEETING ITEM MEMORANDUM**

MEETING DATE: November 12th, 2024

TO: Village President Stoner and Village Trustees

PREPARED BY: Jason J. Peters, Administrator

AGENDA ITEM: #8 Action on Plan Commission Recommendation (Spaay & Frederick)

- a. Request by John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)
- b. Request by John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)

BACKGROUND:

In early May, Staff met with Steven Frederick regarding the possible split of the lot located at 5025 12th Street. In a previous memo, it was mentioned that this was to build a new house on the lot. This was a miscommunication. President Stoner and Administrator Peters spoke with Mr. Frederick on November 7th. The lot split is being

done by the Spaays so that Mr. Frederick can buy the house that currently sits on the lot. The Spaays will then retain the new vacant lot. They have no plans to sell or build on the newly created lot. The lot split would also require a rezoning of a part of the lot from A-2 to R-2. In late August, applications for the rezone and to create two (2) 4.82 acre lots was submitted.

PRIOR ACTION TAKEN:

The Plan Commission met on October 14th to review the Developer's application for the rezoning and land division. The requisite public hearing for the rezoning was held. There were no comments from the public. The Plan Commission's recommendation was to approve the request for rezoning (6-0) and the request for the Certified Survey Map (6-0).

The Board reviewed this matter at our November 6th Work Session. As stated above, there are no plans to build on the newly create lot. This will be held by the current owner. Mr. Frederick also stated that he has spoken to the DNR and FEMA regarding the drainage area in the front of the property. If something was to be built on this new lot in the future this area would not be blocked off. A culvert would need to be stalled if any driveway or other improvement were to go in this area. Given this fact, Administration does not believe that submission of a site plan would be necessary to simply approve this lot split. This issue can be addressed through the building inspector, if and when, this lot were to be developed.

SUGGESTED ACTION/ACTION REQUESTED/COMMENTS:

Administration recommends both items 8a and 8b be approved with conditions listed below. In the event that the Village Board agrees with the suggested action, a suggested motion to approve would be as follows:

8a

“Motion to approve request by John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers, subject to the following conditions:

- 1. Subject to conditions as listed in Planning Memo dated September 30th, 2024;*
- 2. Subject to final CSM being reviewed and approved by Village Planning; and*
- 3. Execution of a Developer's Agreement between the Village of Somers and John & Ruthann Spaay for the newly created lot.”*

8b

“Motion to approve request by John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers, subject to the following conditions:

- 1. Subject to conditions as listed in Planning Memo dated September 30th, 2024;*
- 2. Subject to final CSM being reviewed and approved by Village Planning; and*
- 3. Execution of a Developer’s Agreement between the Village of Somers and John & Ruthann Spaay for the newly created lot.”*

ATTACHMENTS:

Planning and Zoning Memo to Plan Commission dated September 30th, 2024

Plan Commission Minutes – Highlighted

Rezoning Application

Land Division Application

DOA Letter



KENOSHA COUNTY

Shelly Billingsley, Director
Department of Public Works &
Development Services

Andy M. Buehler, Director
Division of Planning & Development

TO: Village of Somers Plan Commission
FROM: Luke Godshall, Senior Land Use Planner
APPLN DATE: 08-28-24
RPT DATE: 09-30-24
MTG DATE: 10-14-24
RE: Spaay Rezone & CSM

PROJECT/SITE INFO:

1. Petitioner/Agent: Steven Frederick
2. Property Owner: John & Ruthann Spaay
3. Location/Address: 5025 12th St.
4. Tax key Number(s): 82-4-222-142-0070
5. Area: 9.750 acres
6. Existing Zoning: R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist.
7. Proposed Zoning: R-2 Suburban Single-Family Residential Dist.
8. Existing Land Use: Suburban-Density Residential
9. Proposed Land Use: Suburban-Density Residential

PROJECT OVERVIEW:

The Petitioner is proposing to split the subject parcel into two (2) 4.82-acre Lots via Certified Survey Map. The Petitioner has also submitted a corresponding rezoning application to assign R-2 zoning to the entirety of both Lots of the CSM, as the subject parcel is currently split zoned R-2 and A-2.

PLANNER COMMENTS:

Each of the proposed Lots meet the minimum lot size and width requirements for the R-2 zoning district (minimum lot area of 40,000 square feet (0.92 acres) and a minimum lot width of at least 150 feet). Additionally, the existing single-family dwelling shown on Lot 2 of the CSM meets all minimum setback requirements for the R-2 zoning district.

Kenosha County Highways has commented on the proposed CSM and indicates that each of the proposed Lots of the CSM will be allowed its own driveway access, with the driveway on Lot 1 being located as far east as possible so as to be as far away from the intersection of CTH E & 52nd Ave. as possible.

Highways has also indicated that the south half of the CTH E right-of-way shall be a total of 60 feet. The submitted CSM indicates a total of 47 feet of right-of-way dedication for the south half

of CTH and therefore will need to be revised to indicate a total of 60 feet of right-of-way dedicated to the public for the south half of CTH E.

The submitted Certified Survey Map generally complies with the Village of Somers' Chapter 18 Land Division and Platting Control Ordinance. Noted below are several revisions that should be made to the final version of the CSM prior to its recordation in the Kenosha County Register of Deeds office.

STAFF RECOMMENDATION:

Should the Plan Commission choose to recommend approval of this Certified Survey Map, staff would recommend the following conditions:

1. Subject to revising the CSM to address any review comments received from the Wisconsin Department of Administration.
2. Subject to making the following revisions to the CSM:
 - a. Label a total of 60' as being dedicated to the public for the south half of CTH (submitted CSM indicates 47').
 - b. Revise the Lot area calculations as needed to account for a total of 60' of right-of-way dedication on the south half of CTH E.
 - c. Add the name and address of the current property owner.
 - d. Add a note indicating the Lots will be served by public sanitary sewer.
 - e. On Sheet 2 of 2, in the Village of Somers Approval area change 'Jason Peters, Village Administrator' to 'George Stoner, Village President'.
3. Subject to sending an electronic copy of the revised CSM showing the aforementioned changes to the Kenosha County Department of Planning and Development office to receive sign-off prior to the surveyor preparing the recordable CSM document for signatures. The county contact is Luke Godshall who can be contacted at the following e-mail address: luke.godshall@kenoshacounty.org.
4. No land division shall be recorded if any portion or part of the parent parcel has any unpaid taxes or outstanding special assessments without prior approval of the local and Kenosha County Treasurer.
5. Subject to the Certified Survey Map (CSM) being recorded with the Kenosha County Register of Deeds within twelve (12) months of receiving approval from the Somers Village Board.



Village of Somers
Proceeding from the Village Plan
Commission Meeting
Oct. 14, 2024

1. Call to Order

Chairman Stoner called the meeting to order at 5:28 p.m.

Present:

- Chairman George **Stoner**
- Commissioner Troy **Steege**
- Commissioner Gregg **Thompson**
- Commissioner Vinnie **Chambers**
- Commissioner Don **Boxx**
- Commissioner Michael **DeLuca**

Absent:

- Commissioner Jerry **Romanowski**
- Alternate Pat **Juliana**

Staff present in person: Administrator Jason Peters, Assistant to the Administrator Kevin Poirier, Somers Fire Chief Ben Andersen, Deputy Chief Jodin Froeber

Village Trustees: Jackie Nelson

Kenosha County Planners: Luke Godshall

Village Engineer: Brett Biber

2. Pledge of Allegiance

Chairman **Stoner** led everyone in the Pledge of Allegiance.

3. Approve Minutes of the Aug. 12, 2024 Meeting

Commissioner **Boxx** moved to approve to the minutes.

Seconded by Commissioner **Steege**.

Motion carried. 6-0 vote.

4. Correspondence

None

5. Citizen Comments

Tom **Harper** – 8910 12th Street

Mr. **Harper** mentioned that his property is very close to the school and bring up parking concerns. He mentions that cars have parked on the road during events and it is a safety concern, He also mentions noise from indoor and outdoor events and hopes that sound proofing options will be considered.

- 6. Rezone: Public Hearing and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requesting approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

- 7. Land Division (Certified Survey Map): Discussion and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requests approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Thompson** seconds the motion.

Motion carried. 6-0.

Passes unanimously

- 8. Comprehensive Plan Amendment: Public Hearing and Action on Request by:** Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St., Kenosha, WI 53142 (Agent); requests an amendment to the land use plan map for the Village of Somers (Map 80) as adopted in the Multi-

Jurisdictional Comprehensive Plan for Kenosha County: 2035 from High-Density Residential to Medium-Density Residential, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Administrator **Peters** mentions that there have been changes to the project since the concept was presented to the board at their July 8th meeting.

Bear Development Project Manager Dan **Szczap** mentions that the project presented was a residential mix of 33 single-family homes and 64 two-family buildings that received positive feedback when it was presented in July. They therefore started the process to re-zone and amend the comprehensive land use plan accordingly. They are proposing zero lot-line parcels to accommodate the two-family buildings. It would look like a condominium but be on a fee-simple lot. They have done this before in Mount Pleasant.

Szczap suggests three ways to move forward:

1. Move forward with the concept plan and using a PUD to handle deviations
2. Changing the two-family dwellings to condos
3. Changing the zoning code to accommodate zero-lot-line development

Administrator **Peters** mentions that Bear is also looking at the possibility of filling the site and changing it to single-family homes.

Szczap mentions that the soil condition and high ground water on the property has been challenging and that the fill would allow for the structures to be built with basements. They are in discussion to secure fill for the site but that has not been finalized.

Commissioner **Boxx** gets confirmation from **Szczap** that if sufficient fill is secured, the duplexes would be changed to single-family homes on the entire site. If the fill is not secured, the developer would plan to move forward with zero-lot-line dwellings to accommodate the challenges of the site and be a financially viable project. The developer states that condominiums is a complicated form of ownership and is not their preference.

Commissioner **Boxx** mentions that the developer is asking for a reduction of the setbacks from what the zoning allows.

Commissioner **Steege** asks whether, in the case the site is converted to single-family homes, the setbacks would be compliant with current ordinances.

Szczap states that he is not sure as the site plan is likely to change and they may ask for

minor relief.

Commissioner **Steege** asks for clarification about the changes from the plan resented in July.

Szczap states that the building and roads through the site did not change, only the lot lines through the duplexes have changed to create zero-lot-line fee-simple lots.

Commissioner **Boxx** asks details about the maintenance of the property.

Szczap explains that declarations, homeowner association and/or covenants would govern maintenance, common areas, architecture and insurance.

Commissioner **DeLuca** asks again whether the developer would still ask for zero-lot-lots if they secured the fill.

Szczap states that if the fill is secured, they would eliminate the two-family units in favor of single-family lots.

Chairman **Stoner** is concerned about zero-lot-line and issues with neighbors maintaining their properties to different standards.

Szczap states that the management documents would be similar to a condominium.

Chairman **Stoner** mentions concerns about the fill process and that the size of the lots for single family homes would have to change.

Chairman **Thompson** mentions being in favor of the medium density single-family project but is concern about the timing of the fill as it relates to weather.

Commissioner **Boxx** asks whether the management documents from a similar site could be provided to the commission for review.

Szczap states that that document is owned by a third party and would need approval before sharing it.

Commissioner **Steege** makes a motion to approve the request.

Commissioner **Chambers** seconds the motion.

Motion carried. 6-0.

Passes unanimously

9. **Rezoning w/ PUD:**

Public Hearing and Action on Request by: Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St.,

Kenosha, WI 53142 (Agent); requesting approval of a rezoning from R-9 Multiple-Family Residential Dist., C-1 Lowland Resource Conservancy Dist., and A-2 General Agricultural Dist. to R-9 Multiple-Family Residential Dist., R-5 Urban Single-Family Residential Dist., and C-1 Lowland Resource Conservancy Dist., and PUD Planned Unit Development Overlay, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **DeLuca** makes a motion to table the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

10. Conditional Use Permit:

Public Hearing and Action on Request by: Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requesting a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments:

Tom **Terletzky** – 9015 12th Street

Mr. **Terletzky** mentions his support for the school's expansion. He raises concerns about the extension of the water to the property from 100th Avenue. That resulted in the homeowners paying a large amount of the cost to the benefit of a proposed subdivision that would use a lot more of that water. **Terletzky** objects to the high cost to homeowners along 12th Street. He mentions that the Village has the authority to decide how the cost of the pipe would be charged to the benefiting properties.

Terletzky proposes a 20/30/50 cost split as follows: 20 percent would be paid by the school, 50 percent would be paid by the proposed subdivision and the remaining would be paid by the residents along Highway E according to frontage. The subdivision would pay back the assessment when the lots are sold. He mentions that this solution would speed up repayment.

Chairman **Stoner** mentions that they have no control over developers and mentions the additional infrastructure cost incurred to develop a subdivision. He states that other communities also assess property owner for utilities going by their properties. He also disputes the benefits

Terletzky mentions that homeowners have been subsidizing growth in their community

by paying for the infrastructure and that it is not fair. He mentions that residents are not notified of upcoming assessments until it is too late.

Chairman **Stoner** asks what the public notification process is.

Administrator **Peters** explains the process and what would trigger notification.

Terletzky mentions that by the time homeowners are notified it is too late for residents to object. He mentions that by that time the project has been approved and bids are under way. He reiterates that having residents pay for 70 percent of the cost is wrong.

Commissioner **Boxx** makes a motion to approve the request.
Commissioner **Steege** seconds the motion.

Chairman **Stoner** is concerned that the stormwater part of the project has not started. He also mentions that he will not support the school expansion without connection to municipal water in part due to fire protection. He mentions his support for the overall projects but also mentions that future athletic fields would need to be serviced by municipal water for bathrooms and any concession stands.

Commissioner **Steege** mentions his concerns with the stormwater plan, the traffic analysis and appropriate parking.

Excel Engineer Jeremy **Wagner** mentions that they are coordinating with Kenosha County about the stormwater project. He mentions that they just received the traffic analysis and shows only one additional car stacking at the four-way intersection. As to the parking, he states that the project will add 250 stalls.

Commissioner **Steege** asks whether the parking calculation accounts for gymnasium activities.

Wagner states that it does.

Chairman **Stoner** asks whether they are designing the water system for the project.

Wagner states that they are doing the plumbing but not the water main coming to the property. He states that the Village Engineer will design that.

Engineer Brett **Biwer** mentions that the design is very preliminary so far.

9100 11th Street

Fire Chief **Andersen** whether the State has reviewed the fire suppression system.

Chief **Andersen** states that it would not happen until the project is approved. He states that while the State would not force the property owner to connect to the water but the local ordinance would within six months. He mentions that it would be more cost effective to

plan for municipal water connection at the time of the expansion.

Shoreland Lutheran High School President Paul **Scriver** mentions that they are waiting for a final decision about municipal water connection. He states that preliminary engineering states they do not need to connect. Once the school hears that the water connection is necessary, they will design the project accordingly. He states that the school is not opposed to connection to municipal water.

Commissioner **Thompson** asks about the process to design and bidding for the water line going down 12th Street.

Administrator **Peters** explained that the Village engineer would start the design once the project is approved.

Commissioner **Thompson** asks if the design process would start before a decision was made about how the project would be paid for. He also asks whether the subdivision project is moving forward.

Administrator **Peters** answers that there has been no contact with the subdivision developer for the past two years.

Commissioner **Steege** mentions that the commission is not responsible for deciding how the project is financed, only approving the plans. The question of the water connection will be up to the Village Board.

Guy Hawley - 9100 11th Street

Mr. **Hawley** mentions that the construction of his house is delayed until the school's stormwater system is in place.

Chairman **Stoner** interjects that this issue has nothing to do with the school's project. He is aware of the situation and offers to speak with Hawley after the meeting.

Commissioner **Boxx** makes a motion to approve the conditional use permit.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

11. Site Plan Review:

Discussion and Action Request by: Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requests site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.
Commissioner **Steege** seconds the motion.

Commissioner **Steege** asks details about the rooftop AC units.

Excel Engineer Alex **Fiebig** answers the questions and states that the number of unit has been reduced and features a two feet parapet.

Motion carried. 6-0.

Passes unanimously

12. Adjourn

Commissioner **Boxx** makes a motion to adjourn.
Commissioner **Steege** seconds the motion.
Motion carried 6-0 vote.

Passes unanimously

Drafted Oct. 16, 2024.

These minutes are not official until approved by the Plan Commission. Submitted by Assistant to the Administrator/Communication Specialist Kevin Poirier



VILLAGE OF SOMERS

Department of Planning and Development

Sept. 2021

RECEIVED

AUG 28 2024

REZONING APPLICATION

Kenosha County
Planning and Development

(a) Property Owner's Name:

Print Name: John Spaay Signature: 

Mailing Address: 6828 GRAND PKWY

City: WAUWATOSA State: WI Zip: 53213

Phone Number: 414-241-6125 E-mail (optional): spaayruthann@gmail.com

Note: Unless the property owner's signature can be obtained in the above space, a letter of agent status **signed** by the legal property owner **must** be submitted if you are a tenant, leaseholder, or authorized agent representing the legal owner, allowing you to act on their behalf.

(b) Agent's Name (if applicable):

Print Name: Steven Frederick Signature: _____

Business Name: Tenant

Mailing Address: 5025 12th St

City: Kenosha State: WI Zip: 53144

Phone Number: 262-515-3637 E-mail (optional): steve3637@live.com

(c) Tax key number(s) of property to be rezoned:

82-4-222-142-0070

Property Address of property to be rezoned:

5025 12th Street, Kenosha, WI 53144

(d) Proposed use (a statement of the type, extent, area, etc. of any development project):

Looking to split the 10 acre parcel down the middle from north to South, creating two
~5 acre parcels. The structures would all be within the newly created ~5 acre East parcel
and the West ~5 acre parcel would be a wooded lot.

REZONING APPLICATION

(e) Check the box next to any and all of the **existing** zoning district classifications present on the subject property:

<input type="checkbox"/> A-1 Agricultural Preservation District	<input type="checkbox"/> B-1 Neighborhood Business District
<input checked="" type="checkbox"/> A-2 General Agricultural District	<input type="checkbox"/> B-2 Community Business District
<input type="checkbox"/> A-3 Agricultural Related Manufacturing, Warehousing and Marketing District	<input type="checkbox"/> B-3 Highway Business District
<input type="checkbox"/> A-4 Agricultural Land Holding District	<input type="checkbox"/> B-4 Planned Business District
<input type="checkbox"/> AE-1 Agricultural Equestrian Cluster Single-Family District	<input type="checkbox"/> B-5 Wholesale Trade and Warehousing District
<input type="checkbox"/> R-1 Rural Residential District	<input type="checkbox"/> BP-1 Business Park District
<input checked="" type="checkbox"/> R-2 Suburban Single-Family Residential District	<input type="checkbox"/> B-94 Interstate Highway 94 Special Use Business District
<input type="checkbox"/> R-3 Urban Single-Family Residential District	<input type="checkbox"/> M-1 Limited Manufacturing District
<input type="checkbox"/> R-4 Urban Single-Family Residential District	<input type="checkbox"/> M-2 Heavy Manufacturing District
<input type="checkbox"/> R-4.5 Urban Single-Family Residential District	<input type="checkbox"/> M-3 Mineral Extraction District
<input type="checkbox"/> R-5 Urban Single-Family Residential District	<input type="checkbox"/> M-4 Sanitary Landfill and Hazardous Waste Disposal District
<input type="checkbox"/> R-6 Urban Single-Family Residential District	<input type="checkbox"/> I-1 Institutional District
<input type="checkbox"/> R-7 Suburban Two-Family and Three-Family Residential District	<input type="checkbox"/> PR-1 Park-Recreational District
<input type="checkbox"/> R-8 Urban Two-Family Residential District	<input type="checkbox"/> C-1 Lowland Resource Conservancy District
<input type="checkbox"/> R-9 Multiple-Family Residential District	<input type="checkbox"/> C-2 Upland Resource Conservancy District
<input type="checkbox"/> R-10 Multiple-Family Residential District	<input type="checkbox"/> FPO Floodplain Overlay District
<input type="checkbox"/> R-11 Multiple-Family Residential District	<input type="checkbox"/> PUD Planned Unit Development Overlay District
<input type="checkbox"/> R-12 Mobile Home/Manufactured Home Park-Subdivision District	<input type="checkbox"/> AO Airport Overlay District
	<input type="checkbox"/> RC Rural Cluster Development Overlay District

(f) Check the box next to any and all of the **proposed** zoning district classifications proposed for the subject property:

<input type="checkbox"/> A-1 Agricultural Preservation District	<input type="checkbox"/> B-1 Neighborhood Business District
<input type="checkbox"/> A-2 General Agricultural District	<input type="checkbox"/> B-2 Community Business District
<input type="checkbox"/> A-3 Agricultural Related Manufacturing, Warehousing and Marketing District	<input type="checkbox"/> B-3 Highway Business District
<input type="checkbox"/> A-4 Agricultural Land Holding District	<input type="checkbox"/> B-4 Planned Business District
<input type="checkbox"/> AE-1 Agricultural Equestrian Cluster Single-Family District	<input type="checkbox"/> B-5 Wholesale Trade and Warehousing District
<input type="checkbox"/> R-1 Rural Residential District	<input type="checkbox"/> BP-1 Business Park District
<input checked="" type="checkbox"/> R-2 Suburban Single-Family Residential District	<input type="checkbox"/> B-94 Interstate Highway 94 Special Use Business District
<input type="checkbox"/> R-3 Urban Single-Family Residential District	<input type="checkbox"/> M-1 Limited Manufacturing District
<input type="checkbox"/> R-4 Urban Single-Family Residential District	<input type="checkbox"/> M-2 Heavy Manufacturing District
<input type="checkbox"/> R-4.5 Urban Single-Family Residential District	<input type="checkbox"/> M-3 Mineral Extraction District
<input type="checkbox"/> R-5 Urban Single-Family Residential District	<input type="checkbox"/> M-4 Sanitary Landfill and Hazardous Waste Disposal District
<input type="checkbox"/> R-6 Urban Single-Family Residential District	<input type="checkbox"/> I-1 Institutional District
<input type="checkbox"/> R-7 Suburban Two-Family and Three-Family Residential District	<input type="checkbox"/> PR-1 Park-Recreational District
<input type="checkbox"/> R-8 Urban Two-Family Residential District	<input type="checkbox"/> C-1 Lowland Resource Conservancy District
<input type="checkbox"/> R-9 Multiple-Family Residential District	<input type="checkbox"/> C-2 Upland Resource Conservancy District
<input type="checkbox"/> R-10 Multiple-Family Residential District	<input type="checkbox"/> FPO Floodplain Overlay District
<input type="checkbox"/> R-11 Multiple-Family Residential District	<input type="checkbox"/> PUD Planned Unit Development Overlay District
<input type="checkbox"/> R-12 Mobile Home/Manufactured Home Park-Subdivision District	<input type="checkbox"/> AO Airport Overlay District
	<input type="checkbox"/> RC Rural Cluster Development Overlay District

REZONING APPLICATION

(g) Your request must be consistent with the existing planned land use category as shown on Map 65 of the adopted "Multi-Jurisdictional Comprehensive Plan for Kenosha County: 2035".

The existing planned land use category for the subject property is:

<input type="checkbox"/> Farmland Protection	<input type="checkbox"/> Governmental and Institutional
<input type="checkbox"/> General Agricultural and Open Land	<input type="checkbox"/> Park and Recreational
<input type="checkbox"/> Rural-Density Residential	<input type="checkbox"/> Street and Highway Right-of-Way
<input type="checkbox"/> Agricultural and Rural Density Residential	<input type="checkbox"/> Other Transportation, Communication, and Utility
<input checked="" type="checkbox"/> Suburban-Density Residential	<input type="checkbox"/> Extractive
<input type="checkbox"/> Medium-Density Residential	<input type="checkbox"/> Landfill
<input type="checkbox"/> High-Density Residential	<input type="checkbox"/> Primary Environmental Corridor
<input type="checkbox"/> Mixed Use	<input type="checkbox"/> Secondary Environmental Corridor
<input type="checkbox"/> Commercial	<input type="checkbox"/> Isolated Natural Resource Area
<input type="checkbox"/> Office/Professional Services	<input type="checkbox"/> Other Conservancy Land to be Preserved
<input type="checkbox"/> Industrial	<input type="checkbox"/> Nonfarmed Wetland
<input type="checkbox"/> Business/Industrial Park	<input type="checkbox"/> Surface Water

(h) Attach a plot plan or survey plat of property to be rezoned (showing location, dimensions, zoning of adjacent properties, existing uses and buildings of adjacent properties, floodways and floodplains)—drawn to scale.

(i) The Village of Somers Department of Planning and Development may ask for additional information.

(j) The name of the County Supervisor of the district wherein the property is located (District Map):

Supervisory District Number: 15 County Board Supervisor: Dave Geertsen

(k) The fee specified in Section 12.05-8 of this ordinance.

Request for Rezoning Petition (payable to "Kenosha County").....\$1,450.00

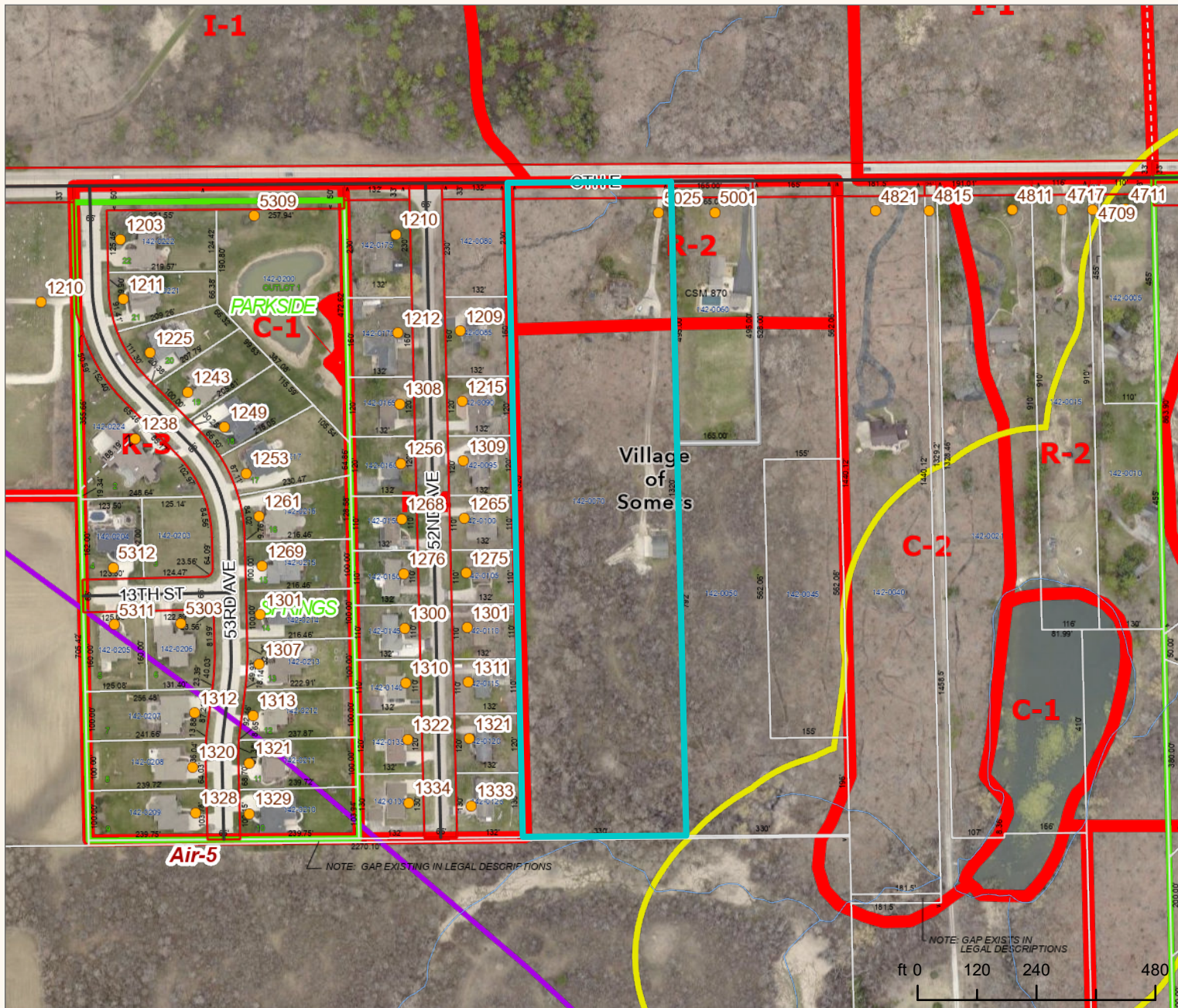
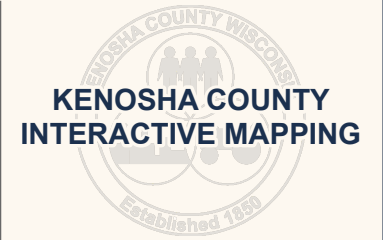
(For other fees see the Fee Schedule)

Note: Agricultural Use Conversion Charge

The use value assessment system values agricultural land based on the income that would be generated from its rental for agricultural use rather than its fair market value. When a person converts agricultural land to a non-agricultural use (e.g. residential or commercial development), that person may owe a conversion charge. To obtain more information about the use value law or conversion charge, contact the Wisconsin Department of Revenue's Equalization Section at 608-266-2149 or visit <http://www.revenue.wi.gov/faqs/slf/useassmt.html>.

Note that the act of rezoning property from an agricultural zoning district to a non-agricultural zoning district does not necessarily trigger the agricultural use conversion charge. It is when the use of the property changes from agricultural that the conversion charge is assessed.

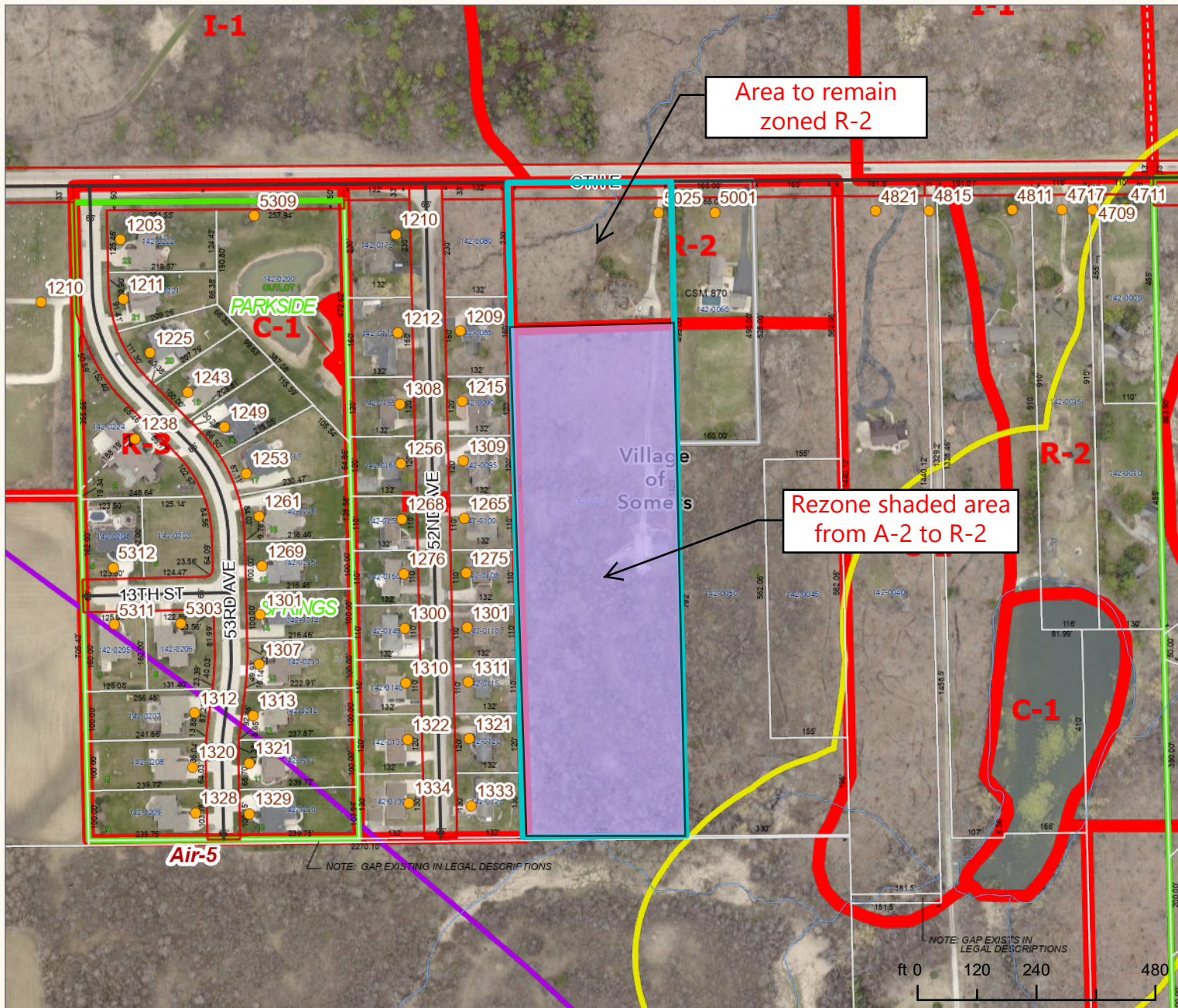
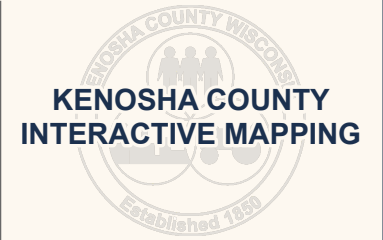
82-4-222-142-0070 - EXISTING ZONING



DISCLAIMER This map is neither a legally recorded map nor a survey and is not intended to be used as one. This drawing is a compilation of records, data and information located in various state, county and municipal offices and other sources affecting the area shown and is to be used for reference purposes only. Kenosha County is not responsible for any inaccuracies herein contained. If discrepancies are found, please contact Kenosha County.

Date Printed: 9/5/2024

82-4-222-142-0070 - PROPOSED ZONING



DISCLAIMER This map is neither a legally recorded map nor a survey and is not intended to be used as one. This drawing is a compilation of records, data and information located in various state, county and municipal offices and other sources affecting the area shown and is to be used for reference purposes only. Kenosha County is not responsible for any inaccuracies herein contained. If discrepancies are found, please contact Kenosha County.

Date Printed: 9/5/2024



VILLAGE OF SOMERS

Department of Planning & Development

VILLAGE OF SOMERS REZONING PROCEDURES

- 1. Contact the Kenosha County Department of Planning & Development and check with staff to determine if your proposed zoning change meets the requirements of the Multi-Jurisdictional Comprehensive Plan for Kenosha County: 2035, Village of Somers General Zoning and Shoreland/Floodplain Zoning Ordinance and the Village of Somers Land Division and Platting Control Ordinance. Note: If the proposed rezoning is part of a proposed land division see the Certified Survey Map Information and Procedures.

- 2. Contact the Kenosha County Department of Planning & Development and schedule a pre-conference meeting, which is required for all rezoning requests.

Meeting Date: 05/02/24

- 3. Contact the Village of Somers to determine if your rezoning petition requires concept review by the Village Plan Commission. If so, contact the Village of Somers clerk to schedule a concept meeting with the Village Plan Commission.

Meeting Date: 05/02/24

- 4. Complete and submit to the Kenosha County Department of Planning & Development the Village of Somers Rezoning Application by the filing deadline.

Filing Deadline: 08/30/2024

- 5. Upon submission you will be given two copies of the date-stamped application. Submit 10 copies of the date-stamped application to the Village of Somers clerk for placement on the agendas of the Village of Somers Plan Commission and the Village of Somers Board and keep one for your records.

- 7. Attend the Village Plan Commission and the Village Board meetings. **NOTE:** You must attend or the Village will not be able to act on your request. At these meetings you will be asked to brief the committee on your request.

Village Plan Commission meeting date (tentative): 10/14/2024

Village Board meeting date (tentative): 10/22/2024

- 8. Village clerk will provide written notice of final action to property owner/applicant.

IMPORTANT TELEPHONE NUMBERS

Kenosha County Center
 Department of Public Works & Development Services
 19600 - 75th Street, Suite 185-3
 Bristol, Wisconsin 53104-9772

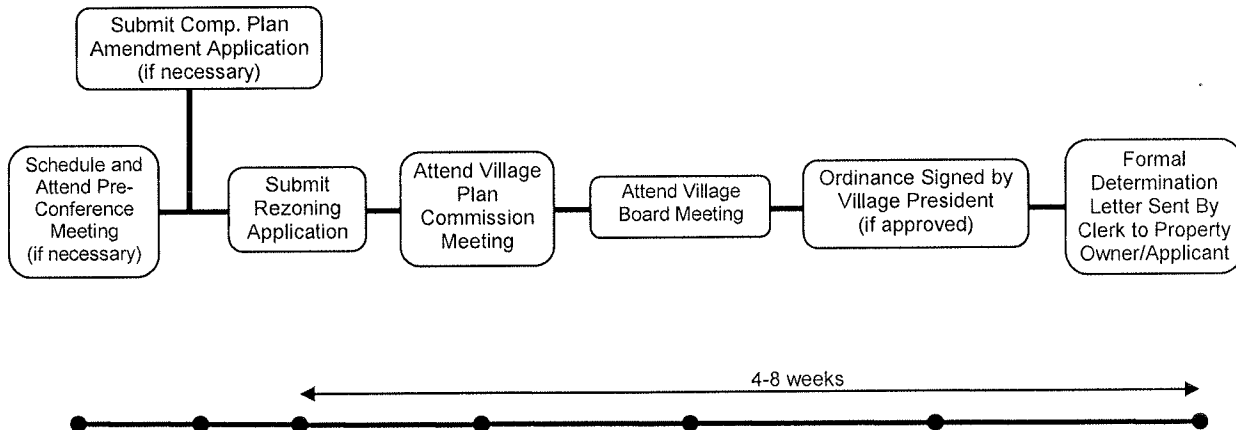
Division of Planning & Development (including Sanitation & Land Conservation) **857-1895**
 Facsimile #..... 857-1920

Public Works Division of Highways 857-1870

Administration Building
 Division of Land Information..... 653-2622

Village of Somers 859-2822
 Wisconsin Department of Natural Resources - Sturtevant Office 884-2300
 Wisconsin Department of Transportation - Waukesha Office 548-8722

Rezoning Procedure Timeline



For Reference Purposes

Village Land Use Plan District Designation(s) (if applicable):

Present Suburban-Density Residential

Proposed Suburban-Density Residential

Present Use(s) of Property: Suburban-Density Residential

Proposed Use(s) of Property: Suburban-Density Residential

The subdivision abuts or adjoins a state trunk highway Yes () No ()

The subdivision will be served by public sewer Yes () No ()

The subdivision abuts a county trunk highway Yes () No ()

The subdivision contains shoreland/floodplain areas Yes () No ()

The subdivision lies within the extra-territorial plat (ETP) authority
area of a nearby Village or City Yes () No ()

*Applicant is responsible for submitting to the ETP authority any fees and documentation
needed to obtain a recommendation.

REQUIRED SIGNATURE(S) FOR ALL APPLICATIONS:

Property Owner's Signature

Date

Property Owner's Signature

Date

REQUIRED APPLICABLE SIGNATURES:

Applicant's Signature

Date

Developer's Signature

Date

PRELIMINARY CERTIFIED SURVEY MAP

KENOSHA COUNTY CERTIFIED SURVEY MAP No. _____

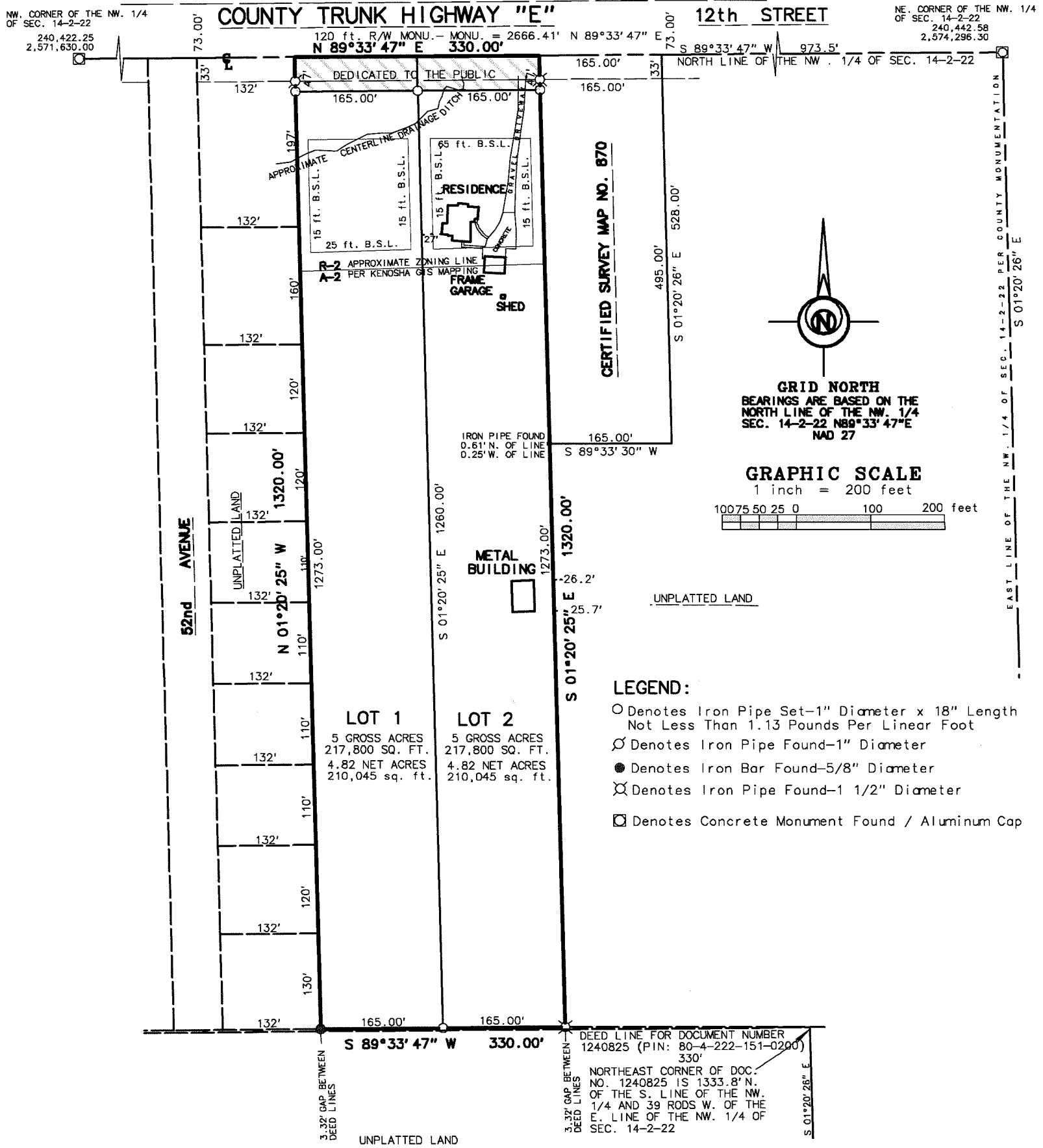
PART OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 14,
TOWNSHIP 2 NORTH, RANGE 22 EAST OF THE 4TH PRINCIPAL MERIDIAN, VILLAGE
OF SOMERS, COUNTY OF KENOSHA AND STATE OF WISCONSIN

ORDERED BY:

Steve Fredrick, Agent For The Owner
5025-12th St.
Kenosha, WI. 53144

SURVEYED & MAPPED BY:

AMBIT LAND SURVEYING
8120-12th Ave.
Wheatland, WI. 53105
262-537-4874
arbitpls@gmail.com



PRELIMINARY CERTIFIED SURVEY MAP

KENOSHA COUNTY CERTIFIED SURVEY MAP No. : _____

PART OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 14,
TOWNSHIP 2 NORTH, RANGE 22 EAST OF THE 4TH PRINCIPAL MERIDIAN, VILLAGE
OF SOMERS, COUNTY OF KENOSHA AND STATE OF WISCONSIN

SURVEYOR'S CERTIFICATE

I, Mark A. Bolender, Professional Land Surveyor, hereby certify:

That I have surveyed and mapped a part of the Northeast Quarter of the Northwest Quarter of Section 14, Township 2 North, Range 22 East of the 4th Principal Meridian, Village of Somers, Kenosha County, Wisconsin, described as follows:

Part of the Northeast Quarter of the Northwest Quarter of Section 14, Town 2 North, Range 22 East of the 4th Principal Meridian and more particularly described as follows: COMMENCING at the northeast corner of the Northwest Quarter of said section; THENCE South 89 degrees 33 minutes 47 seconds West, grid bearing from North, Wisconsin Coordinate System, South Zone, NAD 27, along the north line of said quarter section 973.50 feet to the POINT OF BEGINNING; THENCE South 01 degree 20 minutes 25 seconds East and parallel with the east line of said quarter section 1320.00 feet; THENCE South 89 degrees 33 minutes 47 seconds West and parallel with the north line of said quarter section 330.00 feet; THENCE North 01 degree 20 minutes 25 seconds West and parallel with the east line of said quarter section 1320.00 feet and to the north line of said quarter section; THENCE North 89 degrees 33 minutes 47 seconds East along the north line of said quarter section 330.00 feet to the POINT OF BEGINNING, containing 10.0 acres of land, more or less, Village of Somers, County of Kenosha and State of Wisconsin.

That I have made such survey, land division, and map by the direction of Steve Fredrick, agent to the owner of said land.

That such map is a correct representation of the exterior boundaries of the land surveyed and the division thereof.

That I have fully complied with the provisions of Chapter 236.34 of the Wisconsin Statutes and the subdivision regulations of the Village of Somers, in surveying, dividing, and mapping the same.

_____ Dated this 26th day of August, 2024.
Mark A. Bolender, P.L.S. No. S - 1784

VILLAGE OF SOMERS APPROVAL

Approved as a Certified Survey Map this _____ day of _____, 2024.

_____, Date _____
Jason Peters, Village Administrator

_____, Date _____
Wendy Burnette, Village Clerk/Treasurer

OWNERS CERTIFICATE

STATE OF WISCONSIN }
COUNTY OF KENOSHA } S.S.

As owner(s), I(we) hereby certify that I(we) caused the land described on this plat to be surveyed, divided, mapped and dedicated as represented on this Certified Survey Map. I(we) further certify that this Certified Survey Map to be submitted to the following for approval or objection:
Village of Somers

Witness the hand and seal of said Owner this _____ day of _____, 2024
in the presence of

John Spay

Ruthann Spay

Witness

Personally came before me the above named Owner(s), to me known to be the person who executed the foregoing instrument and acknowledged the same.

Notary Public _____

Kenosha County, Wisconsin My Commission Expires _____



TONY EVERS
GOVERNOR
KATHY BLUMENFELD
SECRETARY
Plat Review
PO Box 1645, Madison WI 53701
E-mail: plat.review@wi.gov
<https://doa.wi.gov/platreview>

October 09, 2024

Mark A. Bolender
AMBIT LAND SURVEYING
ambit@tds.net

FILE NO. 122027
Certified Survey Map

Village of Somers, Kenosha County

Dear Mark A. Bolender:

Per request, we have reviewed this certified survey map (CSM) for compliance with s. 236.34 Wis. Stats. Our comments are as follows:

REVIEW COMMENTS:

s. 236.20 (2) (c) It appears that the 1260.00' length shown for the line common to Lot 1 & Lot 2 should be changed to 1273.00' so that the lots will close to the net areas as noted.

s. 236.20 (2) (j) The gross areas shown for the lots are not appropriate and should be removed as the dedicated to the public areas are not part of the lots.

s. 236.34 (1m) (c) The CSM must be plotted for recording on 8.5" x 14" durable white media with a 1 1/2" binding margin along the top and 1/2" margins on all other sides (the version submitted to the department for review is on 11" x 17" paper size).

s. 236.34 (1m) (d) The Surveyor certificate must state compliance with Section (not Chapter) 236.34, Wis. Stats.

Note: The north part of the line common to lots 1 & 2 does not extend to the section line and should be removed.

If you have any questions regarding this review, please contact me as listed above.

Regards,

Don Sime, PLS
Plat Review

Enc: Certified Survey Map

cc: Clerk, Village of Somers

PRELIMINARY CERTIFIED SURVEY MAP

KENOSHA COUNTY CERTIFIED SURVEY MAP No. _____

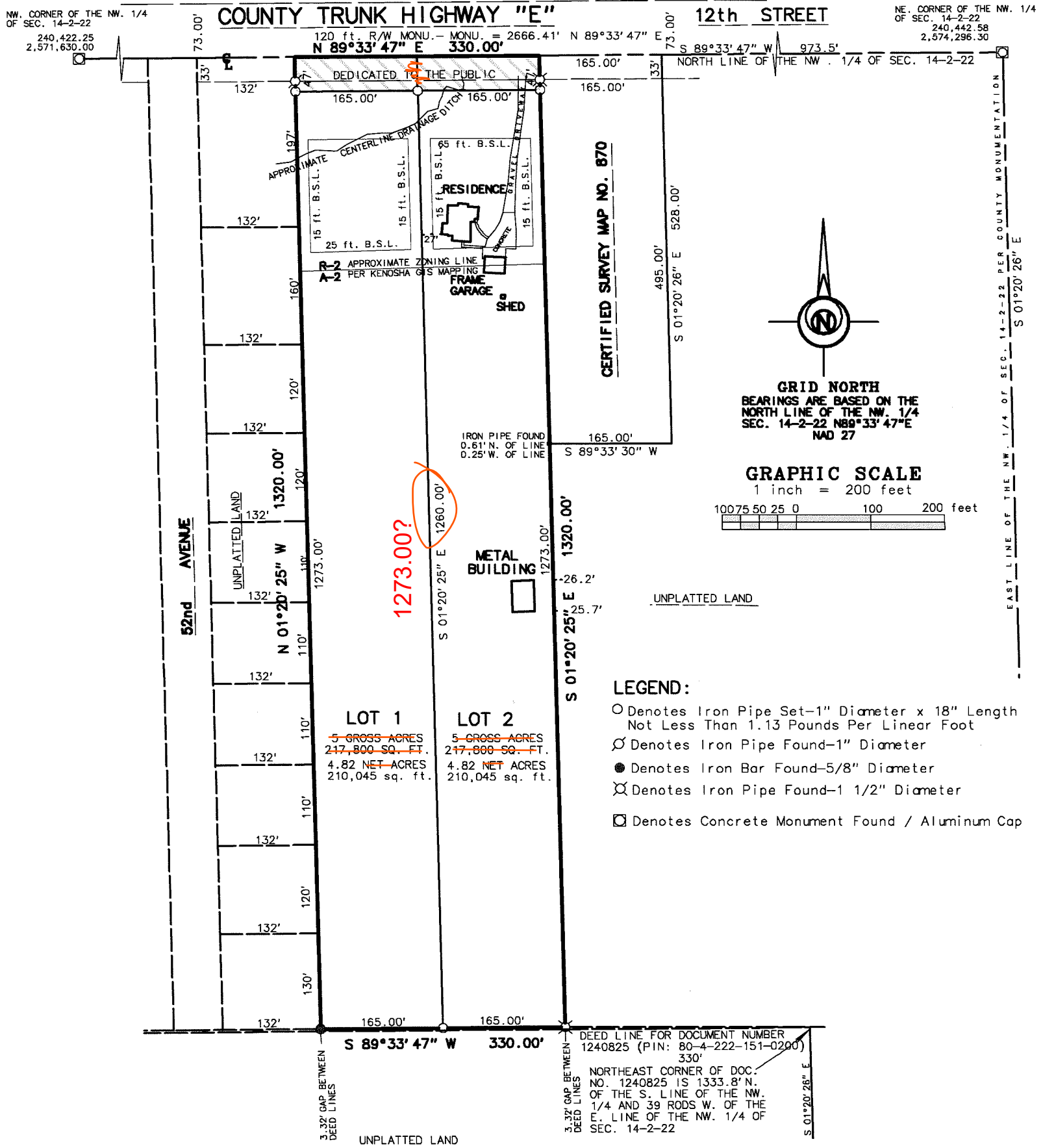
PART OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 14,
TOWNSHIP 2 NORTH, RANGE 22 EAST OF THE 4TH PRINCIPAL MERIDIAN, VILLAGE
OF SOMERS, COUNTY OF KENOSHA AND STATE OF WISCONSIN

ORDERED BY:

Steve Fredrick, Agent For The Owner
5025-12th St.
Kenosha, WI. 53144

SURVEYED & MAPPED BY:

AMBIT LAND SURVEYING
8120-12th Ave.
Wheatland, WI. 53105
262-537-4874
arbitpls@gmail.com



PRELIMINARY CERTIFIED SURVEY MAP

KENOSHA COUNTY CERTIFIED SURVEY MAP No. : _____

PART OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 14,
TOWNSHIP 2 NORTH, RANGE 22 EAST OF THE 4TH PRINCIPAL MERIDIAN, VILLAGE
OF SOMERS, COUNTY OF KENOSHA AND STATE OF WISCONSIN

SURVEYOR'S CERTIFICATE

I, Mark A. Bolender, Professional Land Surveyor, hereby certify:

That I have surveyed and mapped a part of the Northeast Quarter of the Northwest Quarter of Section 14, Township 2 North, Range 22 East of the 4th Principal Meridian, Village of Somers, Kenosha County, Wisconsin, described as follows:

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That I have made such survey, land division, and map by the direction of Steve Fredrick, agent to the owner of said land.

That such map is a correct representation of the exterior boundaries of the land surveyed and the division thereof.

That I have fully complied with the provisions of section Chapter 236.34 of the Wisconsin Statutes and the subdivision regulations of the Village of Somers, in surveying, dividing, and mapping the same.

_____ Dated this 26th day of August, 2024.
Mark A. Bolender, P.L.S. No. S - 1784

VILLAGE OF SOMERS APPROVAL

Approved as a Certified Survey Map this _____ day of _____, 2024.

_____, Date _____
Jason Peters, Village Administrator

_____, Date _____
Wendy Burnette, Village Clerk/Treasurer

OWNERS CERTIFICATE

STATE OF WISCONSIN }
COUNTY OF KENOSHA } S.S.

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Village of Somers

Witness the hand and seal of said Owner this _____ day of _____, 2024
in the presence of

John Spay Ruthann Spay

Witness

Personally came before me the above named Owner(s), to me known to be the person who executed the foregoing instrument and acknowledged the same.

Notary Public _____

Kenosha County, Wisconsin My Commission Expires _____

**VILLAGE OF SOMERS
VILLAGE BOARD
MEETING ITEM MEMORANDUM**

MEETING DATE: November 12th, 2024

TO: Village President Stoner and Village Trustees

PREPARED BY: Jason J. Peters, Administrator

AGENDA ITEM: #9 Action on Plan Commission Recommendation (Shoreland):

- a. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. (For information use only, the property is located at 9026 12th Street.)
- b. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. (For information use only, the property is located at 9026 12th Street.)

BACKGROUND:

In April of 2024, the Board approved a Comp Plan Change, Rezoning and CSM so that Shoreland Lutheran High School could install a stormwater management system. The proposed system is what is known as a Regenerative Stormwater Conveyance. This system was also in preparation for any expansions to their facilities. In August of 2024, Staff met with Shoreland Lutheran to review the proposed additions to their existing buildings. The proposed additions will include the following: an expanded music room, 8 new classrooms, a new main entry/vestibule area, a new fitness center, and a new gymnasium. The existing gymnasium will also be remolded as a part of the plan. The project requires a conditional use permit for the new gymnasium and site plan/exterior fenestration. Shoreland Lutheran submitted their applications at the end of August.

PRIOR ACTION TAKEN:

The Plan Commission met on October 14th to review Shoreland Lutheran's conditional use permit application and the requisite public hearing was held. Public comments were given. One individual was concerned over possible parking on the road and noise from the gymnasium during events. The main concern during public comments stems from the possibility of a water main being brought to the site, especially how it would be paid for and how would the residents be assessed. Administrator Peters will note that under the Village's current ordinance 13.07 (E) the water main would be assessed by front footage. The section reads as follows:

“The amount assessed against any property shall be computed on a front footage basis using the total cost of the improvement including the construction cost for the size of main required to adequately serve and benefit the property as determined by the Utility, but in no case shall such main be less than eight inches in diameter nor shall such main be larger than 12 inches in diameter. The costs of engineering and legal services and any other component of direct or indirect cost which is attributed to the improvement, including the cost of that portion of the water main in intersections of streets shall be included in computing the costs of the improvement.”

The Board may recall that in 2021, Mastercraft proposed a subdivision across the street from this site. At that time the estimate for the total cost of bring water main from 100th Avenue was \$807,000. As this estimate is several years old, Engineering has provided an updated opinion of probable cost (attached as item 9m). This new cost is approximately \$1.5 million and includes running the line to the corner of CTH E and CTH H. The Plan Commission's recommendation was to approve the request for the conditional use permit (6-0) and the request for site plan and exterior fenestration. (6-0).

The Board reviewed this matter at our November 6th Work Session. At be clear, their stormwater plans were submitted and approved subject to conditions in April. Final plans have not been fully approved by Engineering, but it has always been Shoreland intention to install the regenerative storm water conveyance system. They have indicated that they would start this project at the same time they begin work on their expansion.

As stated in my previous memorandum, if the Board determines that they are agreeable to Shoreland Lutheran's planned expansion, that clear direction be given as to whether the water main will be extended from 100th Avenue. The Board will also need to determine how this project will be funded. Administration also recommends that once a clearer picture on how the main will be extended that a meeting be held with those property owners that have front footage along CTH E.

Another topic of concern was the Traffic Impact Analysis. Shoreland has provided their TIA to the County. The County is in the process of reviewing this information. On November 8th, Staff and Village Engineering met with the County's Highway Director to review comments the County will be providing. We expect the County to send their comments to Shoreland in the very near future.

SUGGESTED ACTION/ACTION REQUESTED/COMMENTS:

Based on direction from the Board on November 6th, Staff would recommend approval of the project subject to conditions. This will allow Shoreland to move forward with their plans, including submitting them to the State of Wisconsin. As I stated at our November 6th Work Session, the approval subject to conditions does not mean that this project is a “done deal.” Comments from our Engineers, comments from County Highways, and the water main issue will all still need to be addressed. In the event that the Village Board agrees with the suggested action, a suggested motion to approve would be as follows:

9a

“Motion to approve request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers, subject to the following conditions:

1. *Subject to conditions as listed in Planning Memo dated September 30th, 2024;*
2. *Subject to final plans being reviewed and approved by Village Planning;*
3. *Subject to conditions as listed in Village Engineer Memo dated October 14th, 2024;*
4. *Subject to final plans being reviewed and approved by Village Engineer;*
5. *Subject to final plan being reviewed and approved by Kenosha County Highway Commissioner / Director of Highways; and*
6. *Execution of a Developer’s Agreement between the Village of Somers and Shoreland Lutheran High School Federation, Inc.”*

9b

“Motion to approve request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers, subject to the following conditions:

1. *Subject to conditions as listed in Planning Memo dated September 30th, 2024;*
2. *Subject to final plans being reviewed and approved by Village Planning;*

3. *Subject to conditions as listed in Village Engineer Memo dated October 14th, 2024;*
4. *Subject to final plans being reviewed and approved by Village Engineer;*
5. *Subject to final plan being reviewed and approved by Kenosha County Highway Commissioner / Director of Highways; and*
6. *Execution of a Developer's Agreement between the Village of Somers and Shoreland Lutheran High School Federation, Inc."*

ATTACHMENTS:

Planning and Zoning Memo to Plan Commission dated September 30th, 2024

Engineering Comments

Plan Commission Minutes – Highlighted

Conditional Use Application

Conditional Use Narrative

Civil Plans

Stormwater Memo

Floor Plans

Exterior Elevations

Landscaping Plans

Checklist

Traffic Impact Analysis

Updated Opinion of Cost



KENOSHA COUNTY

Shelly Billingsley, Director
Department of Public Works &
Development Services

Andy M. Buehler, Director
Division of Planning & Development

TO: Village of Somers Plan Commission
FROM: Luke Godshall, Senior Land Use Planner
APPLN DATE: 08-30-24
RPT DATE: 09-30-24
MTG DATE: 10-14-24
RE: Shoreland Lutheran H.S. Conditional Use Permit & Site Plan Review

PROJECT/SITE INFO:

1. Petitioner/Agent: Tom Schermerhorn, Excel Engineering, Inc.
2. Property Owner: Shoreland Lutheran High School
3. Location/Address: 9026 12TH ST
4. Tax key Number(s): 82-4-222-084-0272
5. Area: 55.74 acres
6. Existing Zoning: I-1 Institutional Dist.
7. Proposed Zoning: I-1 Institutional Dist.
8. Existing Land Use: Governmental and Institutional
9. Proposed Land Use: Governmental and Institutional

PROJECT OVERVIEW:

The Petitioner requests conditional use and site plan/building fenestration review and approval for several proposed building additions onto the existing Shoreland Lutheran High School building, along with additional parking areas. The site is zoned I-1 Institutional District, which requires conditional use approval for school auditoriums, gymnasiums and stadiums (the submitted plans indicate the addition of a new practice gymnasium, as well as the remodeling of the existing gymnasium).

PLANNER COMMENTS:

Building Additions & Design:

The submitted plans indicate a total area of 31,569 sq. ft. of building additions, with another 11,047 sq. ft. of existing areas to be remodeled. The new building additions include an expanded music room, 8 new classrooms, a new main entry/vestibule area, a new fitness center, and a new gymnasium (in addition to remodeling the existing gymnasium). The new gymnasium requires conditional use approval under Village ordinance.

Building elevations and renderings submitted indicate the building additions to be constructed with a variety of materials including brick veneer, fiber cement panels, and aluminum composite all in a variety of colors. In addition, the existing school building is shown to have its exterior updated with new brick finish and exterior nichia panels.

All proposed building additions meet all minimum required setback distances from property lines and is less than the maximum allowable building height of 60'.

Parking:

The submitted site plans indicate new parking areas to be added along the south and east ends of the site, with the new parking at the east end of the site replacing the area of the baseball field. The number of total parking spaces according to the submitted plans is 533 spaces. Additionally, the plan indicates 8 bus parking stalls.

Village zoning ordinance parking requirements for colleges and high schools calls for 1 space for every 5 students, plus one space for each employee on the largest work shift. Of the 533 total spaces, 70 would be for employees (according to the information provided on the submitted site plan review checklist). This leaves 463 spaces available for students, enough to accommodate 2,315 students.

The conditional use for the proposed gymnasium requires an additional parking requirement of one space for every 3 seats within the gymnasium. The submitted plans do not indicate the seating capacity of the new and remodeled gymnasiums to be able to determine if the total of 533 parking spaces would be enough to accommodate employees, students and gymnasium seating requirements. Staff has requested the Petitioner provide the total seating capacity of both the new gymnasium and remodeled existing gymnasium.

Sixteen (16) of the proposed parking spaces shown at the southwestern portion of the site would be located closer than the normally required 20' setback from a public right-of-way. The width of the CTH E right-of-way dedication was recently widened, causing Shoreland's existing parking spaces along CTH E to be closer than the 20'. Shoreland wishes to locate the new parking spaces to be in line with those existing spaces to the east and has requested the reduced setback as part of the Conditional Use and site plan approval.

All parking spaces on the submitted site plans are shown to be 9'x18' in size and provide for 24' wide aisles between the ends of parking spaces to meet ordinance requirements.

Landscaping:

A landscaping plan (dated 8-27-24 prepared by Heller & Associates, LLC) was included with the submitted application materials. The plan provides for a wide variety of deciduous and evergreen trees, shrubs and perennials and also indicates that existing trees and vegetation would be retained.

While the landscaping plan generally meets ordinance landscaping requirements, it is recommended that berms be utilized along the parking areas which abut the CTH E and CTH H rights-of-way. The landscaping section of the zoning ordinance indicates that "the use of berms shall constitute over 75% of the parking areas abutting a right-of-way. The berm shall be designed to be meandering and undulating with a minimum height of four feet with slopes no greater than 4:1."

Lighting:

A site photometric plan (dated 8-30-24 prepared by Excel) was included with the submitted application materials. The plan shows that the proposed lighting intensity and proposed lighting fixtures meet ordinance standards.

This project will require approval from the City of Kenosha for an Airport Site Plan review, as the property is located within a City of Kenosha Airport Overlay zoning district.

Planning & Development will issue zoning permits for the project only after the Petitioner has executed a Developer's Agreement with the Village and has met any other of the Village's requirements for the development.

STAFF RECOMMENDATION:

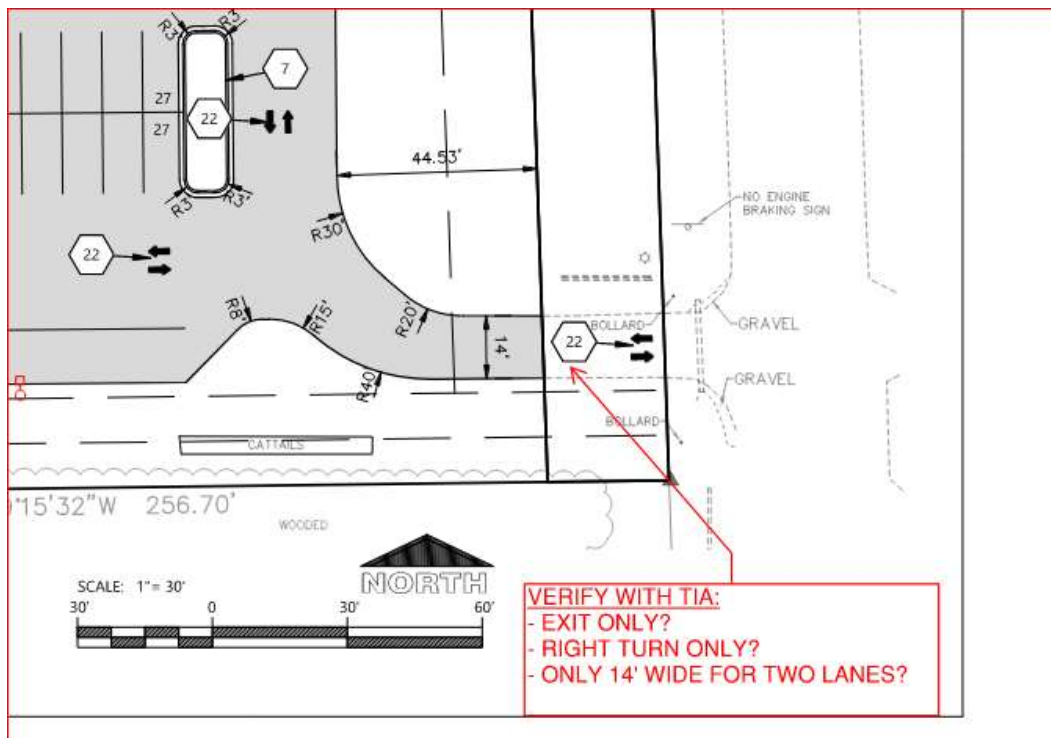
Should the Plan Commission choose to recommend approval of this project, staff would recommend the following conditions:

1. The Conditional Use shall be subject to the conditions present in the Village of Somers General Zoning and Shoreland/Floodplain Zoning Ordinance Section ZN 5.03(8)(b)121; School auditoriums, gymnasiums and stadiums in the I-1 District:
 - a. At least one (1) off-street parking space shall be provided for every three (3) seats located within the auditorium, gymnasium or stadium
(Note: Petitioner shall provide seating capacity total of the gymnasiums to determine if this condition is being met)
 - b. Ingress and egress to the premises shall be determined with due regard to topography and public road and pedestrian traffic taking into consideration hills, curves, speed limits and vision clearance.
 - c. Night lighting shall not shine on adjoining property.
 - d. In the case of outdoor stadiums, the parking area shall be paved or maintained in a dust free condition.
2. The Conditional Use approval to allow for the addition of sixteen (16) parking spaces at the southwest portion of the site to be located closer than 20' from the public right-of-way.
3. Subject to a revised landscaping plan being submitted which includes berms along at least 75% of the parking areas abutting a right-of-way.
4. Subject to submitting plans to be reviewed/approved for any proposed trash enclosures to be located on the site.
5. Subject to receiving approval of any necessary Traffic Impact Analysis (TIA) as may be required by Kenosha County Division of Highways.
6. Subject to receiving approval of an Airport Site Plan Review by the City of Kenosha.
7. Subject to final review and approval by the Village of Somers engineer.
8. Subject to approval by the Somers Fire & Rescue Department.
9. Subject to a Kenosha County zoning permit being issued for any proposed structures including principal buildings, signage, fences, accessory buildings, etc.
10. Subject to implementation of a Developer's Agreement between the Petitioner and the Village of Somers.

Shoreland Lutheran High School
October 14, 2024 Plan Commission Meeting
Village of Somers

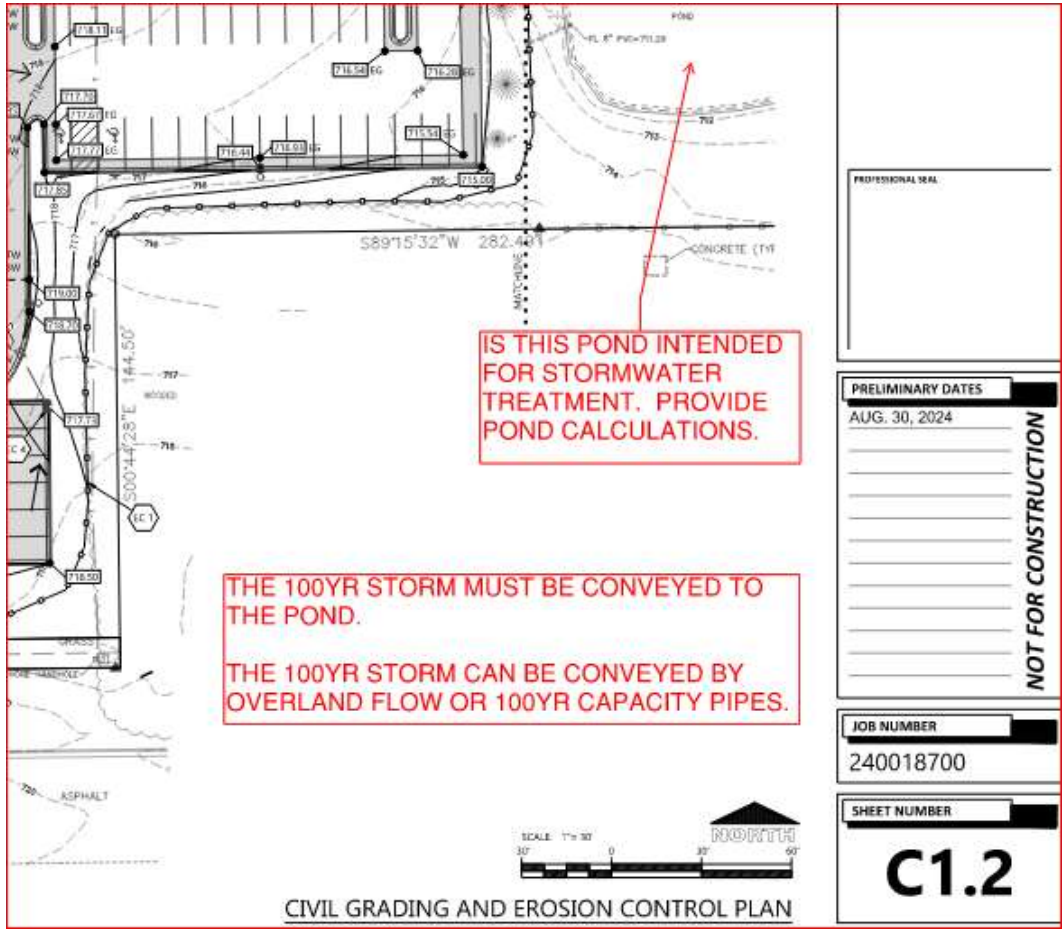
We reviewed the plans and stormwater memo dated August 30, 2024; our comments are listed below.

1. Provide full engineering plans and calculations consolidating the stormwater plan and the site development plan.
2. It is our understanding that the school may be required to extend the public water main to provide water service and fire protection. Provide engineering plans if required.
3. Sheet C1.1: Verify that the existing driveway to CTH H can remain based on the traffic impact analysis (TIA). The driveway is too narrow for two lanes of traffic as shown on the plans.



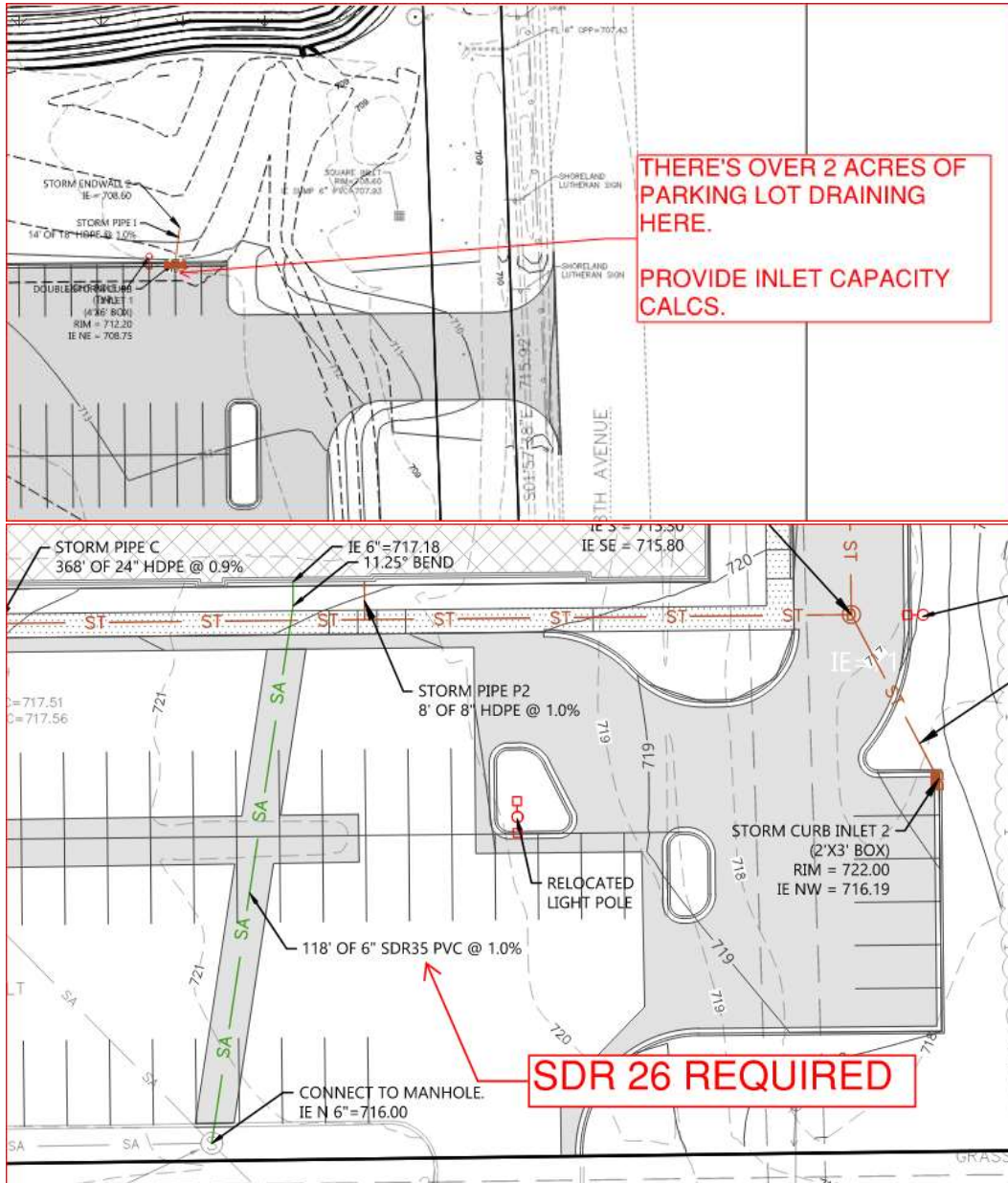
4. Sheet C1.2:

- a. Provide pond calculations if the existing pond is intended to provide storm water treatment.
- b. The 100-yr storm must be conveyed to the stormwater ponds, using overland flow or 100-yr capacity pipes.



5. Sheet C1.3:

- a. Provide inlet capacity calculations for the parking lot.
- b. Verify that the stormwater is not overtopping the inlet and bypassing to the CTH H ditch.
- c. SDR 26 PVC pipes are required for sanitary laterals.



Status of Engineer's Approval: Not Approved

Brett D. Biber

Brett D. Biber, PE

Cc: Jason J. Peters, JD/MPA – Village Administrator
George Stoner – Village President
Doug Snyder, P.E. – Village Engineer





Village of Somers
Proceeding from the Village Plan
Commission Meeting
Oct. 14, 2024

1. Call to Order

Chairman Stoner called the meeting to order at 5:28 p.m.

Present:

- Chairman George **Stoner**
- Commissioner Troy **Steege**
- Commissioner Gregg **Thompson**
- Commissioner Vinnie **Chambers**
- Commissioner Don **Boxx**
- Commissioner Michael **DeLuca**

Absent:

- Commissioner Jerry **Romanowski**
- Alternate Pat **Juliana**

Staff present in person: Administrator Jason Peters, Assistant to the Administrator Kevin Poirier, Somers Fire Chief Ben Andersen, Deputy Chief Jodin Froeber

Village Trustees: Jackie Nelson

Kenosha County Planners: Luke Godshall

Village Engineer: Brett Biber

2. Pledge of Allegiance

Chairman **Stoner** led everyone in the Pledge of Allegiance.

3. Approve Minutes of the Aug. 12, 2024 Meeting

Commissioner **Boxx** moved to approve to the minutes.

Seconded by Commissioner **Steege**.

Motion carried. 6-0 vote.

4. Correspondence

None

5. Citizen Comments

Tom **Harper** – 8910 12th Street

Mr. **Harper** mentioned that his property is very close to the school and bring up parking concerns. He mentions that cars have parked on the road during events and it is a safety concern, He also mentions noise from indoor and outdoor events and hopes that sound proofing options will be considered.

- 6. Rezone: Public Hearing and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requesting approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

- 7. Land Division (Certified Survey Map): Discussion and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requests approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Thompson** seconds the motion.

Motion carried. 6-0.

Passes unanimously

- 8. Comprehensive Plan Amendment: Public Hearing and Action on Request by:** Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St., Kenosha, WI 53142 (Agent); requests an amendment to the land use plan map for the Village of Somers (Map 80) as adopted in the Multi-

Jurisdictional Comprehensive Plan for Kenosha County: 2035 from High-Density Residential to Medium-Density Residential, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Administrator **Peters** mentions that there have been changes to the project since the concept was presented to the board at their July 8th meeting.

Bear Development Project Manager Dan **Szczap** mentions that the project presented was a residential mix of 33 single-family homes and 64 two-family buildings that received positive feedback when it was presented in July. They therefore started the process to re-zone and amend the comprehensive land use plan accordingly. They are proposing zero lot-line parcels to accommodate the two-family buildings. It would look like a condominium but be on a fee-simple lot. They have done this before in Mount Pleasant.

Szczap suggests three ways to move forward:

1. Move forward with the concept plan and using a PUD to handle deviations
2. Changing the two-family dwellings to condos
3. Changing the zoning code to accommodate zero-lot-line development

Administrator **Peters** mentions that Bear is also looking at the possibility of filling the site and changing it to single-family homes.

Szczap mentions that the soil condition and high ground water on the property has been challenging and that the fill would allow for the structures to be built with basements. They are in discussion to secure fill for the site but that has not been finalized.

Commissioner **Boxx** gets confirmation from **Szczap** that if sufficient fill is secured, the duplexes would be changed to single-family homes on the entire site. If the fill is not secured, the developer would plan to move forward with zero-lot-line dwellings to accommodate the challenges of the site and be a financially viable project. The developer states that condominiums is a complicated form of ownership and is not their preference.

Commissioner **Boxx** mentions that the developer is asking for a reduction of the setbacks from what the zoning allows.

Commissioner **Steege** asks whether, in the case the site is converted to single-family homes, the setbacks would be compliant with current ordinances.

Szczap states that he is not sure as the site plan is likely to change and they may ask for

minor relief.

Commissioner **Steege** asks for clarification about the changes from the plan resented in July.

Szczap states that the building and roads through the site did not change, only the lot lines through the duplexes have changed to create zero-lot-line fee-simple lots.

Commissioner **Boxx** asks details about the maintenance of the property.

Szczap explains that declarations, homeowner association and/or covenants would govern maintenance, common areas, architecture and insurance.

Commissioner **DeLuca** asks again whether the developer would still ask for zero-lot-lots if they secured the fill.

Szczap states that if the fill is secured, they would eliminate the two-family units in favor of single-family lots.

Chairman **Stoner** is concerned about zero-lot-line and issues with neighbors maintaining their properties to different standards.

Szczap states that the management documents would be similar to a condominium.

Chairman **Stoner** mentions concerns about the fill process and that the size of the lots for single family homes would have to change.

Chairman **Thompson** mentions being in favor of the medium density single-family project but is concern about the timing of the fill as it relates to weather.

Commissioner **Boxx** asks whether the management documents from a similar site could be provided to the commission for review.

Szczap states that that document is owned by a third party and would need approval before sharing it.

Commissioner **Steege** makes a motion to approve the request.

Commissioner **Chambers** seconds the motion.

Motion carried. 6-0.

Passes unanimously

9. **Rezoning w/ PUD:**

Public Hearing and Action on Request by: Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St.,

Kenosha, WI 53142 (Agent); requesting approval of a rezoning from R-9 Multiple-Family Residential Dist., C-1 Lowland Resource Conservancy Dist., and A-2 General Agricultural Dist. to R-9 Multiple-Family Residential Dist., R-5 Urban Single-Family Residential Dist., and C-1 Lowland Resource Conservancy Dist., and PUD Planned Unit Development Overlay, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **DeLuca** makes a motion to table the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

10. Conditional Use Permit:

Public Hearing and Action on Request by: Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requesting a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments:

Tom Terletzky – 9015 12th Street

Mr. **Terletzky** mentions his support for the school's expansion. He raises concerns about the extension of the water to the property from 100th Avenue. That resulted in the homeowners paying a large amount of the cost to the benefit of a proposed subdivision that would use a lot more of that water. **Terletzky** objects to the high cost to homeowners along 12th Street. He mentions that the Village has the authority to decide how the cost of the pipe would be charged to the benefiting properties.

Terletzky proposes a 20/30/50 cost split as follows: 20 percent would be paid by the school, 50 percent would be paid by the proposed subdivision and the remaining would be paid by the residents along Highway E according to frontage. The subdivision would pay back the assessment when the lots are sold. He mentions that this solution would speed up repayment.

Chairman **Stoner** mentions that they have no control over developers and mentions the additional infrastructure cost incurred to develop a subdivision. He states that other communities also assess property owner for utilities going by their properties. He also disputes the benefits

Terletzky mentions that homeowners have been subsidizing growth in their community

by paying for the infrastructure and that it is not fair. He mentions that residents are not notified of upcoming assessments until it is too late.

Chairman **Stoner** asks what the public notification process is.

Administrator **Peters** explains the process and what would trigger notification.

Terletzky mentions that by the time homeowners are notified it is too late for residents to object. He mentions that by that time the project has been approved and bids are under way. He reiterates that having residents pay for 70 percent of the cost is wrong.

Commissioner **Boxx** makes a motion to approve the request.
Commissioner **Steege** seconds the motion.

Chairman **Stoner** is concerned that the stormwater part of the project has not started. He also mentions that he will not support the school expansion without connection to municipal water in part due to fire protection. He mentions his support for the overall projects but also mentions that future athletic fields would need to be serviced by municipal water for bathrooms and any concession stands.

Commissioner **Steege** mentions his concerns with the stormwater plan, the traffic analysis and appropriate parking.

Excel Engineer Jeremy **Wagner** mentions that they are coordinating with Kenosha County about the stormwater project. He mentions that they just received the traffic analysis and shows only one additional car stacking at the four-way intersection. As to the parking, he states that the project will add 250 stalls.

Commissioner **Steege** asks whether the parking calculation accounts for gymnasium activities.

Wagner states that it does.

Chairman **Stoner** asks whether they are designing the water system for the project.

Wagner states that they are doing the plumbing but not the water main coming to the property. He states that the Village Engineer will design that.

Engineer Brett **Biwer** mentions that the design is very preliminary so far.

9100 11th Street

Fire Chief **Andersen** whether the State has reviewed the fire suppression system.

Chief **Andersen** states that it would not happen until the project is approved. He states that while the State would not force the property owner to connect to the water but the local ordinance would within six months. He mentions that it would be more cost effective to

plan for municipal water connection at the time of the expansion.

Shoreland Lutheran High School President Paul **Scriver** mentions that they are waiting for a final decision about municipal water connection. He states that preliminary engineering states they do not need to connect. Once the school hears that the water connection is necessary, they will design the project accordingly. He states that the school is not opposed to connection to municipal water.

Commissioner **Thompson** asks about the process to design and bidding for the water line going down 12th Street.

Administrator **Peters** explained that the Village engineer would start the design once the project is approved.

Commissioner **Thompson** asks if the design process would start before a decision was made about how the project would be paid for. He also asks whether the subdivision project is moving forward.

Administrator **Peters** answers that there has been no contact with the subdivision developer for the past two years.

Commissioner **Steege** mentions that the commission is not responsible for deciding how the project is financed, only approving the plans. The question of the water connection will be up to the Village Board.

Guy Hawley - 9100 11th Street

Mr. **Hawley** mentions that the construction of his house is delayed until the school's stormwater system is in place.

Chairman **Stoner** interjects that this issue has nothing to do with the school's project. He is aware of the situation and offers to speak with Hawley after the meeting.

Commissioner **Boxx** makes a motion to approve the conditional use permit.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

11. Site Plan Review:

Discussion and Action Request by: Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requests site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.
Commissioner **Steege** seconds the motion.

Commissioner **Steege** asks details about the rooftop AC units.

Excel Engineer Alex **Fiebig** answers the questions and states that the number of unit has been reduced and features a two feet parapet.

Motion carried. 6-0.

Passes unanimously

12. Adjourn

Commissioner **Boxx** makes a motion to adjourn.
Commissioner **Steege** seconds the motion.
Motion carried 6-0 vote.

Passes unanimously

Drafted Oct. 16, 2024.

These minutes are not official until approved by the Plan Commission. Submitted by Assistant to the Administrator/Communication Specialist Kevin Poirier



VILLAGE OF SOMERS

Department of Planning & Development

VILLAGE OF SOMERS CONDITIONAL USE PERMIT PROCEDURES

- 1. Contact the Kenosha County Department of Planning & Development and check with staff to determine if your proposed use is a permitted use, an accessory use or a conditional use. If it is a conditional use, then a conditional use permit must be applied for and received prior to occupying or using the site for that use.

- 2. Contact the Kenosha County Department of Planning & Development and schedule a pre-conference meeting, which is required for all conditional use permit requests.

Meeting Date: 7/18/24

- 3. Contact the Village of Somers to determine if your conditional use permit application requires requires concept review by the Village Plan Commission. If so, contact the Village of Somers clerk to schedule a concept meeting with the Village Plan Commission.

Meeting Date: 7/18/24

- 4. Complete and submit to the Kenosha County Department of Planning & Development the Village of Somers Conditional Use Permit Application by the filing deadline.

Filing Deadline: 8/30/24

- 5. Upon submission your will be given two copies of the date-stamped application. Submit a copy of the date-stamped application to the Village of Somers clerk for placement on the agendas of the Village of Somers Plan Commission and the Village of Somers Board. Keep the other copy for your records.

- 6. Attend the Village Plan Commission and the Village Board meetings. **NOTE:** You must attend or the Village will not be able to act on your request. At these meeting you will be asked to brief the committee on your request and ultimately agree to a list of conditions of approval relating to your proposed use.

Village Plan Commission meeting date (tentative): 10/14/24

Village Board meeting date (tentative): 10/15, 11/6 & 11/12

- 7. Village clerk will provide written notice of final action to property owner/applicant.

- 8. If approved, you may commence with applying for and obtaining any necessary zoning permit(s) for construction (i.e. new buildings, building additions, signage, fencing, etc...) with the Kenosha County Department of Planning and Development.

IMPORTANT TELEPHONE NUMBERS

Kenosha County Center
 Department of Public Works & Development Services
 19600 - 75th Street, Suite 185-3
 Bristol, Wisconsin 53104-9772

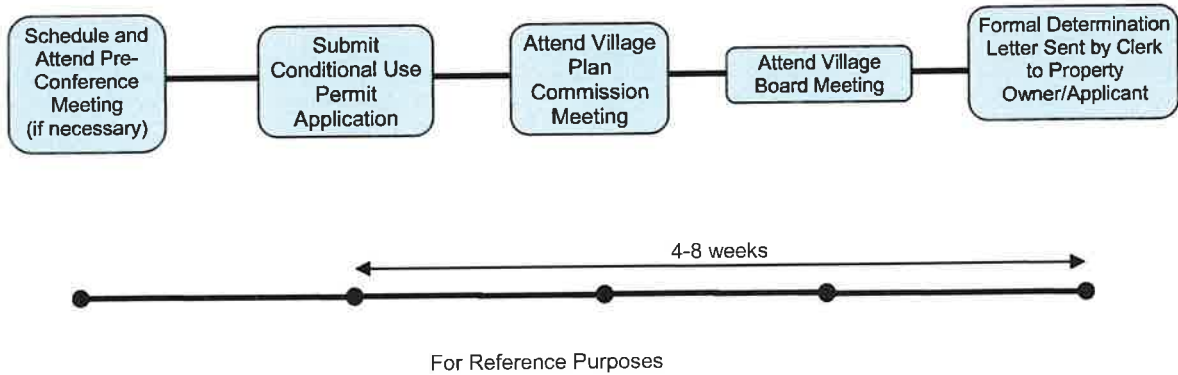
Division of Planning & Development (including Sanitation & Land Conservation)..... **857-1895**
 Facsimile #..... 857-1920

Public Works Division of Highways 857-1870

Administration Building
 Division of Land Information..... 653-2622

Village of Somers 859-2822
 Wisconsin Department of Natural Resources - Sturtevant Office 884-2300
 Wisconsin Department of Transportation - Waukesha Office 548-8722

Conditional Use Permit Timeline





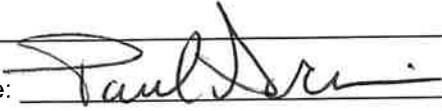
VILLAGE OF SOMERS

Department of Planning & Development

CONDITIONAL USE PERMIT APPLICATION

(a) Property Owner's Name:

Shoreland Lutheran High School Federation, Inc

Print Name: Paul Scriver Signature: 


Mailing Address: 9206 21st Street

City: Kenosha State: WI Zip: 53144

Phone Number: 262-515-4527 E-mail (optional): scriver@slhs.com

Note: Unless the property owner's signature can be obtained in the above space, a letter of agent status signed by the legal property owner must be submitted if you are a tenant, leaseholder, or authorized agent representing the legal owner, allowing you to act on their behalf.

(b) Agent's Name (if applicable):

Print Name: Tom Schermerhorn Signature: 


Business Name: Excel Engineering Inc

Mailing Address: 100 Camelot Dr

City: Fond du Lac State: WI Zip: 54935

Phone Number: 920-322-1751 E-mail (optional): tom.s@ExcelEngineer.com

(c) Architect's Name (if applicable):

Print Name: Tom Schermerhorn Signature: 


Business Name: Excel Engineering Inc

Mailing Address: 100 Camelot Dr

City: Fond du Lac State: WI Zip: 54935

Phone Number: 920-322-1751 E-mail (optional): tom.s@ExcelEngineer.com

(d) Engineer's Name (if applicable):

Print Name: Ben Warntjes Signature: 

Business Name: Excel Engineering Inc

Mailing Address: 100 Camelot Dr

City: Fond du Lac State: WI Zip: 54935

Phone Number: 920-322-1575 E-mail (optional): ben.w@excelengineer.com

(e) Tax key number(s) of subject site:

82-4-222-084-0272

Address of the subject site:

9026 12th St

(f) Plan of Operation (or attach separate plan of operation)

Type of structure:

School building

Proposed operation or use of the structure or site:

Gym and classroom addition on the east side of the building, choir room addition to the southwest corner, parking lot modifications and parking lot expansion to the east. CUP is required for the gym.

Number of employees (by shift): 50 existing, 10 proposed, 1 shift

Hours of Operation: 7:45 AM - 3:00 PM

Any outdoor entertainment? If so, please explain: Athletic Fields

Any outdoor storage? If so, please explain: None

Zoning district of the property: I-1 Institutional

(g) Attach a plat of survey prepared by a professional land surveyor or site plan drawn to scale and approved by the Kenosha County Department of Planning & Development showing all of the information required under section 12.05-1(h)3 for a zoning permit. In addition, the plat of survey or site plan layout shall show the location, elevation and use of any abutting lands and the location and foundation elevations of structures within 50 feet of the subject site; soil mapping unit lines; ordinary high water mark, historic high water marks and floodlands on or within 50 feet of the subject premises, existing and proposed gravel, asphalt and concrete surfaces, existing and proposed signage, existing and proposed buildings, and existing and proposed landscaping.

CONDITIONAL USE PERMIT APPLICATION

For conditional use permit applications that are made within shoreland and floodland areas, such description shall also include information that is necessary for the Village Plan Commission to determine whether the proposed development will hamper flood flows, impair floodplain storage capacity, or cause danger to human, animal or aquatic life. This additional information may include plans, certified by a registered professional engineer or professional surveyor, showing existing and proposed elevations or contours of the ground; fill or storage elevation; basement and first floor elevations of structures; size, location, and spatial arrangement of all existing and proposed structures on the site; location and elevation of streets water supply and sanitary facilities; aerial photographs, and photographs showing existing surrounding land uses and vegetation upstream and downstream; soil types and any other pertinent information required by either the Village of Somers Planning & Development office, Village Plan Commission or Village Board:

(h) The Kenosha County Department of Planning & Development may ask for additional information.

(i) The fee specified in Section 12.05-8 of this ordinance.

Request for Conditional Use Permit (payable to "Kenosha County") \$1,350.00

(For other fees see the [Fee Schedule](#))

ATTACH BUSINESS
SUMMARY HERE

ATTACH TO-SCALE MAP OF
PROPERTY SHOWING SITE
PLAN OF OPERATION HERE



August 30, 2024

Site Plan Review & CUP Application - Project Narrative

Project: Shoreland Lutheran Addition – Phase 1
9026 12th Street
Kenosha, WI 53144

Shoreland Lutheran High School Federation, Inc. is requesting a site plan review and conditional use permit approval for building and parking lot additions to the existing school campus located at 9026 12th Street in the Village of Somers. The property is zoned I-1 (Institutional) and the underlying private school use is considered permitted under the I-1 zoning district. However, the building additions in this proposed phase will include new classrooms in conjunction with a new gymnasium/fitness center and a remodeling of the existing gymnasium. In the I-1 zoning district gymnasiums are considered conditional uses. Therefore, a conditional use permit is required.

As a part of the parking lot addition scope of work, due to the anticipated additional traffic on the eastern side of the site, a traffic impact analysis (TIA) is being completed. Based on the TIA recommendations the existing drive onto 88th Avenue could be demolished, and a new drive entrance would be constructed onto 88th Avenue.

Stormwater for the overall school campus has been designed and is currently in the review process with the Village. The stormwater will be constructed concurrently with the building addition. The existing well onsite will be used to supply water to the proposed addition. A new sanitary lateral will be installed out of the addition. The sanitary lateral will connect into the existing private interceptor main.

As a part of the conditional use permit for the proposed gymnasium, the development is requesting to proceed with the following exceptions from the Village of Somers Zoning Ordinance:

Section ZN3.06(3)(j) – Parking Requirements

- Non-Residential Use Pavement Front Yard Setback
 - The Traffic, Parking, and Access section of the City Zoning Ordinance requires that all non-residential parking spaces and driveways to be a minimum of 20 feet from the established right-of-way. However, due to the size constraints of the existing institutional facility, an attempt to match the existing parking lot setback is proposed for the additional parking area in the front of the site at approximately 7 feet from the right-of-way line (encroaching approximately 13 feet into the required pavement setback area; see attached site plan indicating this setback area proposed). This exception is required to provide adequate and safe vehicle and pedestrian circulation areas in front of the existing building at the main entrance of the facility. When the existing parking lot was constructed the parking lot met setback requirements. Due to changes in setback requirements and a 10' strip of the shoreland property being given to the county to expand the ROW of

12th Street the existing parking does not comply with current setbacks. The proposed parking expansion at the front of the existing facility cannot be shifted to meet this pavement setback as it would impact the number of parking stall spaces for the proposed building expansion as well as impact the onsite maneuver area for traffic circulation through the front of the existing site.

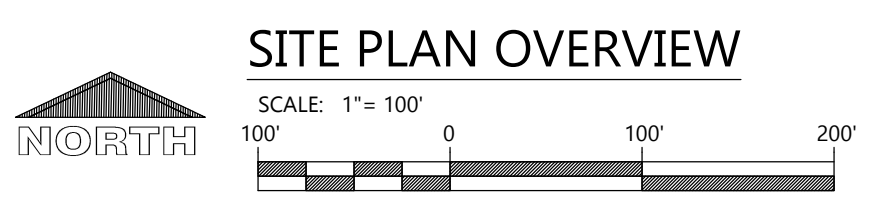
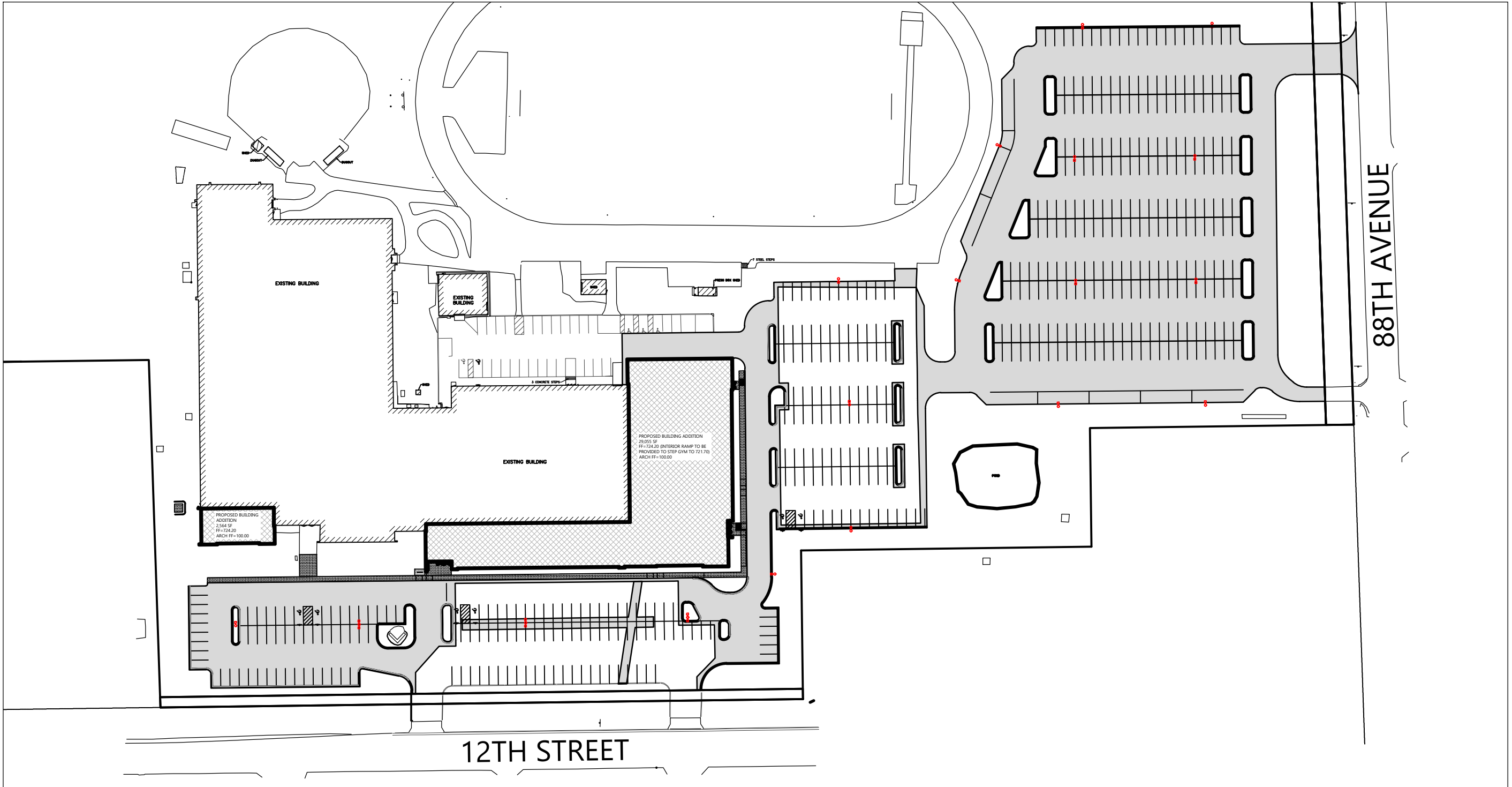
PROPOSED BUILDING ADDITION PHASE ONE FOR: SHORELAND LUTHERAN HIGH SCHOOL

KENOSHA, WI

PROJECT INFORMATION

SITE INFORMATION:			
PROPERTY AREA: LOT 1 OF PROPOSED CSM	2,427,904 S.F. (55.74 ACRES)		
EXISTING ZONING: I-1 (INSTITUTIONAL)			
PROPOSED ZONING: I-1 (INSTITUTIONAL)			
PROPOSED USE: PHASE 1 - EXISTING SCHOOL BLDG. ADDITION & PARKING LOT EXPANSION			
AREA OF SITE DISTURBANCE: 5.40 ACRES			
EXISTING SITE DATA			
BUILDING FLOOR AREA	2.14	93,153	3.8%
PAVEMENT (ASP., CONC. & GRAVEL)	4.77	207,664	8.6%
TOTAL IMPERVIOUS	6.91	300,817	12.4%
LANDSCAPE/ OPEN SPACE	48.83	2,127,087	87.6%
PROJECT SITE	55.74	2,427,904	100.0%
PROPOSED SITE DATA			
BUILDING FLOOR AREA	2.80	122,121	5.0%
PAVEMENT (ASP., CONC. & GRAVEL)	6.70	291,777	12.0%
TOTAL IMPERVIOUS	9.50	413,898	17.0%
LANDSCAPE/ OPEN SPACE	46.24	2,014,006	83.0%
PROJECT SITE	55.74	2,427,904	100.0%

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN
CALL DIGGERS HOTLINE
1-800-242-8511
TOLL FREE TELEFAX (414) 259-0947
TDD (FOR THE HEARING IMPAIRED)
1-800-542-2289
WISCONSIN STATUTE 182.0175 (1974)
REQUIRES MINIMUM OF 3 WORK DAYS
NOTICE BEFORE YOU EXCAVATE



LEGEND

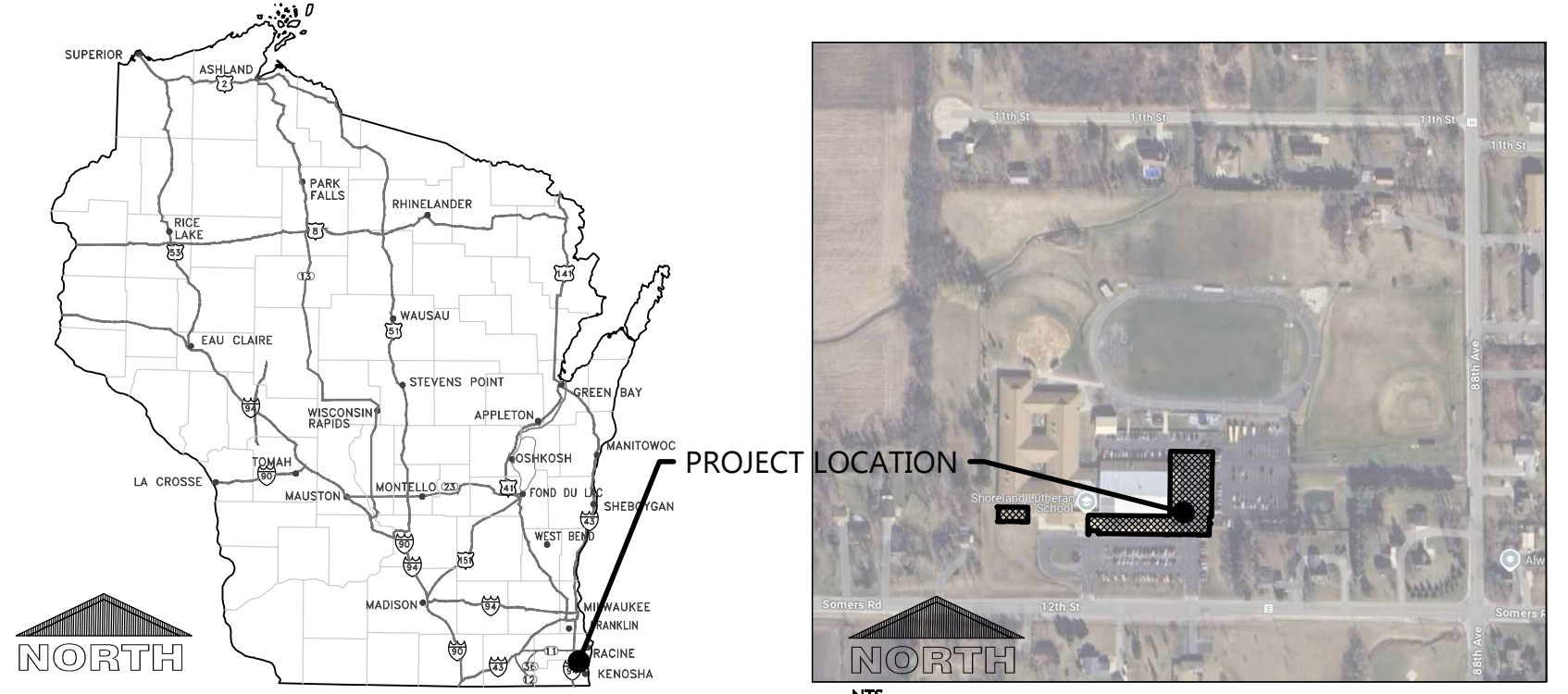
NOTE: ALL SYMBOLS SHOWN MAY NOT APPEAR ON DRAWINGS.

SYM.	IDENTIFICATION	SYM.	IDENTIFICATION
0000.00	PROPOSED SPOT ELEVATIONS (FLOW LINE OF CURB UNLESS OTHERWISE SPECIFIED)	0000.00C	PROPOSED SPOT ELEVATIONS (TOP OF CURB, FLOWLINE OF CURB)
0000.00G	EXISTING GRADE SPOT ELEVATIONS	0000.00FL	PROPOSED SPOT ELEVATIONS (TOP OF WALK, BOTTOM OF WALK @ FLOWLINE)
0000.00BG	PROPOSED SPOT ELEVATIONS (REFERENCE R-WALL DETAIL) BG-FINISHED SURFACE GRADE AT BACK OF WALL	0000.00TW	PROPOSED SPOT ELEVATIONS (TOP OF WALK, BOTTOM OF WALK @ FLOWLINE)
0000.00FG	PROPOSED SPOT ELEVATIONS (REFERENCE R-WALL DETAIL) FG-FINISHED SURFACE GRADE AT FRONT OF WALL		
EXISTING SITE SYMBOLS			
EXISTING SIGN		EXISTING UTILITY POLE	
EXISTING HANDICAP PARKING STALL		EXISTING UTILITY POLE WITH GUY WIRE	
EXISTING WATER VALVE IN BOX		EXISTING STREET LIGHT	
EXISTING WATER VALVE IN MANHOLE		EXISTING TELEPHONE PEDESTAL	
EXISTING WATER SERVICE VALVE		EXISTING ELECTRIC PEDESTAL	
EXISTING WELL		EXISTING ELECTRIC BOX	
EXISTING STORM CATCH BASIN		EXISTING FLOOD LIGHT	
EXISTING STORM CURB INLET		EXISTING TELEPHONE MANHOLE	
EXISTING SQUARE CATCH BASIN		EXISTING CABLE TV PEDESTAL	
EXISTING LIGHT POLE		EXISTING GAS VALVE	
1-1/4" REBAR SET WEIGHING 4.30 LB/FT.		EXISTING HEDGE	
3/4" REBAR SET WEIGHING 1.50 LB/FT.		EXISTING WOODED AREA	
1-1/4" REBAR FOUND		EXISTING MARSH AREA	
3/4" REBAR FOUND		EXISTING DECIDUOUS TREE WITH TRUNK DIAMETER	
2" IRON PIPE FOUND		EXISTING CONIFEROUS TREE	
1" IRON PIPE FOUND		EXISTING SHRUB	
SECTION CORNER		EXISTING STUMP	
PROPOSED SITE SYMBOLS			
PROPOSED SIGN		PROPOSED STORM FIELD INLET - ST FI	
PROPOSED HANDICAP PARKING STALL		PROPOSED LIGHT POLE	
PROPOSED WATER VALVE IN BOX		PROPOSED DRAINAGE FLOW	
PROPOSED WATER VALVE IN MANHOLE		PROPOSED APRON END SECTION	
PROPOSED WATER SERVICE VALVE		SOIL BORING	
PROPOSED WELL		CENTER LINE	
PROPOSED STORM CATCH BASIN - ST CB		PROPOSED CLEANOUT	
PROPOSED STORM CURB INLET - ST CI		PROPOSED DOWNSPOUT TO GRADE	
		PROPOSED DOWNSPOUT TO RISER	
EXISTING LINETYPES			
EXISTING CHAINLINK FENCE		POL	EXISTING POLISH SEWER AND MANHOLE
EXISTING WOOD FENCE		P	EXISTING PROCESS SEWER AND MANHOLE
EXISTING BARBED WIRE FENCE		CLW	EXISTING CLEAR WATER LINE
EXISTING CURB AND GUTTER		FO	EXISTING UNDERGROUND FIBER OPTIC LINE
EXISTING GUARD RAIL		E	EXISTING UNDERGROUND ELECTRIC CABLE
EXISTING GROUND CONTOUR		T	EXISTING UNDERGROUND TELEPHONE CABLE
EXISTING STORM SEWER AND MANHOLE		G	EXISTING UNDERGROUND GAS LINE
EXISTING SANITARY SEWER AND MANHOLE		OU	EXISTING OVERHEAD UTILITY LINE
EXISTING WATER LINE AND HYDRANT			RAILROAD TRACKS
INTERIOR PROPERTY LINE			RIGHT-OF-WAY LINE
PROPOSED LINETYPES			
PROPOSED CHAINLINK FENCE		POL	PROPOSED POLISH SEWER AND MANHOLE
PROPOSED WOOD FENCE		P	PROPOSED PROCESS SEWER AND MANHOLE
PROPOSED BARBED WIRE FENCE		CLW	PROPOSED CLEAR WATER LINE
PROPOSED CURB AND GUTTER		FO	PROPOSED UNDERGROUND FIBER OPTIC LINE
PROPOSED GUARD RAIL		E	PROPOSED UNDERGROUND ELECTRIC CABLE
PROPOSED GROUND CONTOUR		T	PROPOSED UNDERGROUND GAS LINE
PROPOSED STORM SEWER AND MANHOLE - ST MH		G	PROPOSED UNDERGROUND TELEPHONE CABLE
PROPOSED SANITARY SEWER AND MANHOLE - SAN MH		OU	PROPOSED OVERHEAD UTILITY LINE
PROPOSED WATER LINE AND HYDRANT			MATCHLINE
PROPOSED PROPERTY LINE			GRADING/SEEDING LIMITS

PROJECT CONTACTS

OWNER INFORMATION: SHORELAND LUTHERAN HIGH SCHOOL ATTN: PAUL SCRIVER 9026 12TH STREET KENOSHA, WI 53144 Phone: (262) 515-4527 Email: scriver@slhs.us	CIVIL: BEN WARNTJES Phone: (920) 322-1575 E-mail: ben.warntjes@excelengineer.com	VILLAGE ADMINISTRATOR: JASON PETERS Phone: (262) 859-2822 E-mail: jpeters@somers.org	CITY ENGINEER: BAXTER & WOODMAN CONSULTING ENGINEERS DOUG SNYDER Phone: (815) 444-3349 E-mail: dsnyder@baxterwoodman.com	CITY FIRE CHIEF: BEN ANDERSEN Phone: (262) 859-2277 E-mail: bandersen@somers.org	CITY BUILDING INSPECTOR: SCOTT SEYMOUR Phone: (262) 859-2822 E-mail: sseymour@somers.org	CITY DIRECTOR OF PUBLIC WORKS: ANDREW KREYE Phone: (262) 859-2822 E-mail: akreye@somers.org
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LOCATION MAP



PROJECT NOTES

- GENERAL PROJECT NOTES**
- ALL DRIVEWAYS AND CURB CUTS TO BE CONSTRUCTED ACCORDING TO LOCAL ORDINANCES. CONTRACTOR TO OBTAIN ALL NECESSARY PERMITS.
 - THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL WORK IN ROW PERMITS.

CONSTRUCTION STAKING SERVICES

CONSTRUCTION STAKING SHALL BE COMPLETED BY EXCEL ENGINEERING AS REQUESTED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. CONTRACTOR TO CONTACT RYAN WILGREEN AT 820-936-9800 OR RYAN.WILGREEN@EXCELENGINEER.COM TO GET STAKING PRICES TO INCLUDE IN BID TO OWNER. PAYMENT OF STAKING COSTS ABOVE AND BEYOND THE BASE PRICE DUE TO RESTAKING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR, NOT THE OWNER. CAD DRAWING FILES AND SURVEY CONTROL WILL NOT BE PROVIDED FOR STAKING PURPOSES.

SHEET INDEX

SHEETS BELOW INTENDED TO BE PRINTED IN COLOR. REFER TO DIGITAL FORMAT DRAWINGS IF PRINTED GRAYSCALE TO ENSURE SCOPE CLARITY.

NUMBER	SHEET NAME / DESCRIPTION
C0.1	CIVIL COVER SHEET
C0.2	CIVIL SPECIFICATIONS
C1.0	EXISTING SITE AND DEMOLITION PLAN
C1.1	SITE PLAN
C1.2	GRADING AND EROSION CONTROL PLAN
C1.3	UTILITY PLAN
C2.0	DETAILS
C3.1	SITE PHOTOMETRIC PLAN & DETAILS
L1.0	OVERALL LANDSCAPE PLAN
L1.1	ENLARGED LANDSCAPE PLAN
L1.2	ENLARGED LANDSCAPE PLAN
L1.3	LANDSCAPE DETAILS, NOTES & SCHEDULES

PROPOSED BUILDING ADDITION PHASE ONE FOR:
SHORELAND LUTHERAN HIGH SCHOOL
 9026 12TH STREET • KENOSHA, WI 53144



PRELIMINARY DATES

AUG. 30, 2024

NOT FOR CONSTRUCTION

JOB NUMBER

240018700

SHEET NUMBER

C0.1

CIVIL SPECIFICATIONS

DIVISION 31 EARTH WORK

31 10 00 SITE CLEARING (DEMOLITION)

- A. CONTRACTOR SHALL CALL DIGGER'S HOT LINE AND CONDUCT A PRIVATE UTILITY LOCATE AS REQUIRED TO ENSURE THAT ALL UTILITIES HAVE BEEN LOCATED BEFORE STARTING SITE DEMOLITION. DESIGN ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCIES BETWEEN FIELD CONDITIONS PRIOR TO CONSTRUCTION.
- B. CONTRACTOR TO FIELD TELEVISION EXISTING SANITARY AND STORM LATERALS THAT ARE SCHEDULED TO BE RE-USED AND/OR CONNECTED TO ON SITE AT TIME OF DEMOLITION. THE TELEVISION SHALL BE COMPLETED TO ENSURE THE EXISTING LATERALS ARE FREE OF OBSTRUCTIONS AND IN SOUND STRUCTURAL CONDITION. TELEVISIONS OF THESE LATERALS SHALL BE COMPLETED AT BEGINNING OF CONSTRUCTION AND DESIGN ENGINEER SHALL BE NOTIFIED OF ANY PIPE OBSTRUCTIONS AND/OR STRUCTURAL DEFICIENCIES IMMEDIATELY AFTER COMPLETION OF FIELD TELEVISION.
- C. DEMOLITION PLAN IS AN OVERVIEW OF DEMOLITION TO TAKE PLACE ON SITE. CONTRACTOR TO FIELD VERIFY EXISTING SITE CONDITIONS PRIOR TO BEGINNING. CONTRACTOR SHALL REMOVE, RE-PLACE OR DEMOLISH ALL ITEMS AS NEEDED DURING CONSTRUCTION.
- D. CONTRACTOR TO PROTECT EXISTING IMPROVEMENTS THAT ARE SCHEDULED TO REMAIN. ANY DAMAGE TO EXISTING FACILITIES SHALL BE REPAIRED AT CONTRACTORS EXPENSE.
- E. ALL CONCERTE NOTED TO BE REMOVED SHALL BE REMOVED TO THE NEAREST CONTROL POINT.

31 20 00 EARTH MOVING

- A. CONTRACTOR SHALL CALL DIGGER'S HOT LINE AND CONDUCT A PRIVATE UTILITY LOCATE AS REQUIRED TO ENSURE THAT ALL UTILITIES HAVE BEEN LOCATED BEFORE STARTING EXCAVATION. DESIGN ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCIES BETWEEN FIELD CONDITIONS PRIOR TO CONSTRUCTION.
- B. PROVIDE LABOR, MATERIALS AND EQUIPMENT FOR ALL EXCAVATION, GRADING, FILL AND BACKFILL WORK AS REQUIRED TO COMPLETE THE GENERAL CONSTRUCTION WORK. ALL EXCAVATION AND BACKFILL FOR ELECTRICALS AND MECHANICALS ARE THE RESPONSIBILITY OF THE RESPECTIVE CONTRACTOR UNLESS OTHERWISE SPECIFIED IN THE BID DOCUMENTS.
- C. ALL ORGANIC TOPSOIL INSIDE THE BUILDING AREA, UNDER PAVED AREAS, AND AT SITE FILL AREAS SHALL BE REMOVED. PROOF ROLL SURFACES BEFORE PLACING FILL WITH HEAVY PNEUMATIC TIRED EQUIPMENT, SUCH AS A FULLY LOADED TANDUM AXLE DUMP TRUCK, TO IDENTIFY SOFT SPOTS AND AREAS OF EXCESS YIELDING. CONTRACTOR SHALL VERIFY TOPSOIL DEPTHS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REVIEW AND FOLLOW THE RECOMMENDATIONS OF THE GEOTECHNICAL REPORT AND ACCOUNT FOR EXISTING CONDITIONS PRIOR TO SUBMITTING BID FOR THE PROJECT. EXCESS MATERIALS SHALL BE REMOVED FROM THE SITE UNLESS OTHERWISE DIRECTED IN THE PLANS OR BY LOCAL ZONING REQUIREMENTS.
- D. PLACE AND COMPACT FILL MATERIAL IN LAYERS TO REQUIRED ELEVATIONS. UNIFORMLY MOISTEN OR REWET SURGRADE AND EACH SUBSEQUENT FILL OR BACKFILL LAYER BEFORE COMPACTION AS RECOMMENDED TO ACHIEVE SPECIFIED DRY DENSITY. REMOVE AND REPLACE, OR SCARIFY AND AIR DRY, OTHERWISE SATISFACTORY SOIL MATERIAL THAT IS TOO WET TO COMPACT TO SPECIFIED DRY DENSITY.
- E. PLACE BACKFILL AND FILL MATERIALS IN LAYERS NOT MORE THAN 8" IN LOOSE DEPTH FOR MATERIAL COMPACTED BY HEAVY COMPACTION EQUIPMENT, AND NOT MORE THAN 4" IN LOOSE DEPTH FOR MATERIAL COMPACTED BY HAND-OPERATED TAMPIERS.
- F. COMPACT THE SOIL TO NOT LESS THAN THE FOLLOWING PERCENTAGES OF MAXIMUM DRY DENSITY ACCORDING TO ASTM D 998, STANDARD PROCTOR TEST. FILL MAY NOT BE PLACED ON FROZEN GROUND AND NO FROZEN MATERIALS MAY BE USED FOR BACK FILL. APPLY THE MORE STRINGENT REQUIREMENTS WHEN COMPARING BETWEEN THE FOLLOWING AND THE GEOTECHNICAL REPORT.
1. UNDER FOUNDATIONS, SURGRADE, AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 98 PERCENT.
2. UNDER INTERIOR SLAB-ON-GRADE WHERE GROUNDWATER IS MORE THAN 3 FEET BELOW THE SLAB, PLACE A DRAINAGE COURSE LAYER OF 3/4" CRUSHED STONE WITH 5% TO 12% FINES PER THICKNESS INDICATED ON FOUNDATION PLANS ON PREPARED SURGRADE. COMPACT SURGRADE AND DRAINAGE COURSE TO NOT LESS THAN 95 PERCENT.
3. UNDER INTERIOR SLAB-ON-GRADE WHERE GROUNDWATER IS WITHIN 3 FEET OF THE SLAB SURFACE, PLACE A DRAINAGE COURSE LAYER OF CLEAN 3/4" CRUSHED STONE WITH NO MORE THAN 5% FINES PER THICKNESS INDICATED ON FOUNDATION PLANS ON PREPARED SURGRADE. COMPACT THE SURGRADE AND DRAINAGE COURSE TO NOT LESS THAN 95 PERCENT.
4. UNDER EXTERIOR CONCRETE AND ASPHALT PAVEMENTS - COMPACT THE SURGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 95 PERCENT.
5. UNDER WALKWAYS - COMPACT SURGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 95 PERCENT.
6. UNDER LAWNS OR UNPAVED AREAS - COMPACT SURGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 85 PERCENT.
- G. CONTRACTOR SHALL ENGAGE A QUALIFIED INDEPENDENT TESTING AND INSPECTING AGENCY TO PERFORM FIELD TESTS AND INSPECTIONS. CONTRACTOR SHALL PROVIDE DOCUMENTATION OF PAVING DENSITY TESTING AND PROOF-ROLLING TO ENGINEER UPON COMPLETION. IT IS SUGGESTED THAT THE GEOTECHNICAL FIRM USED TO PERFORM THE SUBSURFACE SOIL INVESTIGATION BE ENGAGED FOR THE FIELD QUALITY CONTROL TESTS. THE GEOTECHNICAL REPORT WAS PERFORMED BY NAME OF GEOTECHNICAL FIRM.
- H. ALLOW THE TESTING AGENCY TO TEST AND INSPECT SURGRADES AND EACH FILL OR BACKFILL LAYER. PROCEED WITH SUBSEQUENT EARTHWORK ONLY AFTER TEST RESULTS ARE PREVIOUSLY COMPLETED WORK COMPLY WITH REQUIREMENTS. PROVIDE ONE TEST FOR EVERY 2000 SQUARE FEET OF PAVED AREA OR BUILDING SLAB. ONE TEST FOR EACH SPREAD FOOTING, AND ONE TEST FOR EVERY 50 LINEAR FEET OF WALL STRIP FOOTING.
1. WHEN THE TESTING AGENCY REPORTS THAT SURGRADES, FILLS, OR BACKFILLS HAVE NOT ACHIEVED DEGREE OF COMPACTION SPECIFIED, SCARIFY AND MOISTEN OR REWET, OR REMOVE AND REPLACE SOIL TO DEPTH REQUIRED; RECOMPACT AND RETEST UNTIL SPECIFIED COMPACTION IS OBTAINED.
2. IF THE BUILDING SITE SHALL BE GRADED TO PROVIDE DRAINAGE AWAY FROM THE BUILDING AS INDICATED ON THE PLANS, SITE EARTHWORK SHALL BE GRADED TO WITHIN 0.10' OF REQUIRED EARTHWORK ELEVATIONS ASSUMING POSITIVE DRAINAGE IS MAINTAINED IN ACCORDANCE WITH THE GRADING PLAN.

31 30 00 EROSION CONTROL/STORMWATER MANAGEMENT

- A. THE EXCEL ENGINEERING DESIGN ENGINEER SHALL PREPARE A SITE SPECIFIC EROSION CONTROL AND A STORMWATER MANAGEMENT PLAN PURSUANT TO NR 216.45 AND NR 216.47. THE DESIGN ENGINEER SHALL ALSO FILE A CONSTRUCTION NOTICE OF INTENT WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES PURSUANT TO NR 216.45 AND TO AN AUTHORIZED LOCAL PROGRAM PURSUANT TO NR 216.415 TO OBTAIN COVERAGE UNDER THE GENERAL WIPDES STORM WATER PERMIT.
- B. THE CONTRACTOR SHALL KEEP THE NOTICE OF INTENT PERMIT, APPROVED EROSION CONTROL AND STORMWATER MANAGEMENT PLANS, AND PLAN AMENDMENTS ON THE CONSTRUCTION SITE AT ALL TIMES PURSUANT TO NR 216.455 UNTIL PERMIT COVERAGE IS TERMINATED.
- C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL LOCAL EROSION CONTROL PERMITS.
- D. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEETING THE MONITORING, MAINTENANCE, AND REPORTING REQUIREMENTS OF NR 216.48. INSPECTIONS OF IMPLEMENTED EROSION AND SEDIMENT CONTROL, BEST MANAGEMENT PRACTICES MUST AT A MINIMUM BE INSPECTED EVERY 7 CALENDAR DAYS OR EVERY 14 DAYS AND WITHIN 24 HOURS OF THE END OF A RAIN EVENT OF 0.5" OR MORE. A RAIN EVENT MAY BE CONSIDERED TO BE THE TOTAL AMOUNT OF RAIN RECORDED IN ANY CONTINUOUS 24-HOUR PERIOD. THE CONTRACTOR SHALL REPAIR OR REPLACE EROSION AND SEDIMENT CONTROL AS NECESSARY WITHIN 24 HOURS OF AN INSPECTION OR AFTER A DEPARTMENT NOTIFICATION WHERE REPAIR OR REPAIR IS REQUIRED.
- E. THE CONTRACTOR SHALL MAINTAIN, AT THE CONSTRUCTION SITE OR AVAILABLE VIA AN INTERNET WHERE, WEEKLY WRITTEN REPORTS OF ALL INSPECTIONS CONDUCTED. WISCONSIN DNR CONSTRUCTION SITE INSPECTION REPORT FORM 3400-187 SHALL BE USED. WEEKLY INSPECTION REPORTS SHALL INCLUDE ALL OF THE FOLLOWING:
1. THE DATE, TIME, AND LOCATION OF THE CONSTRUCTION SITE INSPECTION.
 2. THE NAME OF THE INDIVIDUAL WHO PERFORMED THE INSPECTION.
 3. AN ASSESSMENT OF THE CONDITION OF THE EROSION AND SEDIMENT CONTROL.
 4. A DESCRIPTION OF ANY EROSION AND SEDIMENT CONTROL, BEST MANAGEMENT PRACTICE IMPLEMENTATION AND MAINTENANCE PERFORMED.
 5. A DESCRIPTION OF THE PRESENT PHASE OF LAND DISTURBING CONSTRUCTION ACTIVITY AT THE CONSTRUCTION SITE.
- F. EROSION AND SEDIMENT CONTROL, IMPLEMENTED DURING CONSTRUCTION SHALL STRICTLY COMPLY WITH THE GUIDELINES AND REQUIREMENTS SET FORTH IN WISCONSIN ADMINISTRATIVE CODE (WAC) NR 151. THE STATE OF WISCONSIN DEPARTMENT OF NATURAL RESOURCES RUNOFF MANAGEMENT PERFORMANCE STANDARDS. TECHNICAL STANDARDS PUBLISHED BY THE WISCONSIN DNR SHALL ALSO BE UTILIZED TO IMPLEMENT THE REQUIRED PERFORMANCE STANDARDS. THE METHODS AND TYPES OF EROSION CONTROL WILL BE DEPENDENT ON THE LOCATION AND TYPE OF WORK INVOLVED. ALL SEDIMENT CONTROL MEASURES SHALL BE ADJUSTED TO MEET FIELD CONDITIONS AT THE TIME OF CONSTRUCTION, AND INSTALLED PRIOR TO CONSTRUCTION. CONSTRUCTION SHALL BE COMPACTED TO AN AVERAGE DENSITY PER WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- G. DESIGN AND CONSTRUCTION OF ALL CAST-IN-PLACE EXTERIOR CONCRETE FLAT WORK SHALL CONFORM TO AC 308R-08 & AC 318R-08.
- D. ALL EXTERIOR CONCRETE FLAT WORK SHALL BE PROVIDED PER MORE STRINGENT REQUIREMENTS OF THE GEOTECHNICAL REPORT OR THIS SPECIFICATION. CONCRETE FLAT WORK CONSTRUCTION IS AS FOLLOWS:
1. SEVERALS CONCRETE - 4" OF CONCRETE OVER 4" OF 3/4" CRUSHED AGGREGATE BASE.
 2. CONSTRUCTION JOINTS SHALL CONSIST OF 1/8" WIDE BY 1" DEEP TROUGGED JOINT WHERE DESIGN MIXES SHALL BE IN ACCORDANCE WITH ASTM C94
 3. STRENGTH TO BE MINIMUM OF 4000 PSI AT 28 DAYS FOR EXTERIOR CONCRETE.
 4. MAXIMUM CURING TIME SHALL BE 180 TO 240 HOURS.
 5. SLUMP SHALL NOT EXCEED 4" FOR EXTERIOR CONCRETE FLAT WORK
 6. SLUMP SHALL BE 2.5" OR LESS FOR CUR- FORMED CURB AND GUTTER.
 7. SLUMP SHALL BE BETWEEN 1.5" TO 3" FOR NON-SLIP FORMED CURB AND GUTTER.
 8. ALL EXTERIOR CONCRETE SHALL BE ENHANCED WITH 4% TO 7% AIR CONTENT. NO OTHER ADMIXTURES SHALL BE USED WITHOUT APPROVAL OF EXCEL ENGINEERING, INC. CALCIUM CHLORIDE SHALL NOT BE USED.
 9. MAXIMUM AGGREGATE SIZE FOR ALL EXTERIOR CONCRETE SHALL BE 0.75 INCHES.
 10. VERIFY EQUIPMENT CONCRETE PADS SIZES WITH CONTRACTOR REQUIRING PAD. PADS SHALL HAVE REINFORCE 300 FIBERS AT A RATE OF 15 LBS/CU YD, OR 8 X 6 W/1 X W/1 A WELDED WIRE MESH WITH MINIMUM 1 INCH COVER. EQUIPMENT PADS SHALL BE 3.5 INCHES THICK WITH 1 INCH CHAMFER UNLESS SPECIFIED OTHERWISE. COORDINATE ADDITIONAL PAD REQUIREMENTS WITH RESPECTIVE CONTRACTOR.
 11. ALL CONCRETE FLAT WORK SURFACES AND CONCRETE CURB FLOWLINES SHALL BE CONSTRUCTED TO WITHIN 0.05" OF DESIGN SURFACE AND FINISHING GRADES ASSUMING POSITIVE DRAINAGE IS MAINTAINED IN ACCORDANCE WITH THE DESIGN PLANS.
 12. CONCRETE FLAT WORK SHALL HAVE CONSTRUCTION JOINTS OR SAW CUT JOINTS PLACED AS INDICATED ON THE PLANS OR PER THIS SPECIFICATION. SAWCUTS SHALL BE DONE AS SOON AS POSSIBLE, BUT NO LATER THAN 24 HOURS AFTER CONCRETE IS PLACED. CONCRETE CURB AND GUTTER JOINTING SHALL BE PLACED EVERY 10' OR CLOSER IF NEEDED. IF CONCRETE PAVEMENT IS ADJACENT TO CONCRETE CURB, JOINTING IN THE PAVEMENT AND CURB SHALL ALIGN. ALL EXTERIOR CONCRETE SHALL HAVE A BROWN FINISH UNLESS NOTED OTHERWISE. A UNIFORM COAT OF A HIGH SOLIDS CURING COMPOUND MEETING ASTM C693 SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES. ALL CONCRETE IS TO BE CURED FOR 7 DAYS. EXTERIOR CONCRETE SHALL BE SEPARATED FROM BUILDING WITH CONTINUOUS 0.5 INCH FIBER EXPANSION JOINT AND 0.05 INCH FIBER EXPANSION JOINT AT DECORATIVE MASONRY UNITS.
 13. ALL REINFORCING BARS SHALL BE ASTM A618 GRADE 60. THICKNESS OF CONCRETE COVER OVER REINFORCEMENT SHALL BE NOT LESS THAN 3" WHERE CONCRETE IS DEPOSITED AGAINST THE GROUND WITHOUT THE USE OF FORMS AND NOT LESS THAN 1.5" FOR UP TO #5 BARS AND 2" FOR #6 TO #10 BARS IN ALL OTHER LOCATIONS. ALL REINFORCING SHALL BE LAPPED 48 DIAMETERS FOR UP TO #6 BARS, 62 DIAMETERS FOR #7 TO #9 BARS, 68 DIAMETERS FOR #10 BARS OR AS NOTED ON THE DRAWINGS AND EXTENDED AROUND CORNERS WITH CORNER BARS. PLACING AND DETAILING OF STEEL REINFORCING AND REINFORCING SUPPORTS SHALL BE IN ACCORDANCE WITH CRSI AND ACT MANUAL AND STANDARD PRACTICES. THE REINFORCEMENT SHALL NOT BE PAINTED AND MUST BE FREE OF GREASE, OIL, DIRT OR DEEP RUST WHEN PLACED IN THE WORK. ALL WELDED WIRE FABRIC SHALL MEET THE REQUIREMENTS OF ASTM A 1064. WELDED WIRE FABRIC SHALL BE PLACED 2" FROM TOP OF SLAB UNLESS INDICATED OTHERWISE.
 14. CONTRACTOR SHALL ENGAGE A QUALIFIED INDEPENDENT TESTING AND INSPECTING AGENCY TO SAMPLE MATERIALS, PERFORM TESTS, AND SUBMIT TEST REPORTS DURING CONCRETE PLACEMENT. TESTS WILL BE PERFORMED ACCORDING TO ACT 301. CAST AND LABORATORY CURS ONE SET OF FOUR STANDARD CYLINDERS FOR EACH COMPOSITE SAMPLE FOR EACH DAY'S POUR OF EACH CONCRETE MIX EXCEEDING 5 CU YD, BUT LESS THAN 25 CU YD, PLUS ONE SET FOR EACH ADDITIONAL 50 CU YD OR FRACTION THEREOF. PERFORM COMPRESSIVE STRENGTH TESTS ACCORDING TO ASTM C 39. TEST TWO SPECIMENS AT 7 DAYS AND TWO SPECIMENS AT 28 DAYS. PERFORM SLUMP TESTING ACCORDING TO ASTM C 143. PROVIDE ONE TEST AT POINT OF PLACEMENT FOR EACH COMPOSITE SAMPLE, BUT NOT LESS THAN ONE TEST FOR EACH DAY'S POUR OF EACH CONCRETE MIX. PERFORM ADDITIONAL TESTS WHEN CONCRETE CONSISTENCY APPEARS TO CHANGE.
 15. PROTECT FRESHLY PLACED CONCRETE FROM PREMATURE DRYING AND EXCESSIVE COLD OR HOT TEMPERATURES. IN HOT, DRY, AND WINDY WEATHER, APPLY AN EVAPORATION-CONTROL, COMPLIANT ACCORDING TO MANUFACTURER'S INSTRUCTIONS AFTER SCREENING AND BALL FLOATING, BUT BEFORE POWER FLOATING AND TROWELLING.
 16. LIMIT MAXIMUM WATER-CEMENTitious RATIO OF CONCRETE EXPOSED TO FREEZING, THAWING AND DEICING SALTS TO 0.45.
 17. TEST RESULTS WILL BE REPORTED IN WRITING TO THE DESIGN ENGINEER, READY MIX PRODUCER, AND CONTRACTOR WITHIN 56 HOURS AFTER TESTS. REPORTS OF COMPRESSIVE STRENGTH TESTS SHALL CONTAIN THE PROJECT IDENTIFICATION NAME AND NUMBER, DATE OF CONCRETE PLACEMENT, NAME OF CONCRETE TESTING SERVICE, CONCRETE TYPE AND CLASS, LOCATION OF CONCRETE BATCH ON SITE, DESIGN COMPRESSIVE BREAKING STRENGTH, AND TYPE OF BREAK FOR BOTH 7-DAY TESTS AND 28-DAY TESTS.
- H. EROSION CONTROL MEASURES SHALL BE REMOVED UNTIL THE AREAS SERVED HAVE ESTABLISHED VEGETATIVE COVER.
- I. ONCE THE CONSTRUCTION SITE HAS BEEN FULLY STABILIZED AND TEMPORARY EROSION CONTROL, BEST MANAGEMENT PRACTICES HAVE BEEN REMOVED, THE CONTRACTOR SHALL FILE A CONSTRUCTION NOTICE OF TERMINATION WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES IN ACCORDANCE WITH NR 216.55.
1. AT THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL GIVE THE OWNER COPIES OF THE EROSION CONTROL AND STORM WATER MANAGEMENT PLANS, AMENDMENTS TO PLANS, SUPPORTING PLAN DATA, AND CONSTRUCTION SITE EROSION CONTROL INSPECTION REPORTS. THE OWNER SHALL RETAIN THESE FOR A PERIOD OF 3 YEARS FROM THE DATE OF TERMINATING COVERAGE UNDER WIPDES GENERAL PERMIT.
- J. ALL POST CONSTRUCTION STORMWATER MANAGEMENT BEST MANAGEMENT PRACTICES SHALL BE CONSTRUCTED BEFORE THE SITE HAS UNDERGONE FINAL STABILIZATION.

DIVISION 32 EXTERIOR IMPROVEMENTS

32 10 00 AGGREGATE BASE & ASPHALT PAVEMENT

- A. CONTRACTOR TO PROVIDE COMPACTED AGGREGATE BASE AND HOT MIX ASPHALT PAVEMENT WHERE INDICATED ON THE PLANS. ALL AGGREGATE PROVIDED MUST COMPLY WITH SECTION 305 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. PROVIDE HOT MIX ASPHALT MIXTURE TYPES PER SECTION 460 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. CONTRACTOR SHALL OBTAIN AND REVIEW ALL LOCAL PERMITS AND RECOMMENDATIONS FOR GEO-GRID. GEOTEXTILE BELOW CRUSHED AGGREGATE (IF APPLICABLE). CONTRACTOR TO PROVIDE AGGREGATE BASE AND HOT MIX ASPHALT PAVEMENT TYPES AND AS NOTED BELOW:
1. STANDARD ASPHALT PAVING SECTION 1-1/2" SURFACE COURSE (S 11 38-283) 2" BINDER COURSE (4 17 38-285) (MSD) 455.2.5 TACK COAT (STAGED PAVING) 10' OF 1 1/4" CRUSHED AGGREGATE
- B. CONTRACTOR TO COMPACT THE AGGREGATE BASE, ASPHALT BINDER COURSE, AND ASPHALT SURFACE COURSE TO AN AVERAGE DENSITY PER WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. ALL ASPHALT PAVEMENT AREAS SHALL BE PAVED TO WITHIN 0.05" OF DESIGN SURFACE GRADES WITH POSITIVE DRAINAGE BEING MAINTAINED IN ACCORDANCE WITH DESIGN PLANS. A MINIMUM OF 15% DRY DENSITY SHALL BE MAINTAINED IN ALL ASPHALT PAVEMENT AREA.
- C. HOT MIX ASPHALT CONSTRUCTION TO BE PROVIDED PER MORE STRINGENT REQUIREMENTS OF THE GEOTECHNICAL REPORT OR THIS SPECIFICATION.
- D. CONTRACTOR TO PROVIDE 4" WIDE YELLOW PAINTED STRIPING FOR PARKING STALLS, TRAFFIC LANES, AND NO PARKING AREAS. YELLOW PAINT MARKINGS SHALL ALSO BE PROVIDED FOR H.C. ACCESSIBLE SYMBOLS, TRAFFIC ARROWS, AND TRAFFIC MESSAGES.

32 20 00 CONCRETE AND AGGREGATE BASE

- A. CONTRACTOR TO PROVIDE CRUSHED AGGREGATE BASE AND CONCRETE WHERE INDICATED ON THE PLANS.
- B. ALL AGGREGATE PROVIDED MUST COMPLY WITH SECTION 305 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION. ALL AGGREGATE PLACED MUST BE COMPACTED TO AN AVERAGE DENSITY PER WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- C. DESIGN AND CONSTRUCTION OF ALL CAST-IN-PLACE EXTERIOR CONCRETE FLAT WORK SHALL CONFORM TO AC 308R-08 & AC 318R-08.
- D. ALL EXTERIOR CONCRETE FLAT WORK SHALL BE PROVIDED PER MORE STRINGENT REQUIREMENTS OF THE GEOTECHNICAL REPORT OR THIS SPECIFICATION. CONCRETE FLAT WORK CONSTRUCTION IS AS FOLLOWS:
1. SEVERALS CONCRETE - 4" OF CONCRETE OVER 4" OF 3/4" CRUSHED AGGREGATE BASE.
 2. CONSTRUCTION JOINTS SHALL CONSIST OF 1/8" WIDE BY 1" DEEP TROUGGED JOINT WHERE DESIGN MIXES SHALL BE IN ACCORDANCE WITH ASTM C94
 3. STRENGTH TO BE MINIMUM OF 4000 PSI AT 28 DAYS FOR EXTERIOR CONCRETE.
 4. MAXIMUM CURING TIME SHALL BE 180 TO 240 HOURS.
 5. SLUMP SHALL NOT EXCEED 4" FOR EXTERIOR CONCRETE FLAT WORK
 6. SLUMP SHALL BE 2.5" OR LESS FOR CUR- FORMED CURB AND GUTTER.
 7. SLUMP SHALL BE BETWEEN 1.5" TO 3" FOR NON-SLIP FORMED CURB AND GUTTER.
 8. ALL EXTERIOR CONCRETE SHALL BE ENHANCED WITH 4% TO 7% AIR CONTENT. NO OTHER ADMIXTURES SHALL BE USED WITHOUT APPROVAL OF EXCEL ENGINEERING, INC. CALCIUM CHLORIDE SHALL NOT BE USED.
 9. MAXIMUM AGGREGATE SIZE FOR ALL EXTERIOR CONCRETE SHALL BE 0.75 INCHES.
 10. VERIFY EQUIPMENT CONCRETE PADS SIZES WITH CONTRACTOR REQUIRING PAD. PADS SHALL HAVE REINFORCE 300 FIBERS AT A RATE OF 15 LBS/CU YD, OR 8 X 6 W/1 X W/1 A WELDED WIRE MESH WITH MINIMUM 1 INCH COVER. EQUIPMENT PADS SHALL BE 3.5 INCHES THICK WITH 1 INCH CHAMFER UNLESS SPECIFIED OTHERWISE. COORDINATE ADDITIONAL PAD REQUIREMENTS WITH RESPECTIVE CONTRACTOR.
 11. ALL CONCRETE FLAT WORK SURFACES AND CONCRETE CURB FLOWLINES SHALL BE CONSTRUCTED TO WITHIN 0.05" OF DESIGN SURFACE AND FINISHING GRADES ASSUMING POSITIVE DRAINAGE IS MAINTAINED IN ACCORDANCE WITH THE DESIGN PLANS.
 12. CONCRETE FLAT WORK SHALL HAVE CONSTRUCTION JOINTS OR SAW CUT JOINTS PLACED AS INDICATED ON THE PLANS OR PER THIS SPECIFICATION. SAWCUTS SHALL BE DONE AS SOON AS POSSIBLE, BUT NO LATER THAN 24 HOURS AFTER CONCRETE IS PLACED. CONCRETE CURB AND GUTTER JOINTING SHALL BE PLACED EVERY 10' OR CLOSER IF NEEDED. IF CONCRETE PAVEMENT IS ADJACENT TO CONCRETE CURB, JOINTING IN THE PAVEMENT AND CURB SHALL ALIGN. ALL EXTERIOR CONCRETE SHALL HAVE A BROWN FINISH UNLESS NOTED OTHERWISE. A UNIFORM COAT OF A HIGH SOLIDS CURING COMPOUND MEETING ASTM C693 SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES. ALL CONCRETE IS TO BE CURED FOR 7 DAYS. EXTERIOR CONCRETE SHALL BE SEPARATED FROM BUILDING WITH CONTINUOUS 0.5 INCH FIBER EXPANSION JOINT AND 0.05 INCH FIBER EXPANSION JOINT AT DECORATIVE MASONRY UNITS.
 13. ALL REINFORCING BARS SHALL BE ASTM A618 GRADE 60. THICKNESS OF CONCRETE COVER OVER REINFORCEMENT SHALL BE NOT LESS THAN 3" WHERE CONCRETE IS DEPOSITED AGAINST THE GROUND WITHOUT THE USE OF FORMS AND NOT LESS THAN 1.5" FOR UP TO #5 BARS AND 2" FOR #6 TO #10 BARS IN ALL OTHER LOCATIONS. ALL REINFORCING SHALL BE LAPPED 48 DIAMETERS FOR UP TO #6 BARS, 62 DIAMETERS FOR #7 TO #9 BARS, 68 DIAMETERS FOR #10 BARS OR AS NOTED ON THE DRAWINGS AND EXTENDED AROUND CORNERS WITH CORNER BARS. PLACING AND DETAILING OF STEEL REINFORCING AND REINFORCING SUPPORTS SHALL BE IN ACCORDANCE WITH CRSI AND ACT MANUAL AND STANDARD PRACTICES. THE REINFORCEMENT SHALL NOT BE PAINTED AND MUST BE FREE OF GREASE, OIL, DIRT OR DEEP RUST WHEN PLACED IN THE WORK. ALL WELDED WIRE FABRIC SHALL MEET THE REQUIREMENTS OF ASTM A 1064. WELDED WIRE FABRIC SHALL BE PLACED 2" FROM TOP OF SLAB UNLESS INDICATED OTHERWISE.
 14. CONTRACTOR SHALL ENGAGE A QUALIFIED INDEPENDENT TESTING AND INSPECTING AGENCY TO SAMPLE MATERIALS, PERFORM TESTS, AND SUBMIT TEST REPORTS DURING CONCRETE PLACEMENT. TESTS WILL BE PERFORMED ACCORDING TO ACT 301. CAST AND LABORATORY CURS ONE SET OF FOUR STANDARD CYLINDERS FOR EACH COMPOSITE SAMPLE FOR EACH DAY'S POUR OF EACH CONCRETE MIX EXCEEDING 5 CU YD, BUT LESS THAN 25 CU YD, PLUS ONE SET FOR EACH ADDITIONAL 50 CU YD OR FRACTION THEREOF. PERFORM COMPRESSIVE STRENGTH TESTS ACCORDING TO ASTM C 39. TEST TWO SPECIMENS AT 7 DAYS AND TWO SPECIMENS AT 28 DAYS. PERFORM SLUMP TESTING ACCORDING TO ASTM C 143. PROVIDE ONE TEST AT POINT OF PLACEMENT FOR EACH COMPOSITE SAMPLE, BUT NOT LESS THAN ONE TEST FOR EACH DAY'S POUR OF EACH CONCRETE MIX. PERFORM ADDITIONAL TESTS WHEN CONCRETE CONSISTENCY APPEARS TO CHANGE.
 15. PROTECT FRESHLY PLACED CONCRETE FROM PREMATURE DRYING AND EXCESSIVE COLD OR HOT TEMPERATURES. IN HOT, DRY, AND WINDY WEATHER, APPLY AN EVAPORATION-CONTROL, COMPLIANT ACCORDING TO MANUFACTURER'S INSTRUCTIONS AFTER SCREENING AND BALL FLOATING, BUT BEFORE POWER FLOATING AND TROWELLING.
 16. LIMIT MAXIMUM WATER-CEMENTitious RATIO OF CONCRETE EXPOSED TO FREEZING, THAWING AND DEICING SALTS TO 0.45.
 17. TEST RESULTS WILL BE REPORTED IN WRITING TO THE DESIGN ENGINEER, READY MIX PRODUCER, AND CONTRACTOR WITHIN 56 HOURS AFTER TESTS. REPORTS OF COMPRESSIVE STRENGTH TESTS SHALL CONTAIN THE PROJECT IDENTIFICATION NAME AND NUMBER, DATE OF CONCRETE PLACEMENT, NAME OF CONCRETE TESTING SERVICE, CONCRETE TYPE AND CLASS, LOCATION OF CONCRETE BATCH ON SITE, DESIGN COMPRESSIVE BREAKING STRENGTH, AND TYPE OF BREAK FOR BOTH 7-DAY TESTS AND 28-DAY TESTS.

32 30 00 LANDSCAPING AND SITE STABILIZATION

- A. EROSION MATTING:
1. CONTRACTOR TO PROVIDE EROSION CONTROL MATTING (NORTH AMERICAN GREEN S150) OR EQUIVALENT ON ALL SLOPES THAT ARE 4:1 AND GREATER OUTSIDE OF STORMWATER CONVEYANCE CHANNELS AND STORMWATER MANAGEMENT BASINS. LAWN SEED SHALL BE PLACED BELOW MATTING IN ACCORDANCE WITH SEEDING REQUIREMENTS AND MANUFACTURER SPECIFICATIONS.
2. CONTRACTOR TO PROVIDE EROSION MATTING (NORTH AMERICAN GREEN C125) OR EQUIVALENT IN ALL SWALE BOTTOMS AND SIDE SLOPES AS WELL AS STORMWATER MANAGEMENT BASIN BOTTOMS. TOP AND SIDE SLOPES AS REQUIRED. LAWN SEED SHALL BE PLACED BELOW MATTING IN ACCORDANCE WITH SEEDING REQUIREMENTS AND MANUFACTURER SPECIFICATIONS.
- B. RIP RAP:
1. ALL RIP RAP ASSOCIATED WITH STORMWATER MANAGEMENT AND STORMWATER CONVEYANCE AS DELINEATED ON THE PLANS, SHALL BE CONSTRUCTED WITH THE TOP OF RIP RAP MATCHING THE PROPOSED ADJACENT GRADE ELEVATIONS. PLACEMENT OF RIP RAP ABOVE THE PROPOSED ADJACENT GRADE ELEVATIONS IS NOT ACCEPTABLE. ALL RIP RAP SHALL BE PLACED ON THE #16 FILTER FABRIC PER SECTION 640 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURAL CONSTRUCTION.

DIVISION 33 UTILITIES

33 10 00 SITE UTILITIES (DESIGN OF WATER, SANITARY, AND STORM BY EXCEL)

- A. CONTRACTOR TO FIELD VERIFY ALL EXISTING UNDERGROUND UTILITIES ON SITE. CONTRACTOR TO VERIFY PIPE LOCATIONS, SIZES, AND DEPTHS AT POINT OF PROPOSED CONNECTIONS AND VERIFY PROPOSED UTILITY ROUTES ARE CLEAR PER CODES OF ALL EXISTING UTILITIES AND OTHER OBSTRUCTIONS PRIOR TO CONSTRUCTION. COSTS INCURRED FOR FAILURE TO DO SO SHALL BE THE CONTRACTORS RESPONSIBILITY.
- B. CONTRACTOR TO FIELD TELEVISION ALL EXISTING SANITARY AND STORM LATERALS THAT ARE SCHEDULED TO BE RE-USED AND/OR CONNECTED TO ON SITE. THE TELEVISION SHALL BE COMPLETED TO ENSURE THE EXISTING LATERALS ARE FREE OF OBSTRUCTIONS AND IN SOUND STRUCTURAL CONDITION. TELEVISIONS OF THESE LATERALS SHOULD BE COMPLETED AT BEGINNING OF CONSTRUCTION AND DESIGN ENGINEER SHALL BE NOTIFIED OF ANY PIPE OBSTRUCTIONS AND/OR STRUCTURAL DEFICIENCIES IMMEDIATELY AFTER COMPLETION OF FIELD TELEVISION.
- C. ALL SANITARY PIPE SHALL BE IN ACCORDANCE WITH MATERIALS SPECIFIED IN TABLE A. ALLOWABLE PIPE MATERIAL SCHEDULE. ALL SANITARY PIPE BELOW PROPOSED 8 FUTURE BUILDINGS SHALL BE IN ACCORDANCE WITH MATERIALS SPECIFIED IN TABLE A. ALLOWABLE PIPE MATERIAL INSULATION SHALL BE PROVIDED PER STATE PLUMBING CODES AS NECESSARY BASED ON PROPOSED DEPTH PLANS.
- D. SANITARY SHALL BE 48" PRECAST AND CONFORM TO THE STANDARD SPECIFICATIONS FOR SEWER & WATER CONSTRUCTION PER WISCONSIN CURBENT WATER CONSTRUCTION OTHERWISE DIRECTED BY THE ENGINEER. SANITARY FRAME AND GRATE TO BE NEMA# 8-1550-A OR EQUAL. RIM ELEVATION TO BE SET AT FINISHED GRADE IN UNDEVELOPED AREAS AND 12" ABOVE FINISHED GRADE IN UNDEVELOPED AREAS EXCEPT AS OTHERWISE DIRECTED BY THE ENGINEER.
- E. CLEANOUTS SHALL BE PROVIDED FOR THE SANITARY & STORM SERVICES AT LOCATIONS INDICATED ON THE UTILITY PLAN. THE CLEANOUT SHALL CONSIST OF A COMBINATION WYE FITTING IN LINE WITH THE SANITARY/STORM SERVICE WITH THE CLEANOUT LEG OF THE COMBINATION WYE FACING STRAIGHT UP. THE CLEANOUT SHALL CONSIST OF 4" OR 6" VERTICAL PVC PIPE WITH A WATER TIGHT REMOVABLE CLEANOUT PLUG. AN 8" PVC FROST SLEEVE SHALL BE PROVIDED. THE BOTTOM OF THE FROST SLEEVE SHALL TERMINATE 12" ABOVE THE TOP OF THE SANITARY LATERAL OR AT LEAST 6" BELOW THE PREDICTED FROST DEPTH, WHICHEVER IS SHALLOWER. THE CLEANOUT SHALL EXTEND JUST ABOVE THE SURFACE GRADE IN LAWN OR LANDSCAPE AREAS WITH THE FROST SLEEVE TERMINATING AT THE GRADE SURFACE. THE CLEANOUT SHALL EXTEND 24 INCHES BELOW SURFACE GRADE IN PAVED SURFACES WITH A 2URM (2-1474-N) HEAVY DUTY CLEANOUT HOUSING PLACED OVER THE TOP OF THE CLEANOUT FLUSH WITH THE SURFACE GRADE. IN PAVED SURFACES, THE FROST SLEEVE SHALL TERMINATE IN A CONCRETE PAD AT LEAST 6" THICK AND EXTENDING AT LEAST 6" FROM THE SLEEVE ON ALL SIDES, SLOPING AWAY FROM THE SLEEVE. THE CLEANOUT HOUSING SHALL BE CONSTRUCTED PER MANUFACTURERS REQUIREMENTS.
- F. ALL PROPOSED WATER PIPE SHALL BE IN ACCORDANCE WITH MATERIALS SPECIFIED IN TABLE A. ALLOWABLE PIPE MATERIAL SCHEDULE. 6" MINIMUM COVER SHALL BE PROVIDED OVER ALL WATER PIPING UNLESS OTHERWISE SPECIFIED.
- G. ALL PROPOSED STORM PIPE SHALL BE IN ACCORDANCE WITH MATERIALS SPECIFIED IN TABLE A. ALLOWABLE PIPE MATERIAL SCHEDULE. ALL PROPOSED STORM PIPE BELOW BUILDINGS SHALL BE IN ACCORDANCE WITH MATERIALS SPECIFIED IN TABLE A. ALLOWABLE PIPE MATERIAL SCHEDULE. SEE UTILITY PLANS FOR ALL STORM PIPE MATERIAL TYPES TO BE USED. PIPE SHALL BE PLACED MIN. 8" HORIZONTAL FROM FOUNDATION WALLS.
- H. SANITARY, STORM, AND WATER UTILITY PIPE INVERTS SHALL BE CONSTRUCTED WITHIN 0.10' OF DESIGN INVERT ELEVATIONS ASSUMING PIPE SLOPE AND SEPARATION IS MAINTAINED PER THE UTILITY DESIGN PLANS AND STATE REQUIREMENTS.
- I. SITE UTILITY CONTRACTOR SHALL RUN SANITARY SERVICE TO A POINT WHICH IS A MAXIMUM OF 5' FROM THE EXTERIOR WALL OF THE FOUNDATION. SITE UTILITY CONTRACTOR SHALL RUN STORM SERVICE FOR INTERNALLY DRAINAGE BUILDINGS TO A POINT WHICH IS A MAXIMUM OF 5' FROM THE EXTERIOR WALL OF THE FOUNDATION. SITE UTILITY CONTRACTOR SHALL RUN DOWNPOUT LEADS TO BUILDING FOUNDATION AND 18" P" ABOVE SURFACE GRADE FOR CONNECTION TO DOWNPOUT FOR ALL DOWNPOUT TO REER (DR) CONNECTIONS. DOWNPOUTS TO GRADE (DSS) SHALL BE PROVIDED WITH SPLASH BLOCKS AT THE DISCHARGE LOCATION. ALL DOWNPOUT LOCATIONS SHOULD BE VERIFIED WITH ARCHITECTURAL PLANS AND DOWNPOUT CONTRACTOR'S PRIOR TO INSTALLATION OF DOWNPOUT LEADS. DOWNPOUT LEADS SHALL NOT UNDERMINE BUILDING FOUNDATIONS. SITE UTILITY CONTRACTOR SHALL RUN WATER SERVICE TO A POINT WITHIN THE FOUNDATION SPECIFIED IN THE PLUMBING PLANS. CONTRACTOR TO CUT AND CAP WATER SERVICE 12" ABOVE FINISHED FLOOR ELEVATION.
- J. ALL UTILITIES SHALL BE INSTALLED WITH PLASTIC COATED TRACER WIRE (10 TO 14 GAUGE SOLID COPPER OR COPPER COATED STEEL WIRE). PLASTIC WIRE MAY BE TAPED TO PLASTIC WATER OR SEWER PIPE. IF ATTACHED, THE TRACER WIRE SHALL BE SECURED EVERY 6 TO 20 FEET AND AT ALL BENDS. TRACER WIRE SHALL HAVE ACCESS POINTS AT LEAST EVERY 300 FEET. TRACER WIRE SHALL TERMINATE IN ACCORDANCE WITH MANUFACTURER SPECIFICATIONS AT GRADE OR IN TERMINATION BOX PER LOCAL/STATE REQUIREMENTS.
- K. ALL UTILITIES SHALL BE INSTALLED PER STATE, LOCAL, AND INDUSTRY STANDARDS. WATER, SANITARY, AND STORM SEWER SHALL BE INSTALLED PER STANDARD SPECIFICATION FOR SEWER AND WATER CONSTRUCTION IN WISCONSIN. THE EXCEL ENGINEERING DESIGN ENGINEER SHALL BE RESPONSIBLE FOR OBTAINING STATE PLUMBING REVIEW APPROVAL (IF REQUIRED). THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL OTHER PERMITS REQUIRED TO INSTALL WATER, SANITARY AND STORM SEWER.
- L. SEE PLANS FOR ALL OTHER UTILITY SPECIFICATIONS AND DETAILS.



Always a Better Plan

100 Camelot Drive
Fond du Lac, WI 54935
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excelengineer.com

PROJECT INFORMATION

PROPOSED BUILDING ADDITION PHASE ONE FOR:
SHORELAND LUTHERAN HIGH SCHOOL
9026 12TH STREET • KENOSHA, WI 53144

PROFESSIONAL SEAL

PRELIMINARY DATES

AUG. 30, 2024

JOB NUMBER

240018700

SHEET NUMBER

C0.2

NOT FOR CONSTRUCTION

TABLE A: ALLOWABLE PIPE MATERIAL SCHEDULE

Utility	Material	Pipe Code	Fitting Code	Joint Code
Sanitary Sewer	SDR 35 PVC	ASTM D1785, ASTM D2665, ASTM D3034, ASTM F891	ASTM F1336	Push On: ASTM D3212 for Tightness Elastomeric Gasket: ASTM F477
*Sanitary Sewer	SC140 PVC	ASTM D1785, ASTM D2665, ASTM F891	ASTM F1336	Primer: ASTM F656 Solvent Cement: ASTM D2564
Storm Sewer	HDPE	ASTM F2648, ASTM F2306, AASHTO M252, TYPE S 14 IN - 10 IN AASHTO M294, TYPE S 12 IN - 60 IN	ASTM F2648, ASTM F2306, AASHTO M252, or AASHTO M294	Joint: ASTM F2648, ASTM F2306, AASHTO M252, or AASHTO M294 Elastomeric Seal: ASTM F477
Storm Sewer	SDR 35 PVC	ASTM D1785, ASTM D2665, ASTM D3034, ASTM F891	ASTM F1336	Push On: ASTM D3212 for Tightness Elastomeric Seal: ASTM F477

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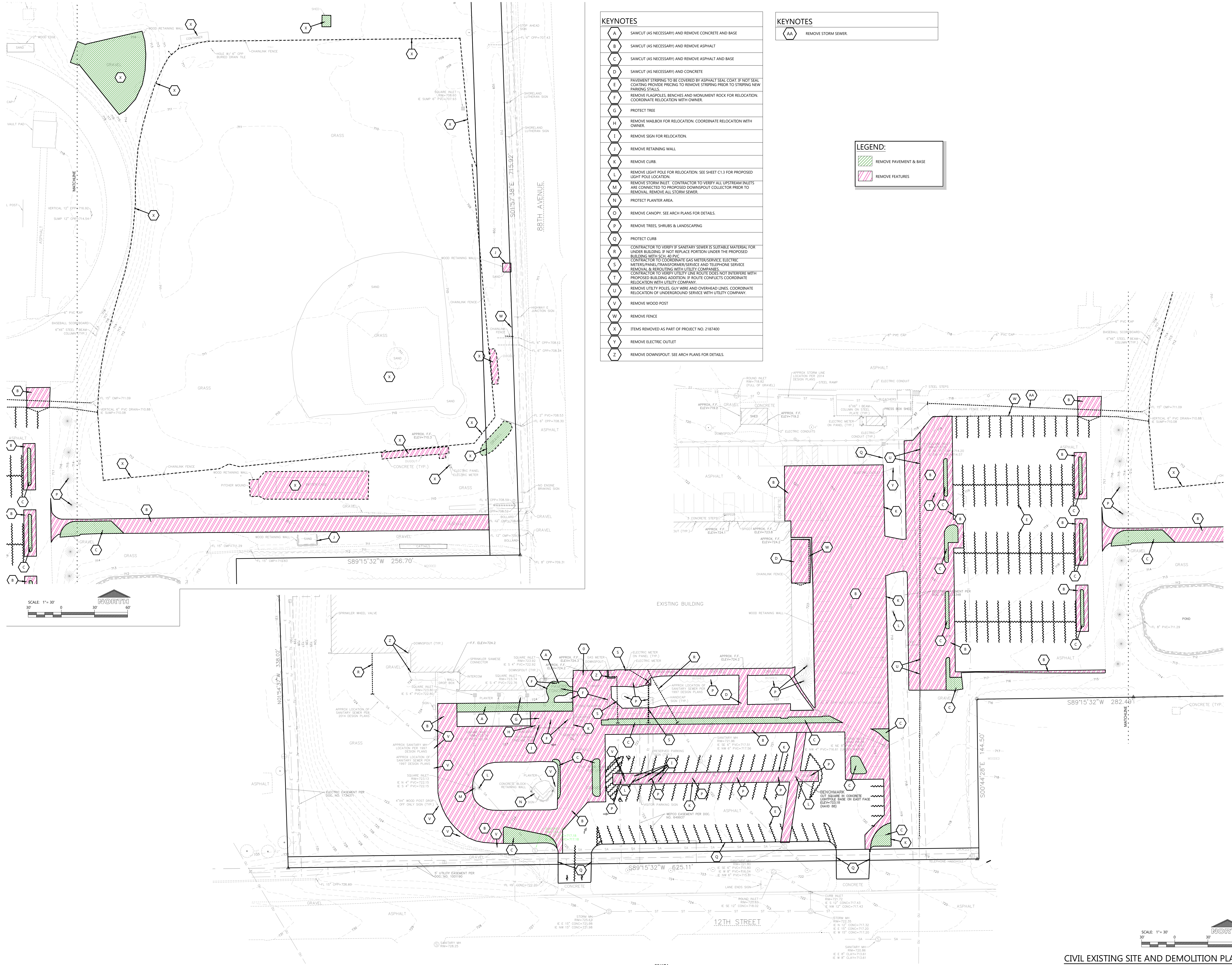
NOT FOR CONSTRUCTION

JOB NUMBER

240018700

SHEET NUMBER

C1.0



KEYNOTES

A	SAWCUT (AS NECESSARY) AND REMOVE CONCRETE AND BASE
B	SAWCUT (AS NECESSARY) AND REMOVE ASPHALT
C	SAWCUT (AS NECESSARY) AND REMOVE ASPHALT AND BASE
D	SAWCUT (AS NECESSARY) AND CONCRETE
E	PAVEMENT STRIPING TO BE COVERED BY ASPHALT SEAL COAT. IF NOT SEAL COATING PROVIDE PRICING TO REMOVE STRIPING PRIOR TO STRIPING NEW PARKING STALLS.
F	REMOVE FLAGPOLES, BENCHES AND MONUMENT ROCK FOR RELOCATION. COORDINATE RELOCATION WITH OWNER.
G	PROTECT TREE
H	REMOVE MAILBOX FOR RELOCATION. COORDINATE RELOCATION WITH OWNER.
I	REMOVE SIGN FOR RELOCATION.
J	REMOVE RETAINING WALL
K	REMOVE CURB.
L	REMOVE LIGHT POLE FOR RELOCATION. SEE SHEET C1.3 FOR PROPOSED LIGHT POLE LOCATION.
M	REMOVE STORM INLET. CONTRACTOR TO VERIFY ALL UPSTREAM INLETS ARE CONNECTED TO PROPOSED DOWNSPOUT COLLECTOR PRIOR TO REMOVAL. REMOVE ALL STORM SEWER.
N	PROTECT PLANTER AREA.
O	REMOVE CANOPY. SEE ARCH PLANS FOR DETAILS.
P	REMOVE TREES, SHRUBS & LANDSCAPING
Q	PROTECT CURB
R	CONTRACTOR TO VERIFY IF SANITARY SEWER IS SUITABLE MATERIAL FOR UNDER BUILDING. IF NOT REPLACE PORTION UNDER THE PROPOSED BUILDING WITH SOLID PVC.
S	CONTRACTOR TO COORDINATE GAS METER SERVICE, ELECTRIC METERS/PANELS/TRANSFORMER SERVICE AND TELEPHONE SERVICE REMOVAL & REROUTING WITH UTILITY COMPANIES.
T	CONTRACTOR TO VERIFY UTILITY LINE ROUTE DOES NOT INTERFERE WITH PROPOSED BUILDING ADDITION. IF ROUTE CONFLICTS COORDINATE RELOCATION WITH UTILITY COMPANY.
U	REMOVE UTILITY POLES, GUY WIRE AND OVERHEAD LINES. COORDINATE RELOCATION OF UNDERGROUND SERVICE WITH UTILITY COMPANY.
V	REMOVE WOOD POST
W	REMOVE FENCE
X	ITEMS REMOVED AS PART OF PROJECT NO. 2187400
Y	REMOVE ELECTRIC OUTLET
Z	REMOVE DOWNSPOUT. SEE ARCH PLANS FOR DETAILS.

KEYNOTES

AA	REMOVE STORM SEWER.
----	---------------------

LEGEND:

[Green Hatched]	REMOVE PAVEMENT & BASE
[Pink Hatched]	REMOVE FEATURES

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PROFESSIONAL SEAL

PRELIMINARY DATES

JULY 9, 2024
JULY 16, 2024
AUG. 30, 2024

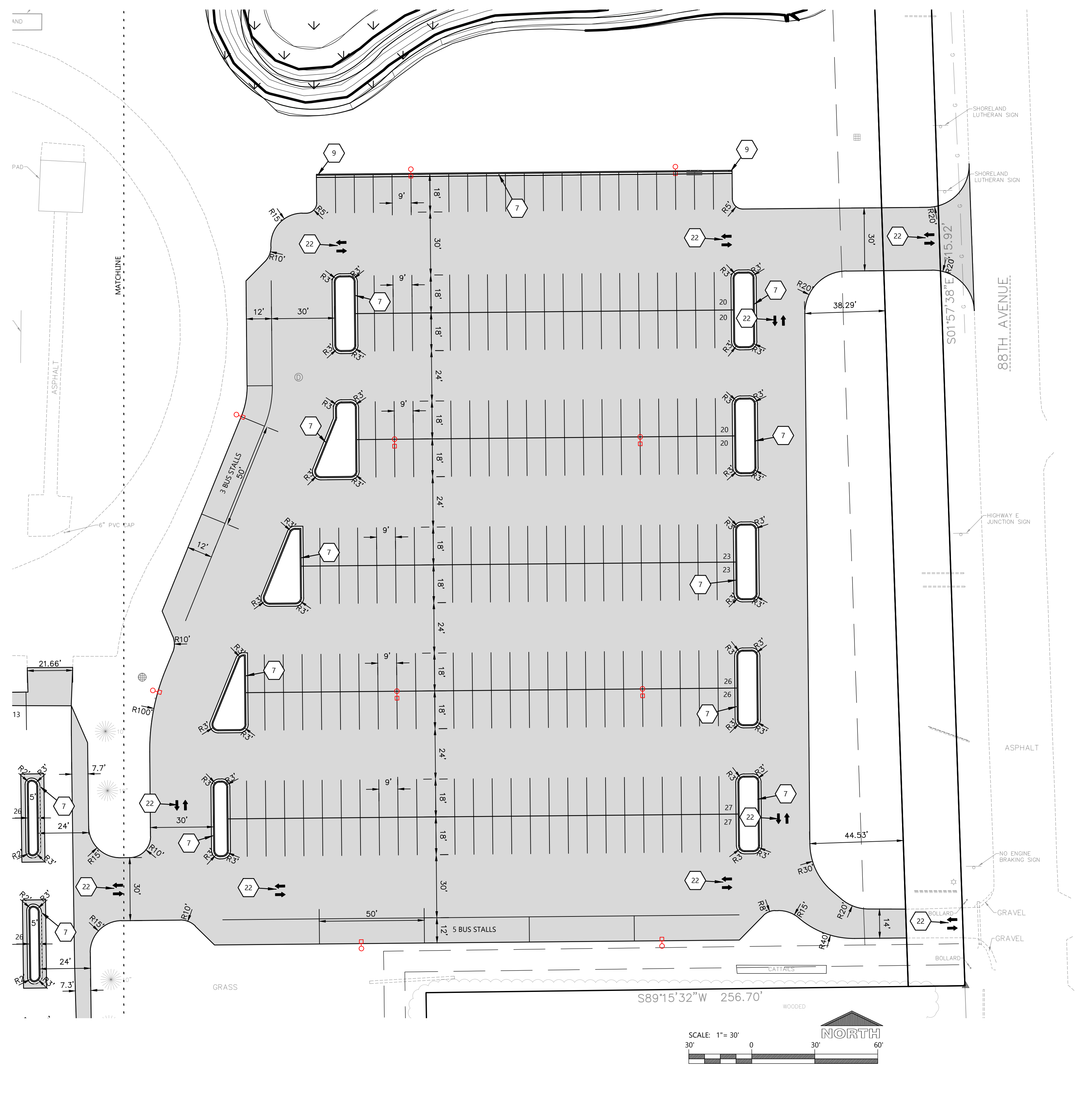
JOB NUMBER

240018700

SHEET NUMBER

C1.1

NOT FOR CONSTRUCTION

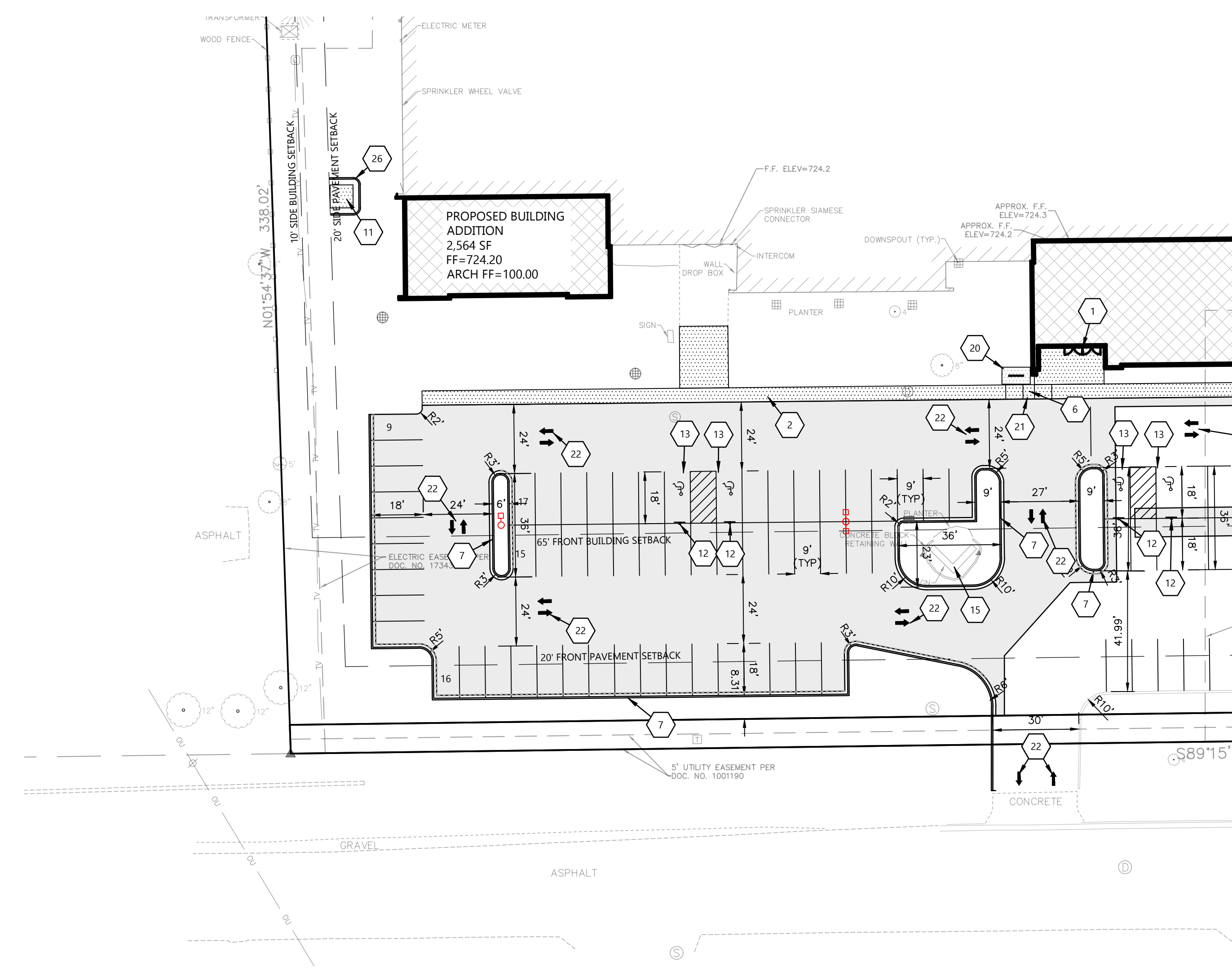
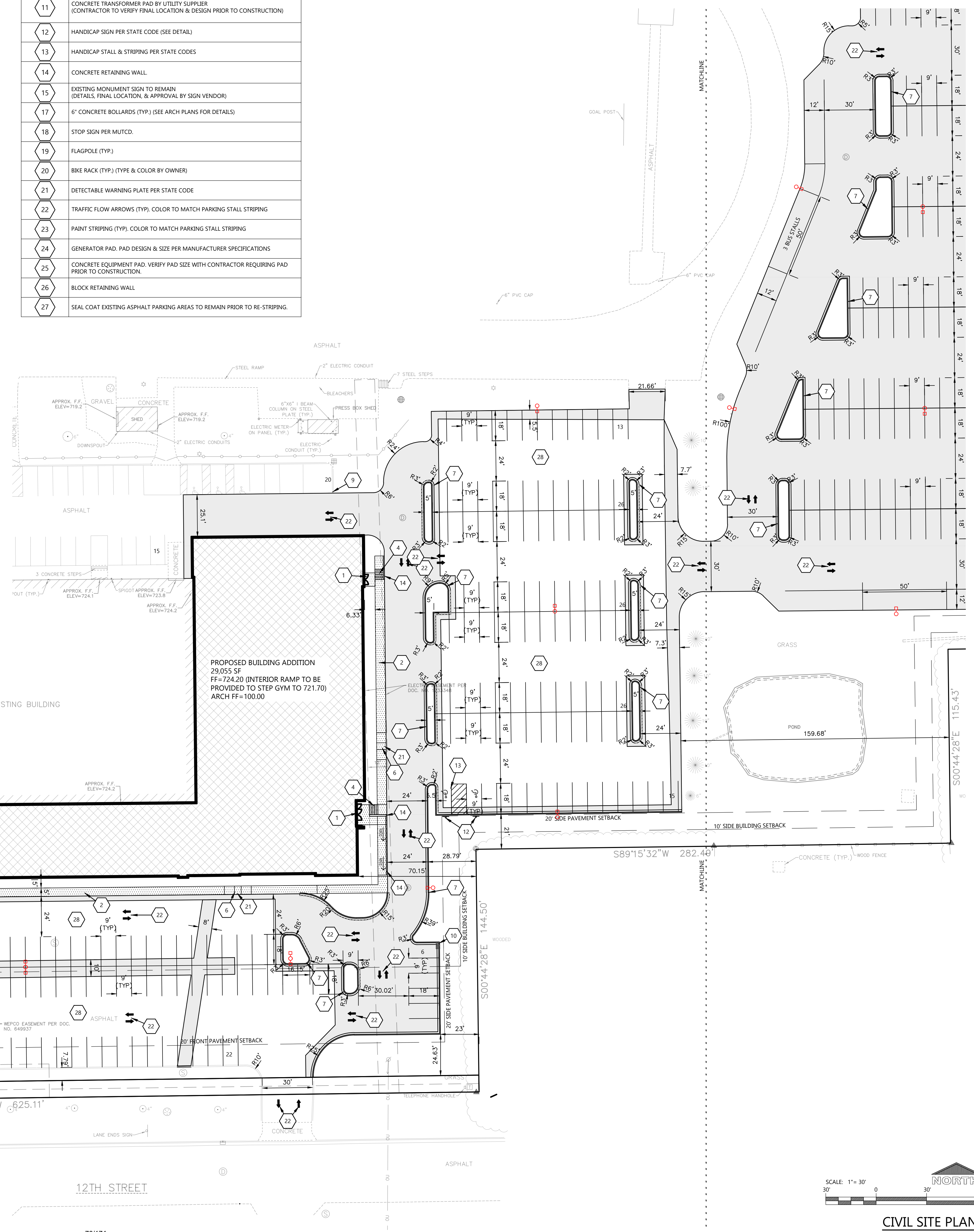


KEYNOTES

- CONCRETE STOOP (SEE STRUCTURAL PLANS FOR DETAILS)
- RAISED WALK (SEE DETAIL)
- FLUSH WALK (SEE DETAIL)
- CONCRETE STAIRS. SEE ARCH/STRUCTURAL PLANS FOR INFORMATION.
- CURB RAMP (SEE DETAIL)
- ADA CURB RAMP (SEE DETAIL)
- 18" CURB & GUTTER (SEE DETAIL)
- 18" MOUNTABLE CURB & GUTTER (SEE DETAIL)
- CURB TAPER (SEE DETAIL)
- CURB CUT (SEE DETAIL)
- CONCRETE TRANSFORMER PAD BY UTILITY SUPPLIER (CONTRACTOR TO VERIFY FINAL LOCATION & DESIGN PRIOR TO CONSTRUCTION)
- HANDICAP SIGN PER STATE CODE (SEE DETAIL)
- HANDICAP STALL & STRIPING PER STATE CODES
- CONCRETE RETAINING WALL
- EXISTING MONUMENT SIGN TO REMAIN (DETAILS, FINAL LOCATION, & APPROVAL BY SIGN VENDOR)
- 6" CONCRETE BOLLARDS (TYP.) (SEE ARCH PLANS FOR DETAILS)
- STOP SIGN PER MUTCD.
- FLAGPOLE (TYP.)
- BIKE RACK (TYP.) (TYPE & COLOR BY OWNER)
- DETECTABLE WARNING PLATE PER STATE CODE
- TRAFFIC FLOW ARROWS (TYP.) COLOR TO MATCH PARKING STALL STRIPING
- PAINT STRIPING (TYP.) COLOR TO MATCH PARKING STALL STRIPING
- GENERATOR PAD. PAD DESIGN & SIZE PER MANUFACTURER SPECIFICATIONS
- CONCRETE EQUIPMENT PAD. VERIFY PAD SIZE WITH CONTRACTOR REQUIRING PAD PRIOR TO CONSTRUCTION.
- BLOCK RETAINING WALL
- SEAL COAT EXISTING ASPHALT PARKING AREAS TO REMAIN PRIOR TO RE-STRIPING.

LEGEND:

HATCH	PAVEMENT SECTION	HATCH	PAVEMENT SECTION
[Hatch]	STANDARD ASPHALT	[Hatch]	HEAVY DUTY CONCRETE
[Hatch]	HEAVY DUTY ASPHALT	[Hatch]	LOADING DOCK CONCRETE
[Hatch]	SIDEWALK CONCRETE	[Hatch]	DUMPSTER PAD / APRON CONCRETE
[Hatch]	LIGHT DUTY CONCRETE	[Hatch]	SHEDDING CURB & GUTTER
[Hatch]	INVERTED CURB & GUTTER	[Hatch]	



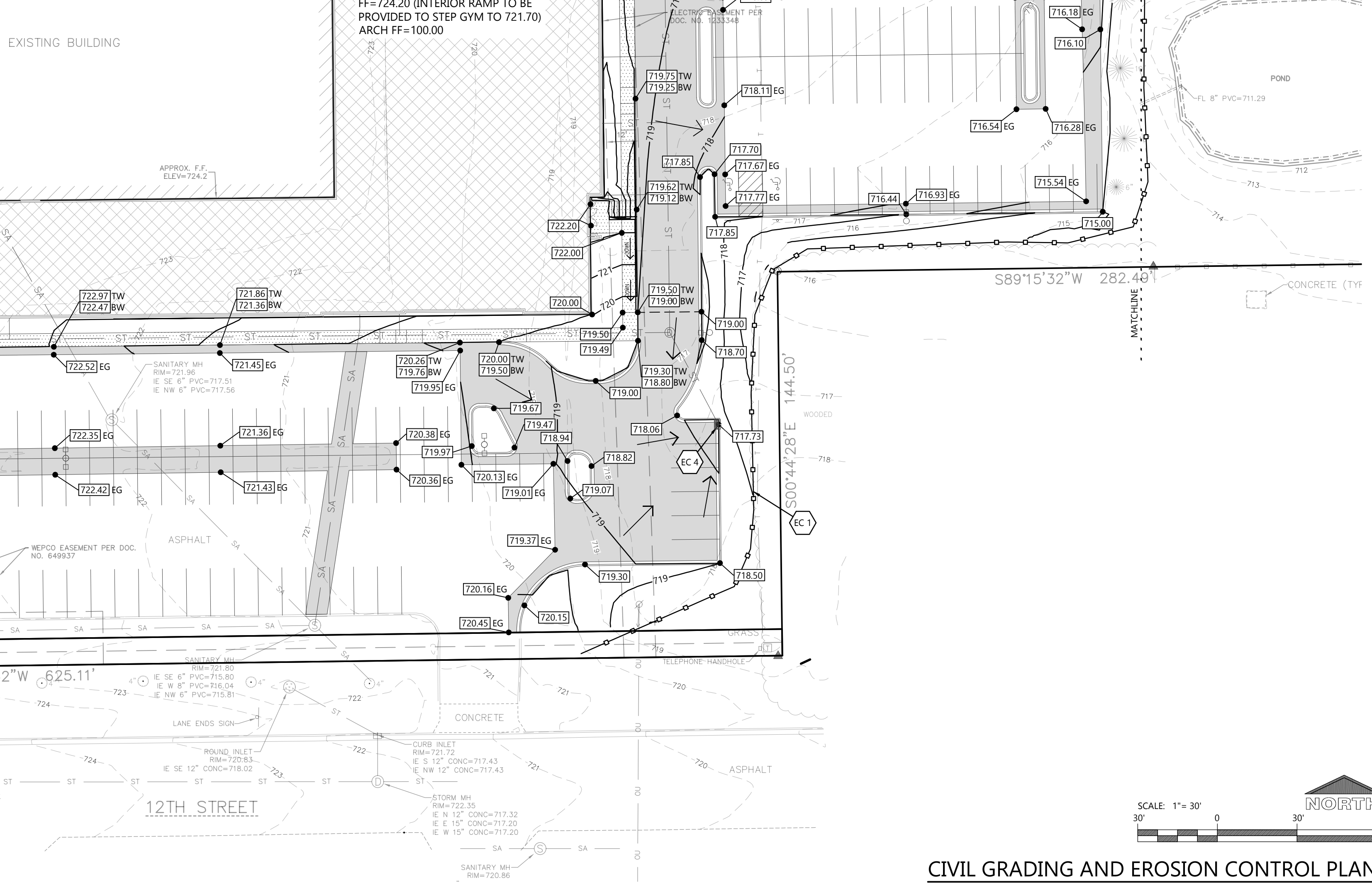
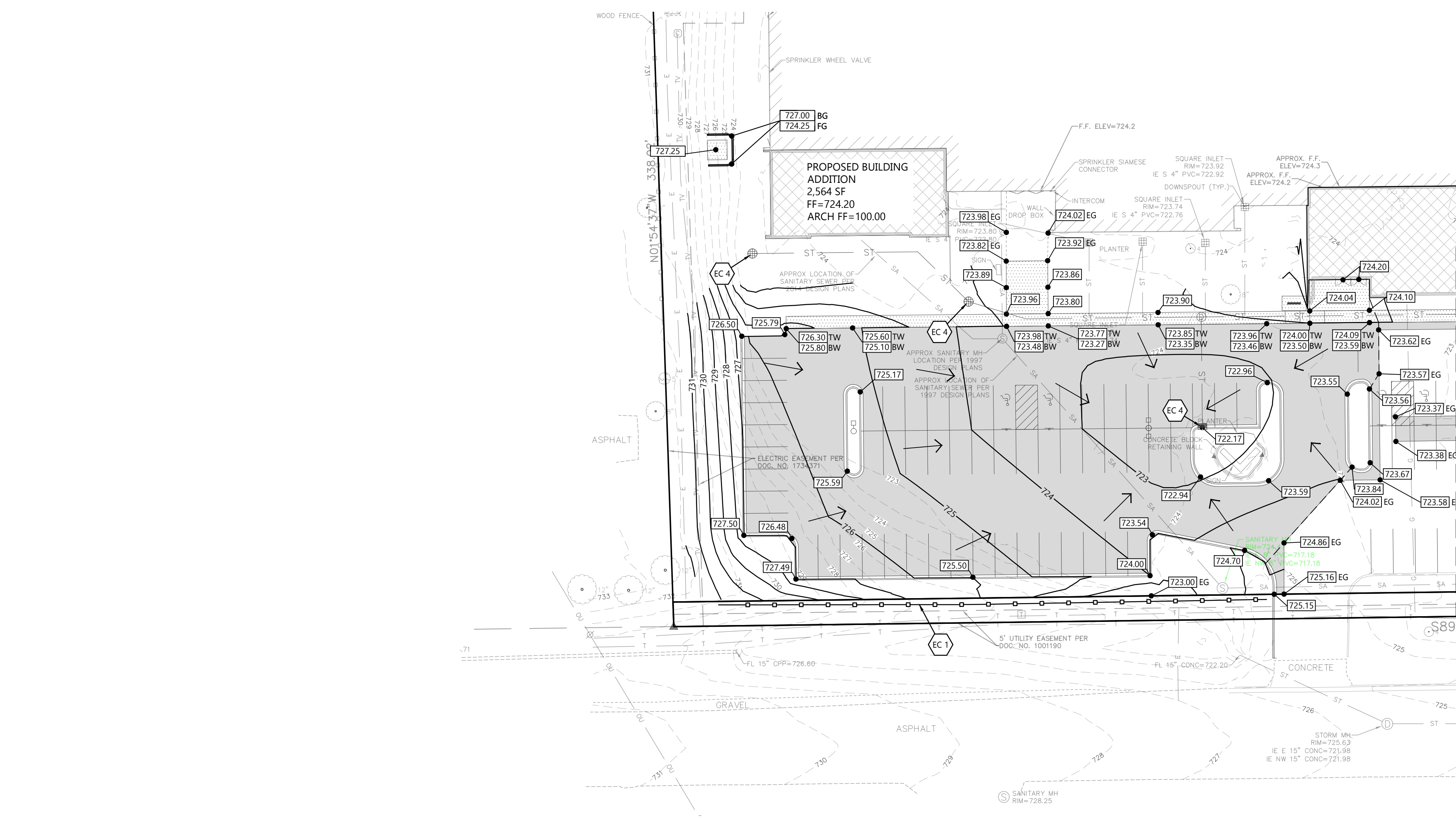
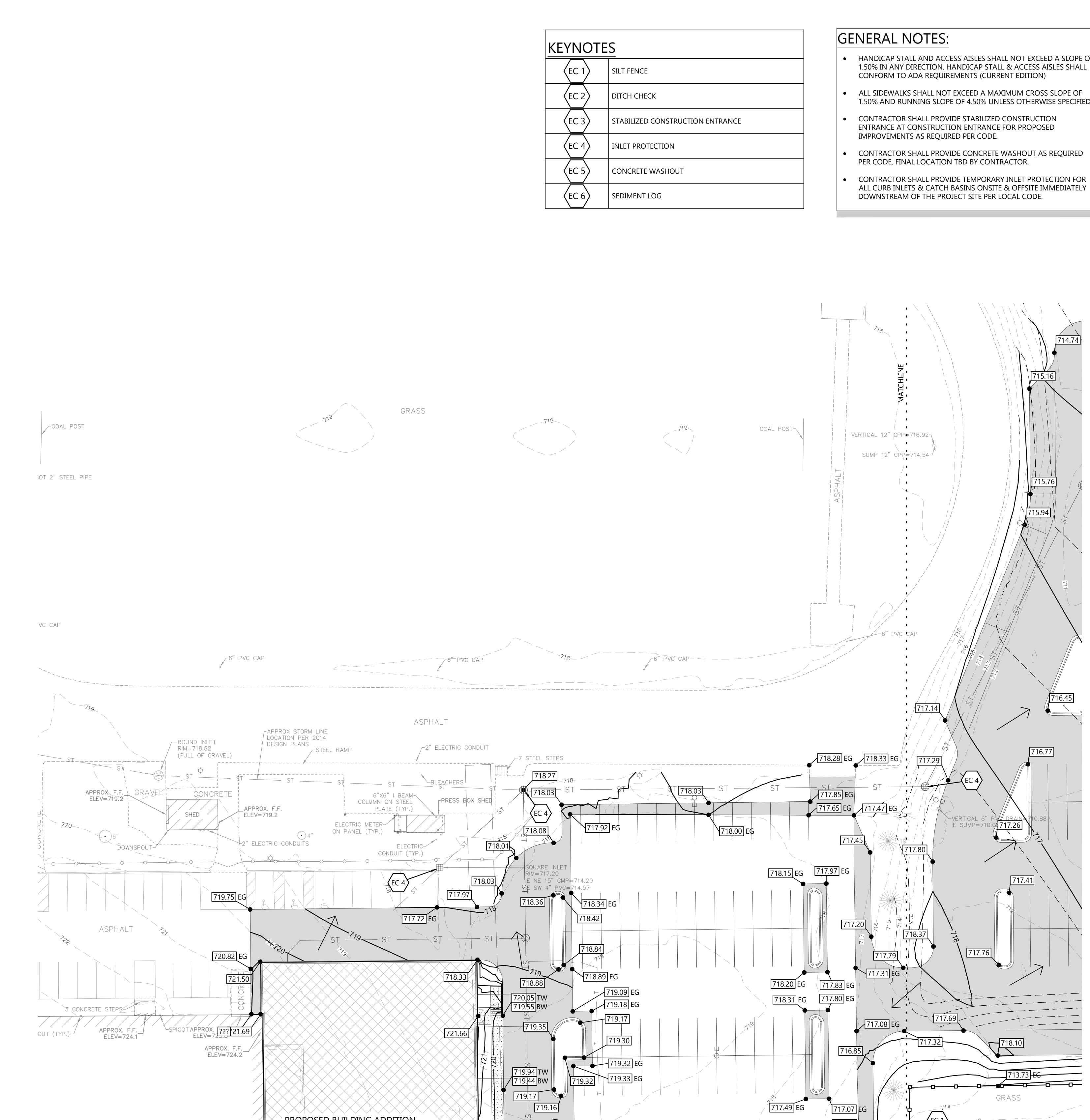
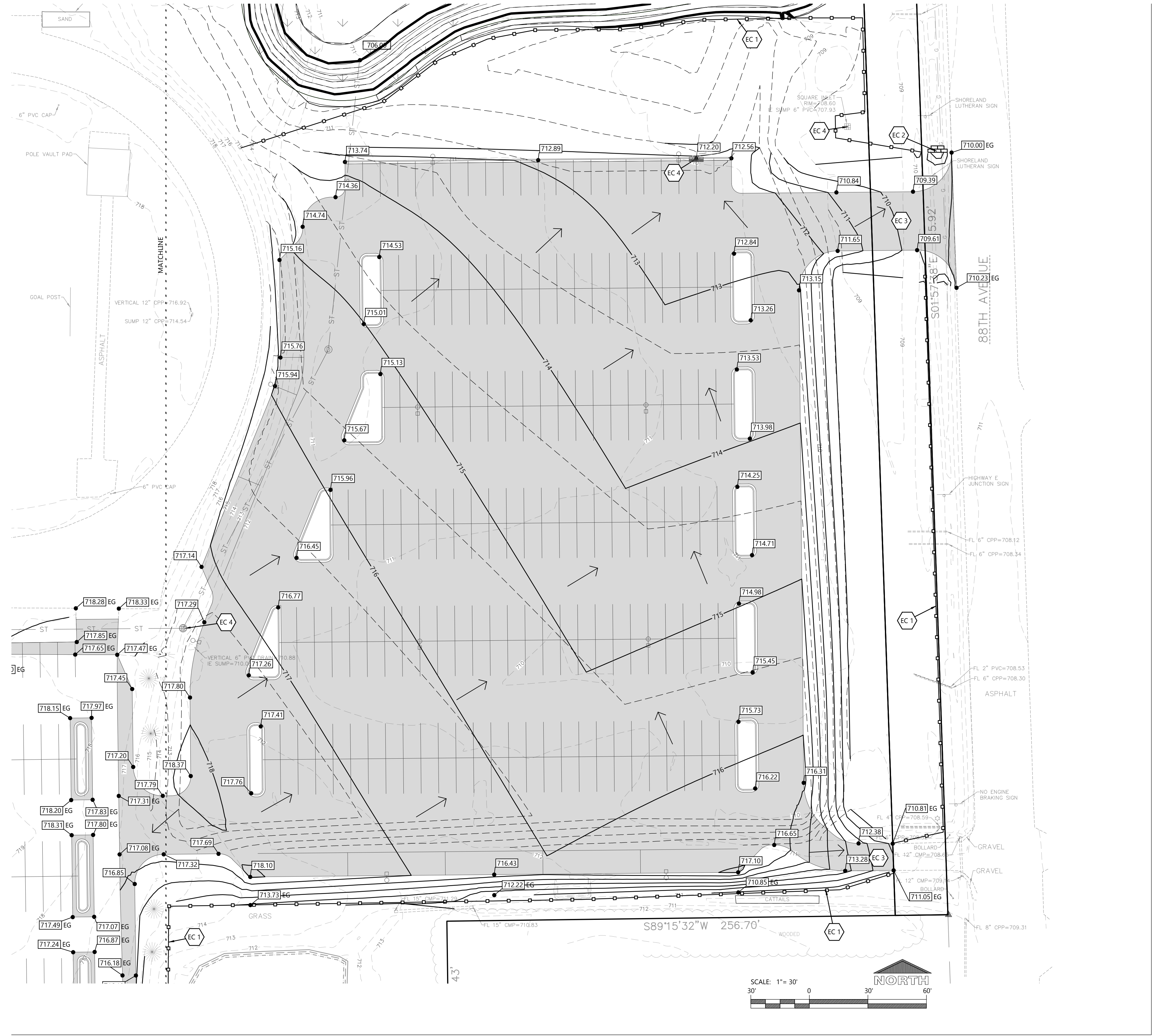
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CIVIL SITE PLAN

KEYNOTES	
EC 1	SILT FENCE
EC 2	DITCH CHECK
EC 3	STABILIZED CONSTRUCTION ENTRANCE
EC 4	INLET PROTECTION
EC 5	CONCRETE WASHOUT
EC 6	SEDIMENT LOG

GENERAL NOTES:

- HANDICAP STALL AND ACCESS ASLES SHALL NOT EXCEED A SLOPE OF 1.50% IN ANY DIRECTION. HANDICAP STALL & ACCESS ASLES SHALL CONFORM TO ADA REQUIREMENTS (CURRENT EDITION)
- ALL SIDEWALKS SHALL NOT EXCEED A MAXIMUM CROSS SLOPE OF 1.50% AND RUNNING SLOPE OF 4.50% UNLESS OTHERWISE SPECIFIED.
- CONTRACTOR SHALL PROVIDE STABILIZED CONSTRUCTION ENTRANCE #1 CONSTRUCTION ENTRANCE FOR PROPOSED IMPROVEMENTS AS REQUIRED PER CODE.
- CONTRACTOR SHALL PROVIDE CONCRETE WASHOUT AS REQUIRED PER CODE. FINAL LOCATION TBD BY CONTRACTOR.
- CONTRACTOR SHALL PROVIDE TEMPORARY INLET PROTECTION FOR ALL CURB INLETS & CATCH BASINS ON-SITE & OFF-SITE IMMEDIATELY DOWNSTREAM OF THE PROJECT SITE PER LOCAL CODE.



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES TECHNICAL STANDARD NO. 1053 (CHANNEL EROSION MAT).

VARIATIONS IN THE DIMENSIONS OR MATERIALS SHOWN HEREON SHALL BE PERMITTED IF THEY PROVIDE EQUIVALENT PROTECTION AND MATERIAL STRENGTH AND IF PRIOR APPROVAL OF THE ENGINEER IS OBTAINED.

LAP JOINTS SHALL NOT BE PLACED IN THE BOTTOM OF V-SHAPED DITCHES.

JUNCTION SLOTS ON ADJACENT STRIPS OF MATTING SHALL BE STAGGERED A MINIMUM OF 4 FEET APART.

EDGES OF EROSION MAT SHALL BE IMPRESSED IN THE SOIL.

EROSION MAT SHALL PAID BY THE SQUARE YARD INSTALLED.

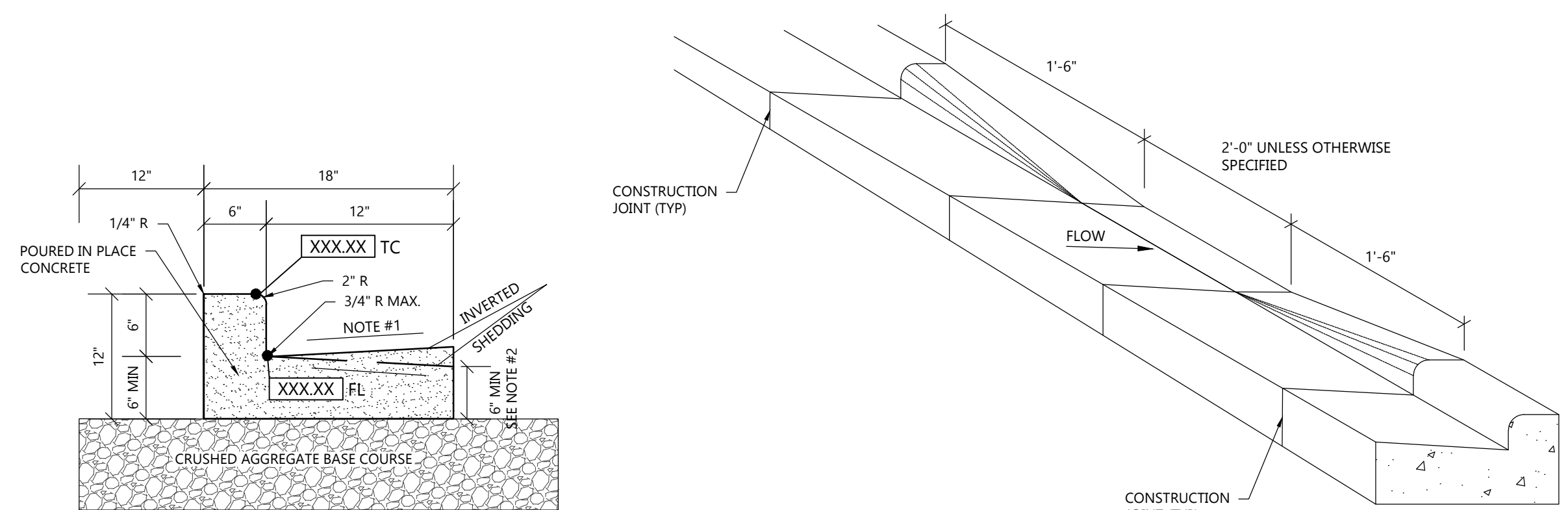
EROSION MAT OVER SEEDING

JUNCTION OR ANCHOR SLOTS SHALL BE AT MINIMUM INTERVALS OF 100 FEET ON GRADES UP TO AND INCLUDING 3%, AND 50 FEET ON GRADES EXCEEDING 3%.

NOTE: SEE SPECIFICATIONS FOR MATTING TYPE

CHANNEL EROSION MAT DETAIL

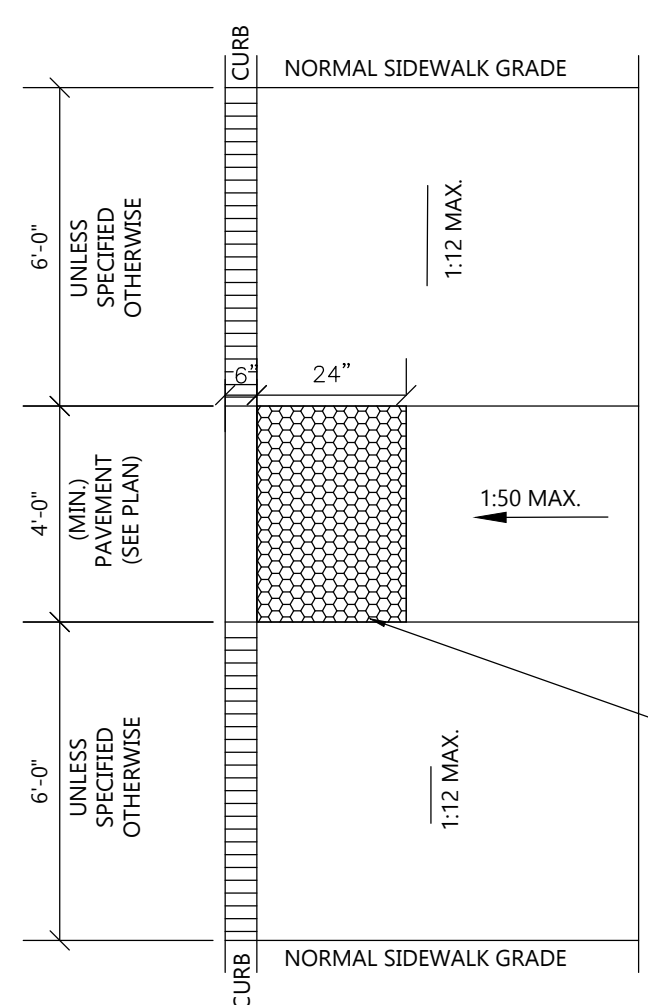
NOT TO SCALE



NOTE:
1. USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
2. THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MIN. GUTTER THICKNESS IS MAINTAINED.
3. SEE SITE PLAN & GRADING PLAN FOR INVERTED & SHEDDING CURB LOCATIONS

18" CONCRETE CURB & GUTTER DETAIL

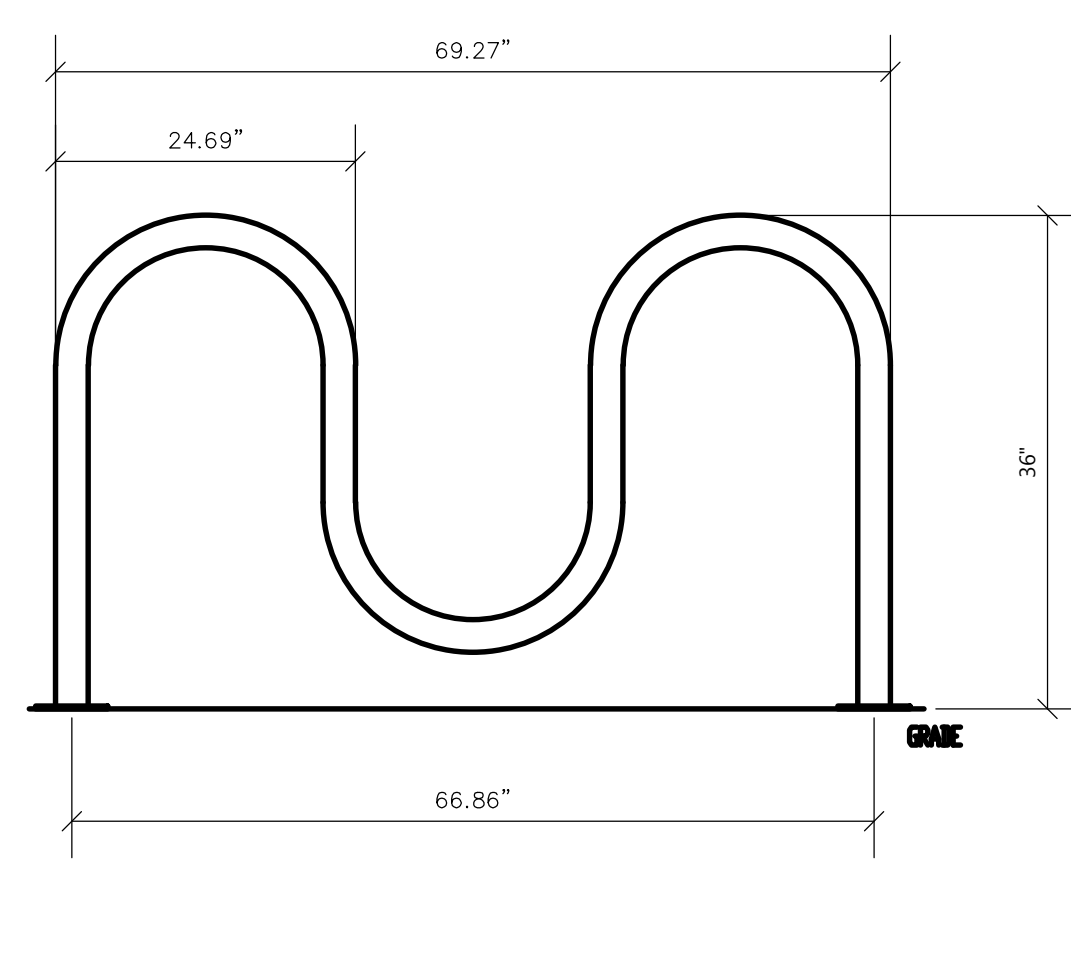
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NOTE:
ADA CURB RAMP SHALL CONFORM TO THE CURRENT EDITION OF ADA STANDARDS FOR ACCESSIBLE DESIGN FOR ALL REQUIREMENTS.
DETECTABLE WARNING SURFACE: SURFACE TO BE TRUNCATED DOMES WITH A BASE DIAMETER OF 0.9" MIN. TO 1.4" MAX. AND A TOP DIAMETER OF 0.5" MIN. TO 0.55" MAX. OF THE BASE DIAMETER. HEIGHT OF DOMES SHALL HAVE A HEIGHT OF 0.2" TRUNCATED DOMES SHALL HAVE A CENTER-TO-CENTER SPACING OF 1.6" MIN. AND 2.4" MAX. AND A BASE-TO-BASE SPACING OF 0.65" MIN. MEASURED BETWEEN THE MOST ADJACENT DOMES ON THE GRID. DOMES SHALL BE ALIGNED IN A SQUARE GRID PATTERN.

ADA SIDEWALK RAMP DETAIL

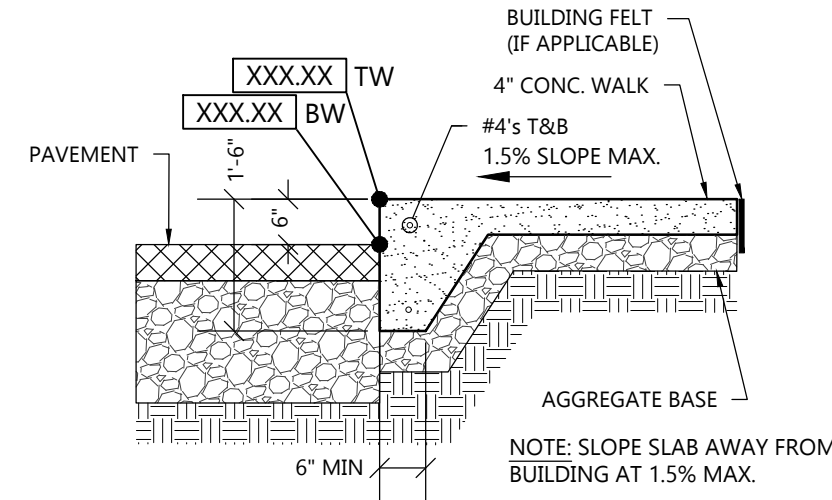
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NOTES:
1. INSTALL BIKE RACKS ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
2. OWNER SHALL SELECT COLOR & FINISH.
3. SEE SITE PLAN FOR APPROX. LOCATION, COORDINATE W/ OWNER PRIOR TO CONSTRUCTION.
4. MANUFACTURED BY MADRAK; PRODUCT: CS200-S-IGSF; DESCRIPTION: CAPITAL SQUARE BIKE RAKE 5 BIKE

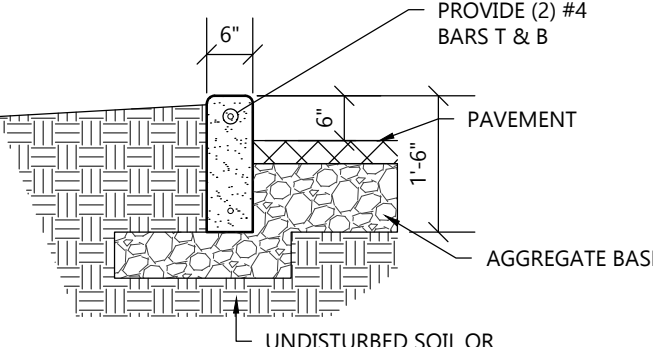
5 BIKE RACK DETAIL-WAVE TYPE

NOT TO SCALE



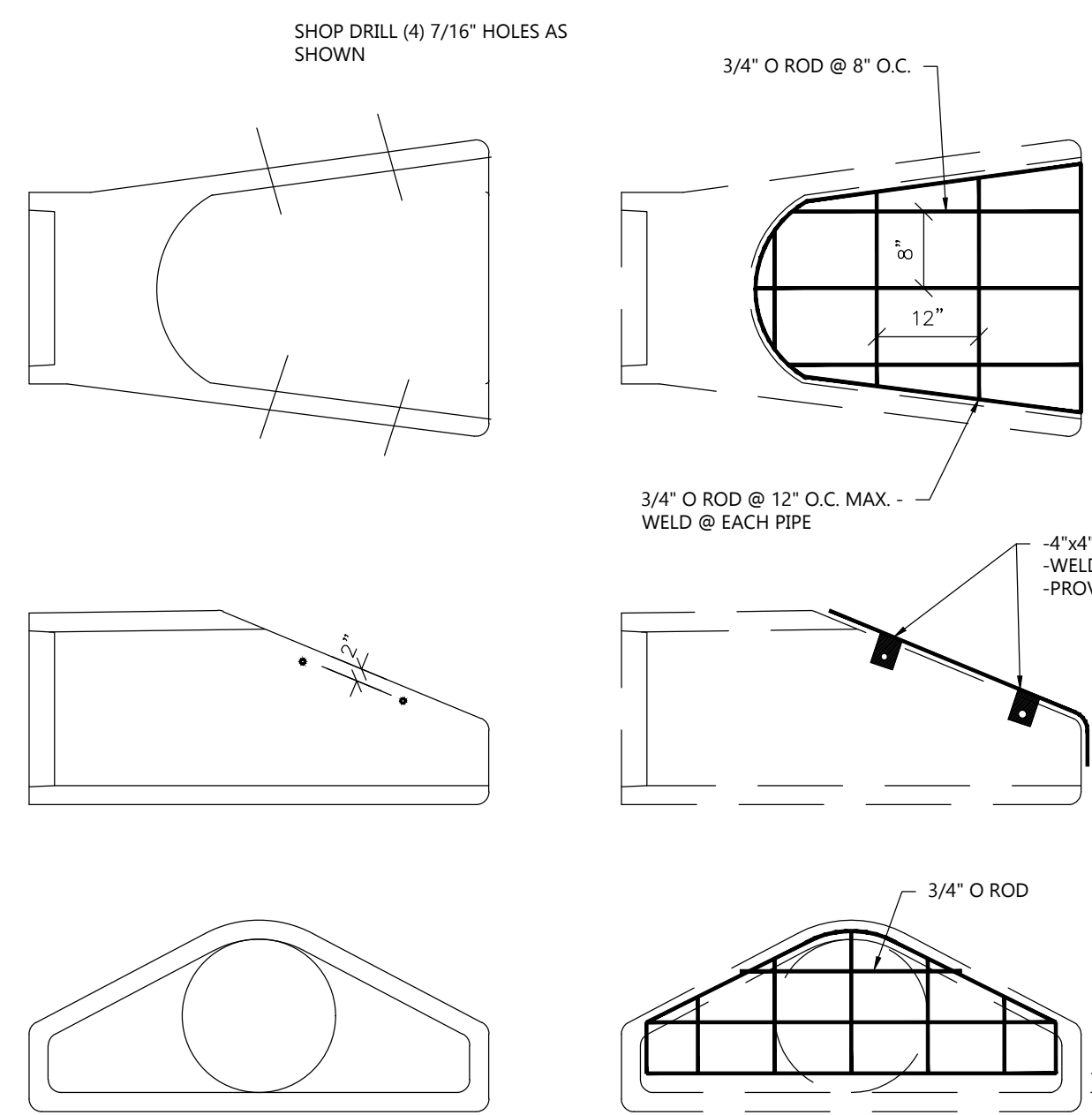
RAISED WALK DETAIL

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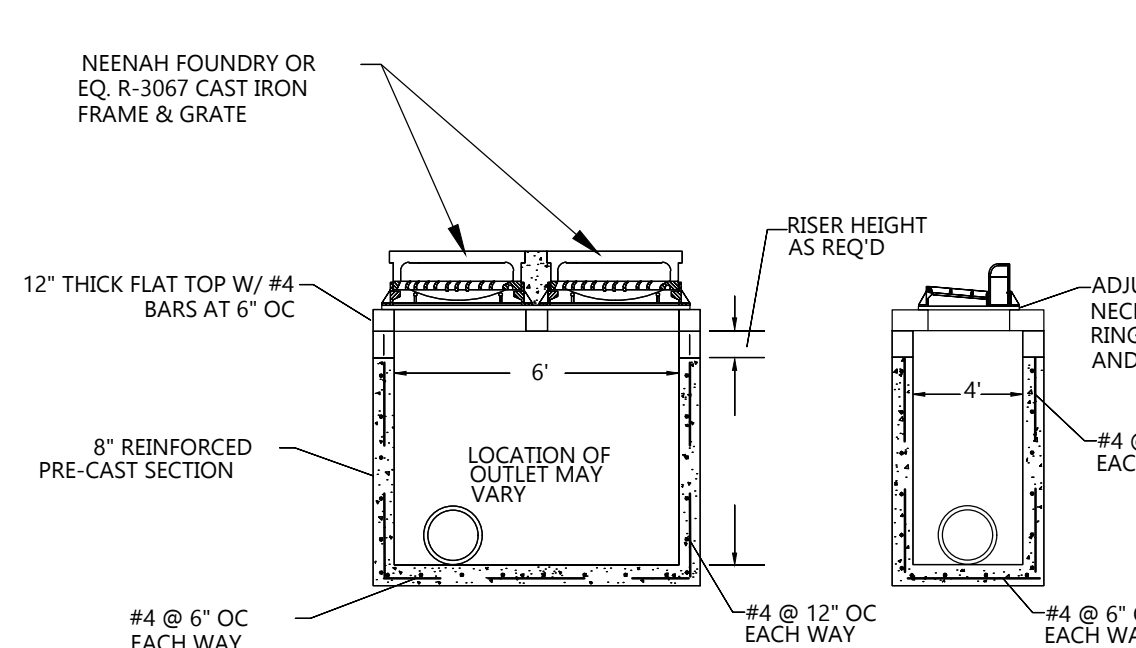
6" CURB HEAD DETAIL

NOT TO SCALE



CURB CUT DETAIL

NOT TO SCALE



DOUBLE FRAME CURB INLET

NO SCALE

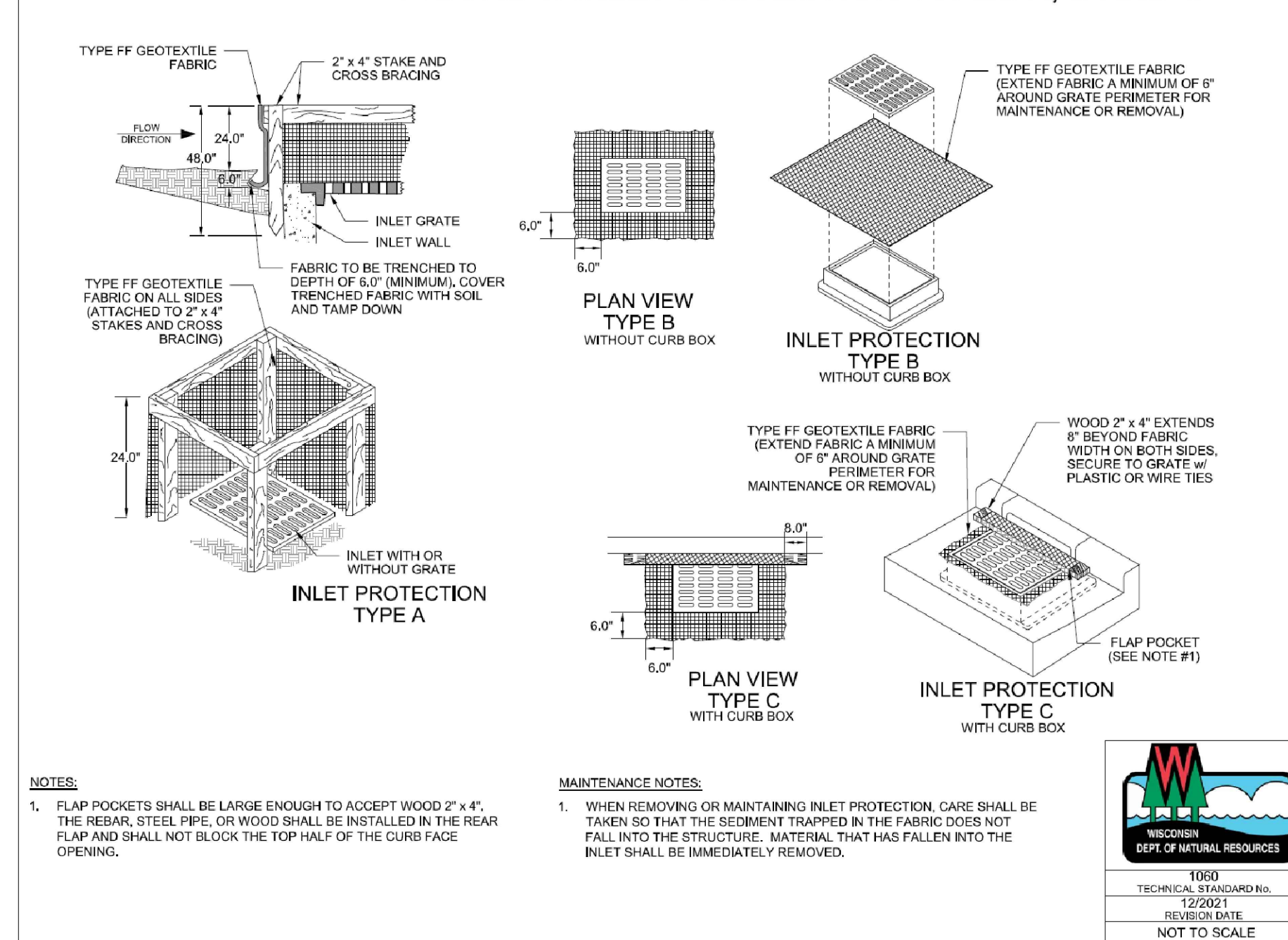
GRATE INSTALLATION DETAIL

NOT TO SCALE

THE CONTRACTOR SHALL BOLT THE GRATE TO THE CONCRETE END WALL WITH FOUR 3/8" X 6" MACHINE BOLTS WITH NUTS ON INSIDE WALL. APPLICABLE FOR PIPE SIZES 18" AND GREATER.
PAINTING SPECIFICATIONS:
THE PIPE GRATE SHALL RECEIVE THE FOLLOWING PREPARATION & PAINTING. SEE NOTES: FIRST COAT- RUST-OLEUM X-60 RED BARE METAL PRIMER OR EQUAL. SECOND COAT- RUST-OLEUM 960 ZINC CHROMATE PRIMER OR EQUAL. THIRD COAT- RUST-OLEUM 1500 HIGH GLOSS 8 METALLIC FINISH OR EQUAL.
NOTES:
• BARE SURFACES - AFTER THROUGH SCRAPING, WIRE BRUSHING & CLEANING, APPLY THE THREE COAT SYSTEM LISTED.
• EACH COAT AN OVERALL COAT
• ALLOW 24-48 HOURS DRYING TIME BETWEEN COATS.

NOTE:
CONTRACTOR IS RESPONSIBLE TO INSTALL SANITARY RISER PER CITY REQUIREMENTS. IF DISCREPANCIES OCCUR, CONTRACTOR TO CONTACT DESIGN ENGINEER PRIOR TO INSTALLATION.

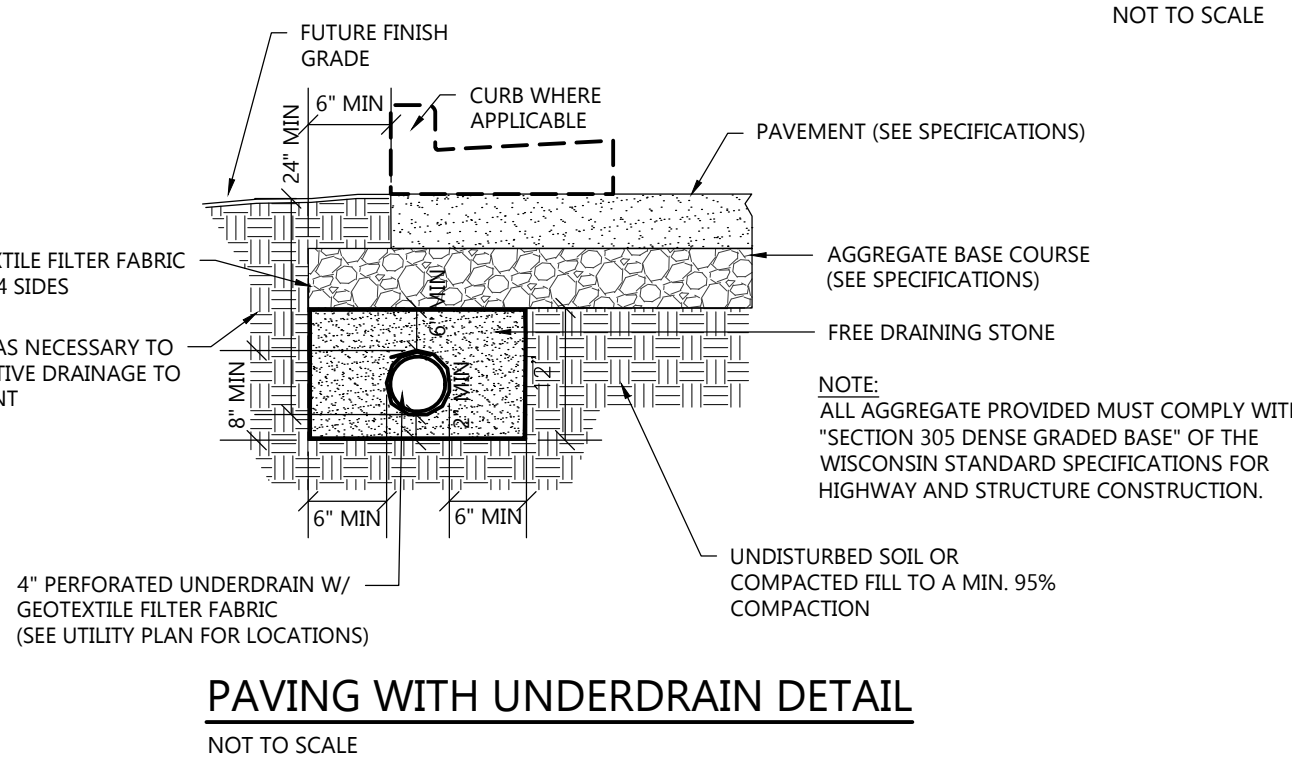
FIGURE 1. INLET PROTECTION TYPES A, B AND C



NOTE:
1. FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2" X 4". THE REAR STEEL PIPE, OR WOOD SHALL BE INSTALLED IN THE REAR FLAP AND SHALL NOT BLOCK THE TOP HALF OF THE CURB FACE OPENING.
2. WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEGMENT TRAPPED IN THE FABRIC DOES NOT FALL INTO THE STRUCTURE. MATERIAL THAT HAS FALLEN INTO THE INLET SHALL BE IMMEDIATELY REMOVED.

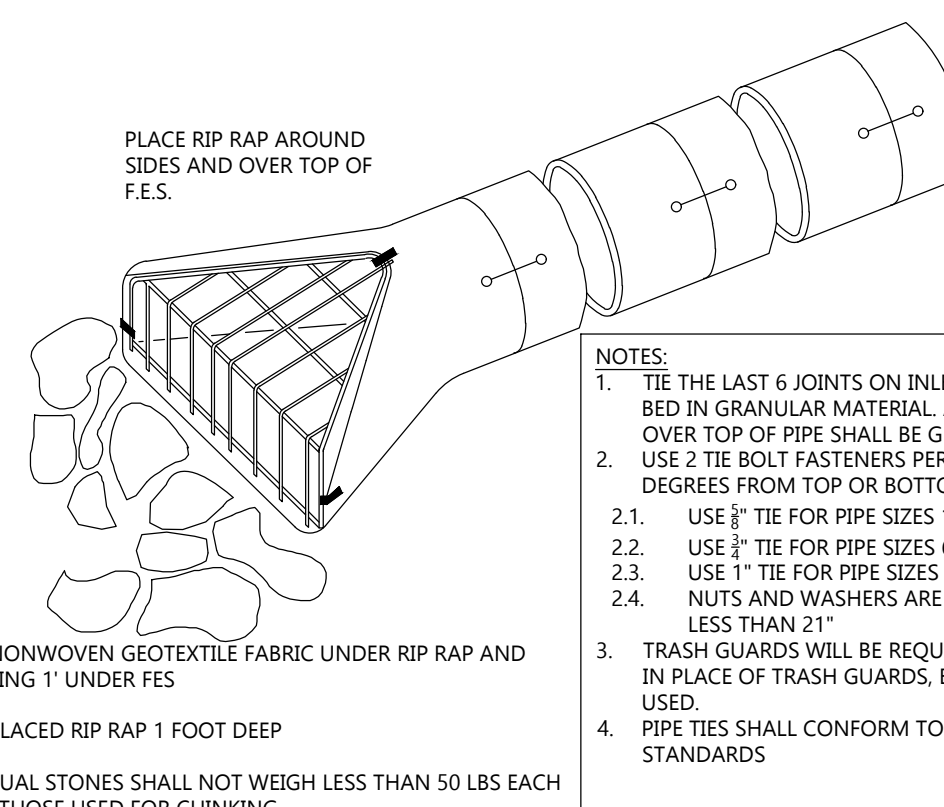
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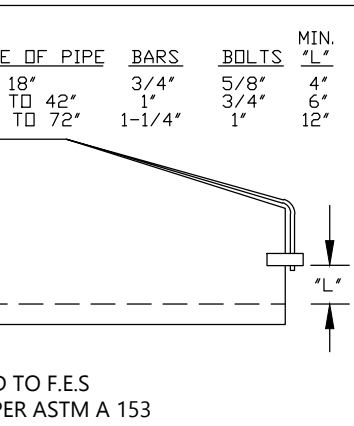
PAVING WITH UNDERDRAIN DETAIL

NOT TO SCALE



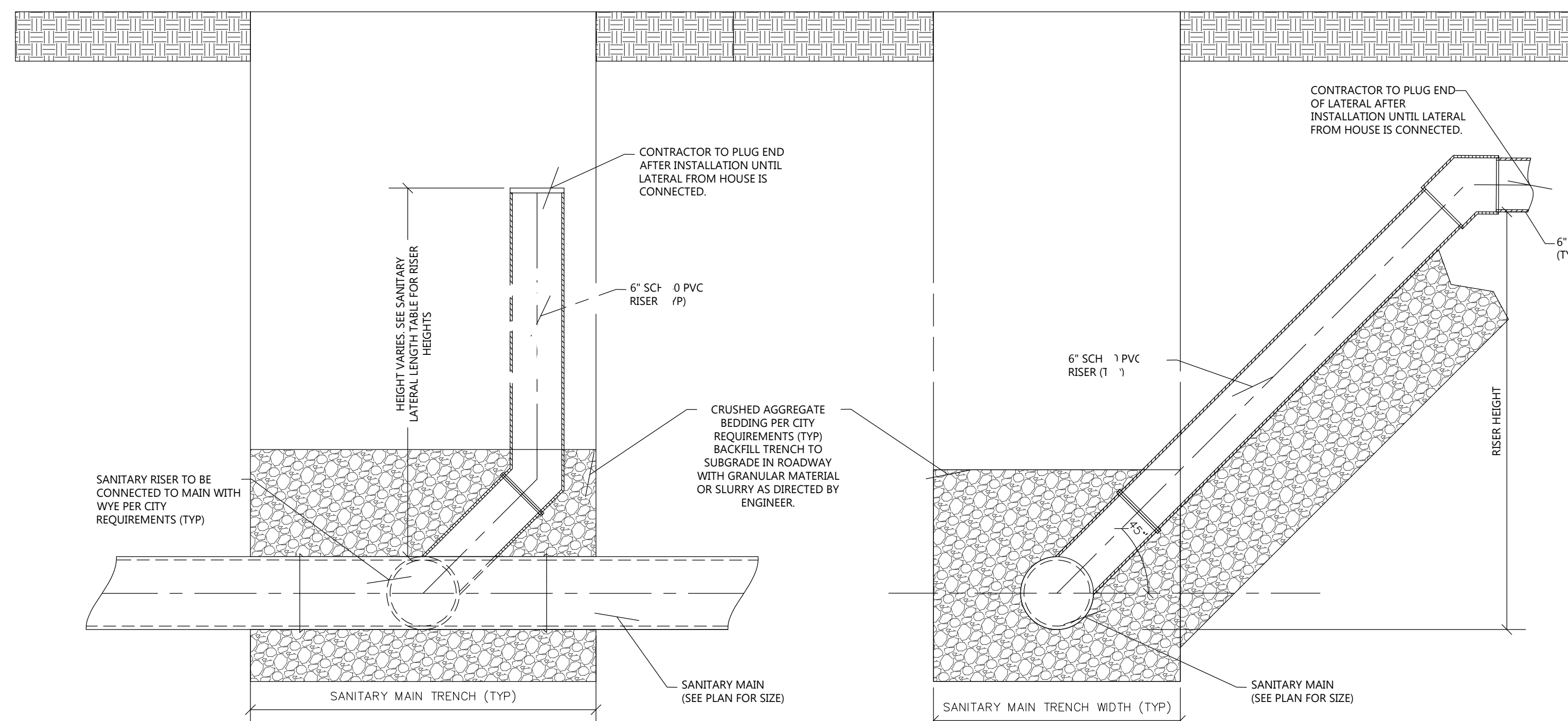
CONCRETE END SECTION DETAIL

NOT TO SCALE



CURB TAPER DETAIL

NOT TO SCALE



PLAN VIEW

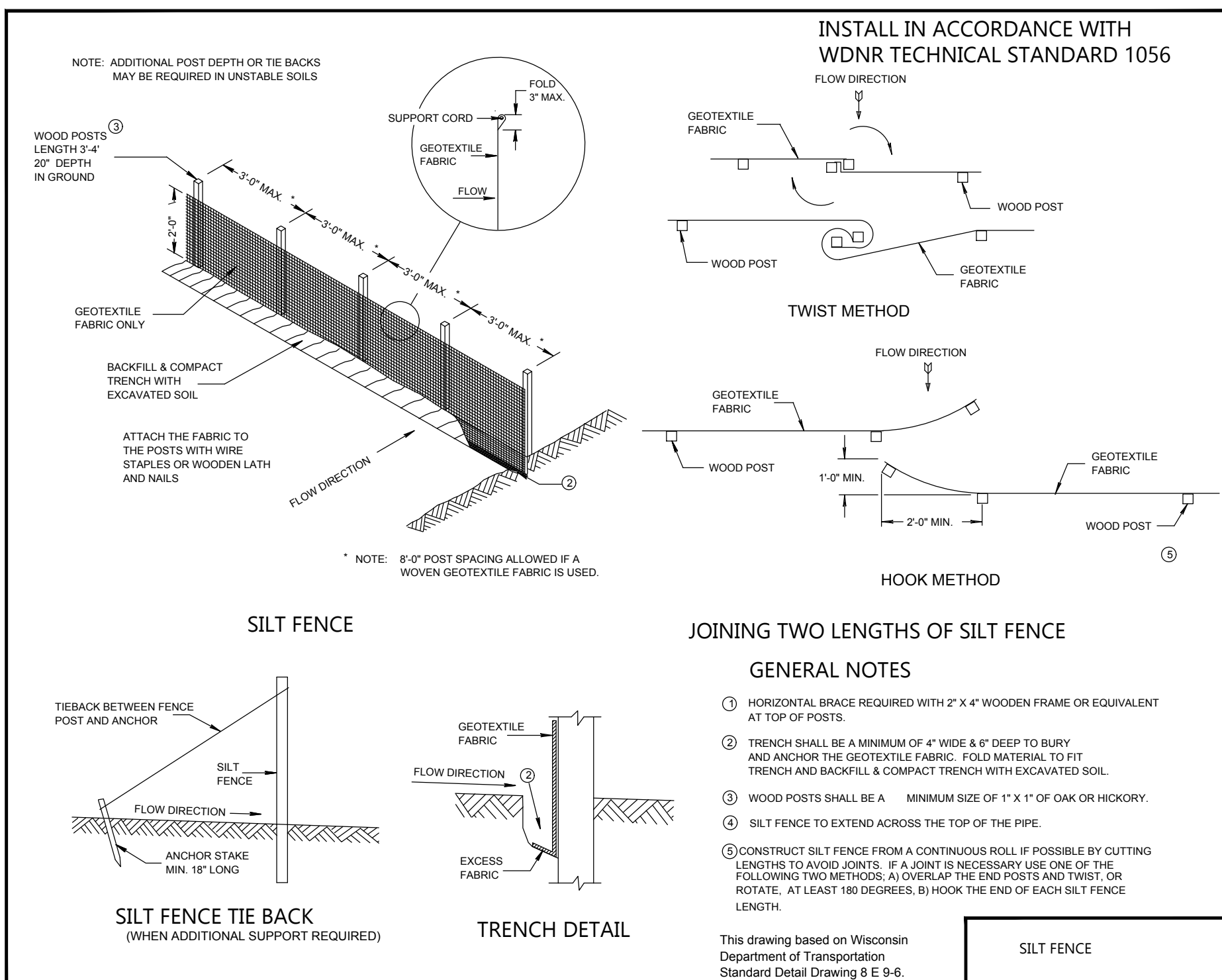
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SANITARY RISER DETAIL

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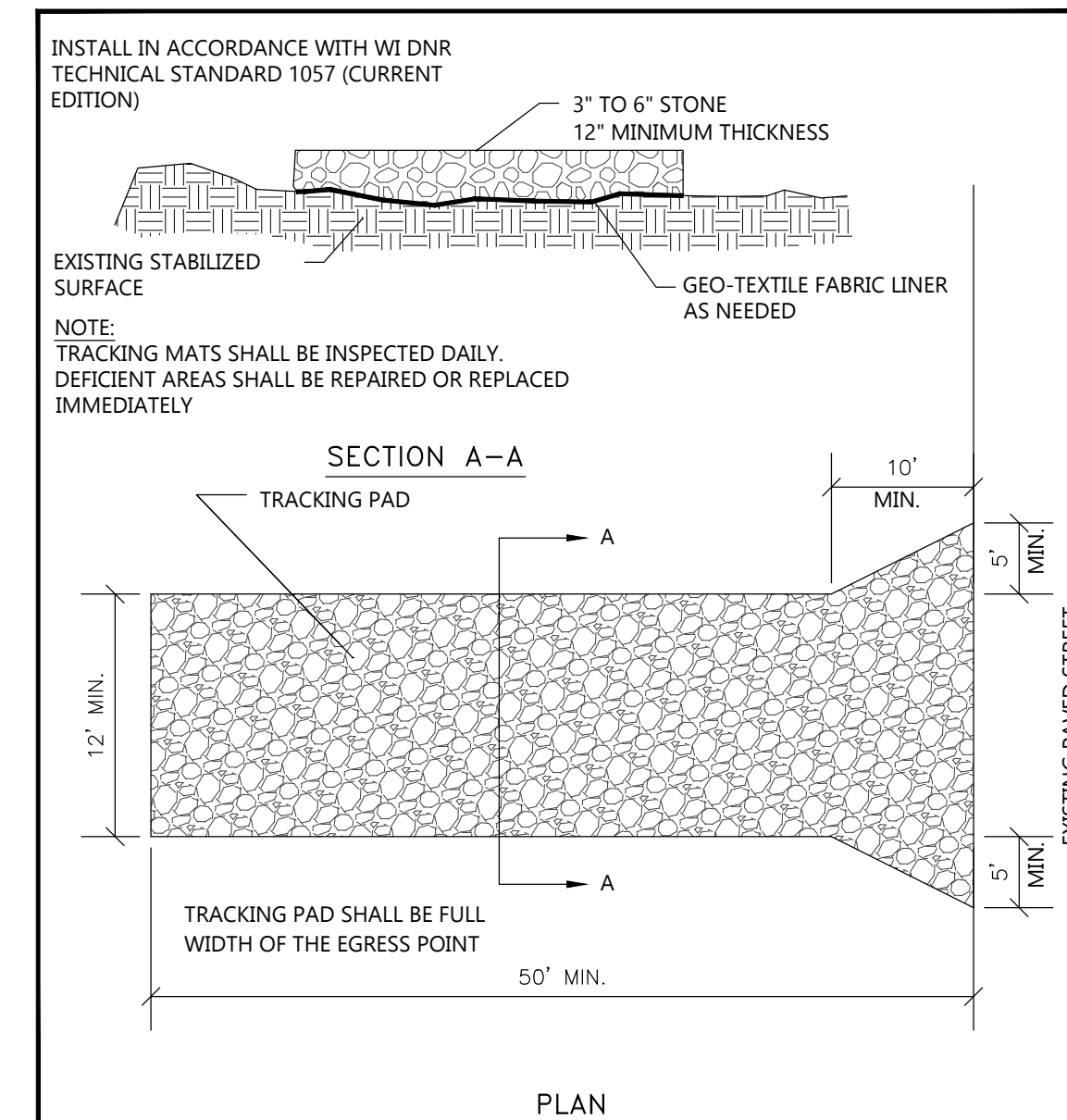
PROFILE VIEW

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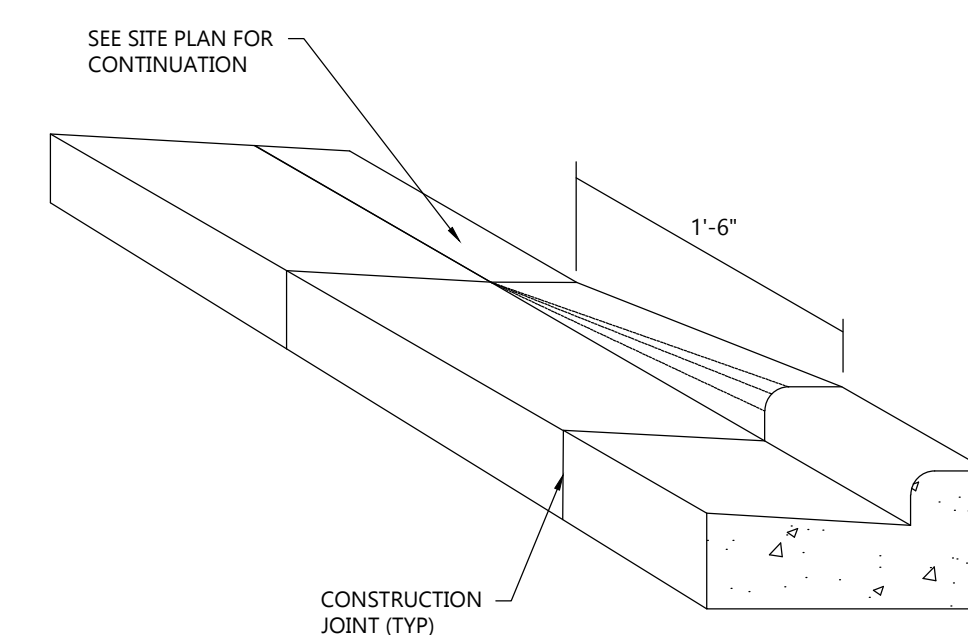
SILT FENCE - INSTALLATION DETAIL

NOT TO SCALE



TRACKPAD DETAILS

NOT TO SCALE



STOP SIGN WITH CONCRETE BASE DETAIL

NOT TO SCALE

HANDICAP SIGNAGE WITH CONCRETE BASE DETAIL

NOT TO SCALE

CIVIL DETAILS

EXCEL
Always a Better Plan
100 Camelot Drive
Fond du Lac, WI 54935
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PRELIMINARY DATES

AUG. 30, 2024

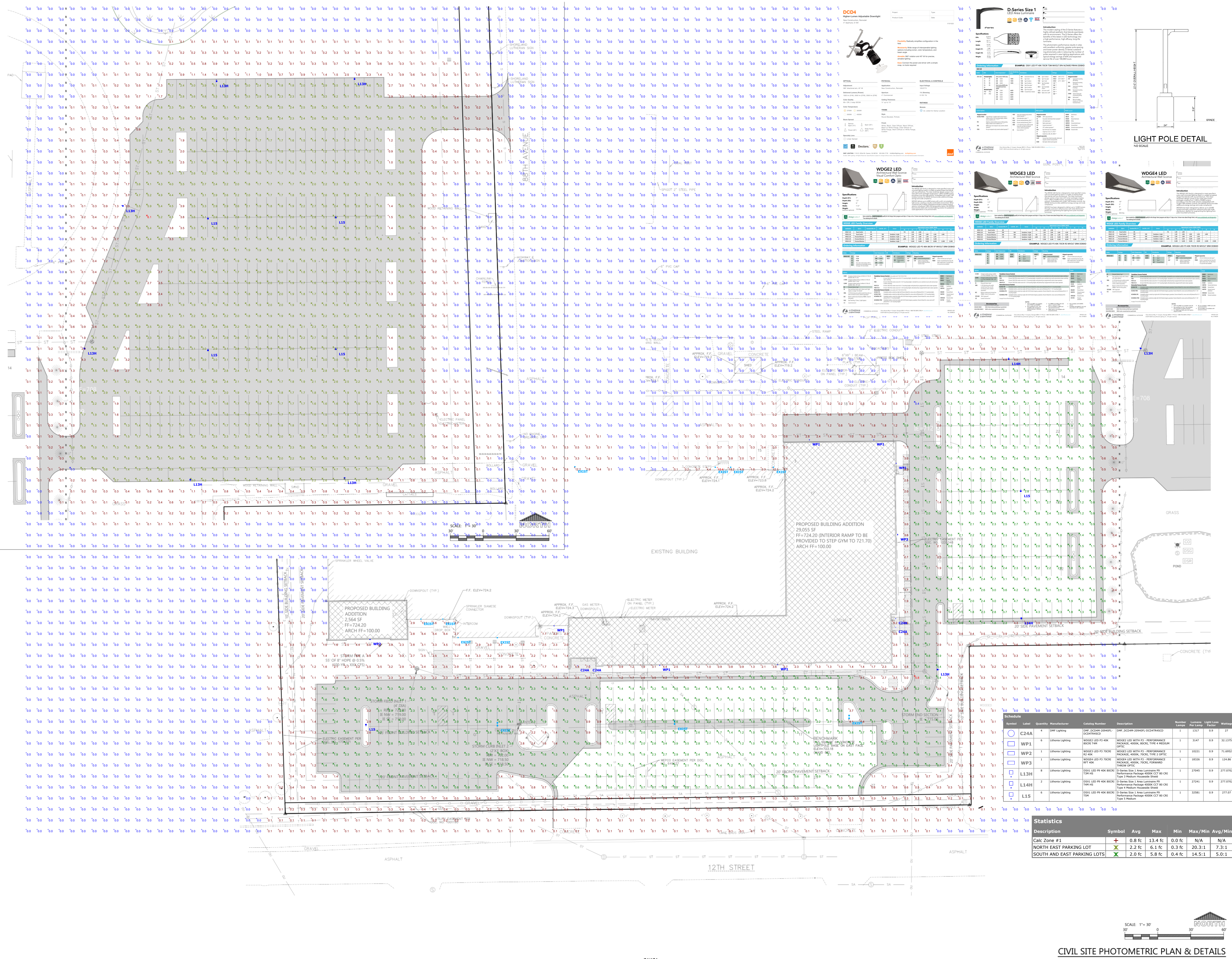
JOB NUMBER

240018700

SHEET NUMBER

C3.1

NOT FOR CONSTRUCTION



D-Series Size 1
 4' x 4' LED Luminaire

WEDGE LED
 4' x 4' LED Luminaire

WEDGE LED
 4' x 4' LED Luminaire

WEDGE LED
 4' x 4' LED Luminaire

Schedule

Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Number Lumens	Lumens Per Area	Factor	Wattage
C24A	DMF Lighting	4	DMF Lighting	DMF_DCDM2004PL_DCDM2004	DMF DCDM2004PL DCDM2004	1	1317	0.9	27
WP1	Lithonia Lighting	6	Lithonia Lighting	WLED3 LED P3 40K 80CK T8M	WLED3 LED WITH P3 - PERFORMANCE PACKAGE, 4000K, 80CK, TYPE 4 MEDIUM THROU OPTIC	1	3147	0.9	32.1375
WP3	Lithonia Lighting	1	Lithonia Lighting	WLED3 LED P3 40K 80CK T8M	WLED3 LED WITH P3 - PERFORMANCE PACKAGE, 4000K, 80CK, TYPE 4 MEDIUM THROU OPTIC	1	10221	0.9	71.6952
L13H	Lithonia Lighting	8	Lithonia Lighting	DCS5 LED P3 40K 80CK T8M H5	DCS5 LED WITH P3 - PERFORMANCE PACKAGE, 4000K, 80CK, TYPE 4 OPTIC THROU OPTIC	1	18326	0.9	124.86
L14H	Lithonia Lighting	2	Lithonia Lighting	DCS5 LED P3 40K 80CK T8M H5	DCS5 LED WITH P3 - PERFORMANCE PACKAGE, 4000K, 80CK, TYPE 4 OPTIC THROU OPTIC	1	27045	0.9	277.0702
L15	Lithonia Lighting	6	Lithonia Lighting	DCS5 LED P3 40K 80CK T8M H5	DCS5 LED WITH P3 - PERFORMANCE PACKAGE, 4000K, 80CK, TYPE 4 OPTIC THROU OPTIC	1	27241	0.9	277.0702
L15	Lithonia Lighting	6	Lithonia Lighting	DCS5 LED P3 40K 80CK T8M H5	DCS5 LED WITH P3 - PERFORMANCE PACKAGE, 4000K, 80CK, TYPE 4 OPTIC THROU OPTIC	1	32591	0.9	277.07

Statistics

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Calc Zone #1	+	0.8 fc	13.4 fc	0.0 fc	N/A	N/A
NORTH EAST PARKING LOT	X	2.2 fc	6.1 fc	0.3 fc	20.3:1	7.3:1
SOUTH AND EAST PARKING LOTS	X	2.0 fc	5.8 fc	0.4 fc	14.5:1	5.0:1

Stormwater & Erosion Control Memo For: Shoreland Lutheran High School Somers, Wisconsin

Aug 30, 2024



Introduction

The proposed development includes an addition to the existing high school and a parking lot expansion. The project is in the village of Somers. The project site is located on the corner of 12th Street and 88th Avenue. The proposed project will include a proposed 29,055 sf gymnasium and classroom addition, a 2,564 sf choir addition, parking lot modifications, and a parking lot expansion to the east. The proposed site is 55.74 acres, and the project will disturb 5.40 acres.

Stormwater Management Requirements

Stormwater management for the proposed project and future site improvements is currently under review. A proposed regenerative stormwater conveyance system (RSC) has been designed and proposed to be installed along the north side of the site. Stormwater modeling for the RSC was done by Aqualis and construction drawings were completed by Excel Engineering. See Aqualis stormwater management plan dated May 17, 2024, and Excel Engineering stormwater and mass grading plans dated June 11, 2024.

Storm Sewer Design

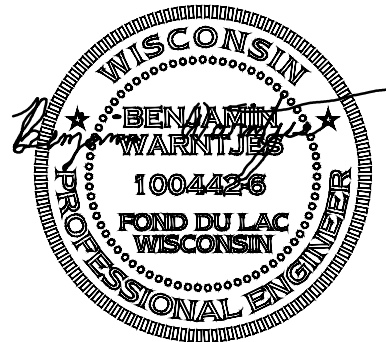
All storm sewers have been designed using DSPS plumbing code section 382.36(5). See attachments for pipe drainage areas and pipe sizing calculations.

Erosion Control

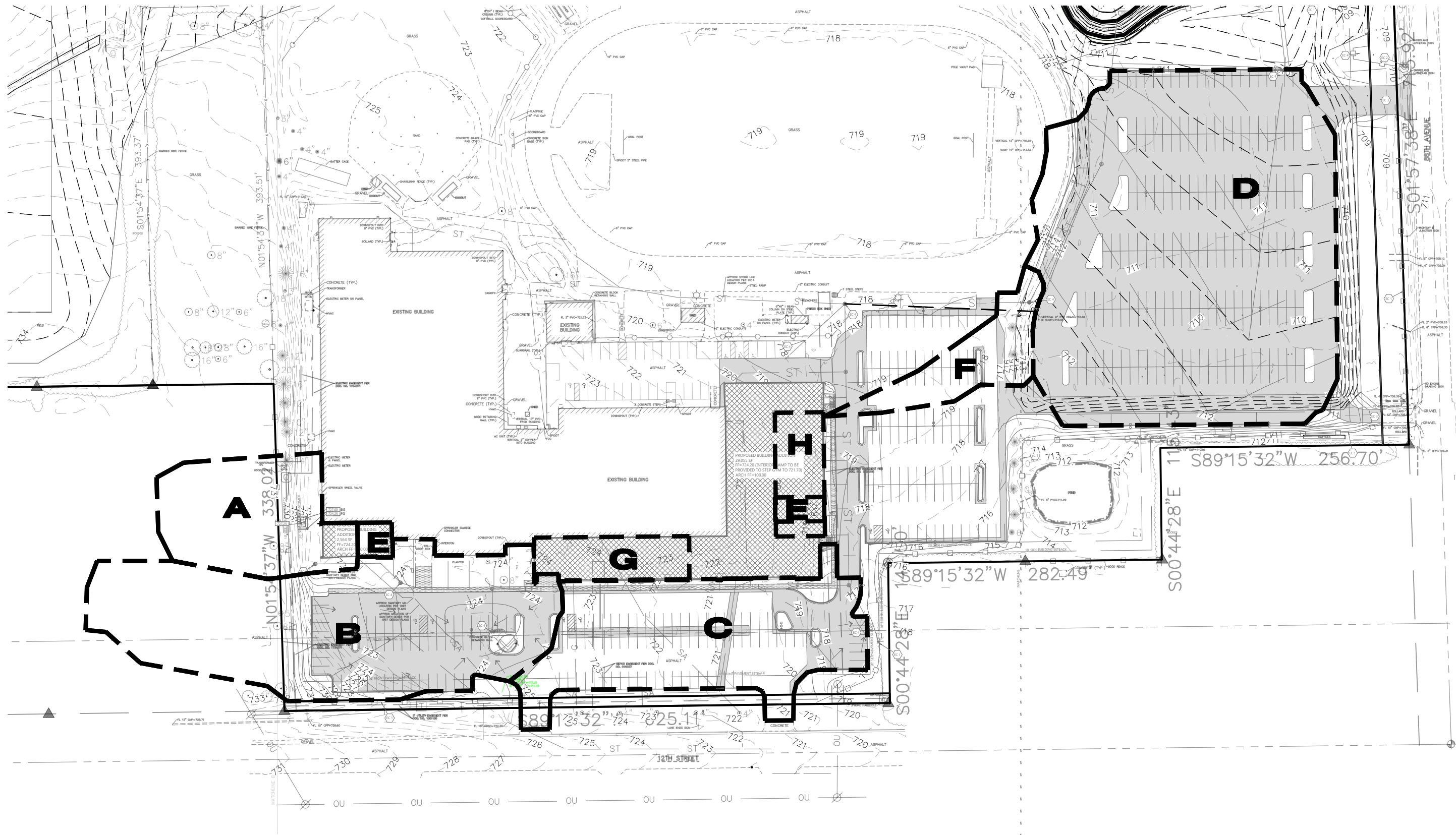
The erosion control specifications, site stabilization notes, dewatering notes, and post construction and maintenance plan will be included on sheet C0.1 of the construction plan set. See sheet L1.3 for seeding notes.

Attachments

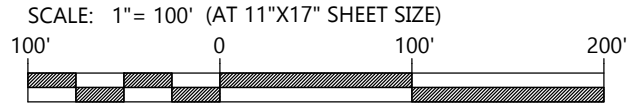
Storm Sewer Basin Map
DSPS Peak Discharge Calculations
Storm Sewer Spreadsheet



Storm Sewer Basin Map



STORM SEWER BASIN MAP



DSPS Peak Discharge Calculations

BASIN ID	PERCENT IMPERVIOUS	TOTAL (S.F.)	TOTAL (AC.)	IMP TOTAL (S.F.)	IMP TOTAL (AC.)	BUILDING (S.F.)	BUILDING (AC.)	DSPS (GPM)	DSPS (CFS)
A	100.00%	21,100	0.48	21,100	0.48	21,100	0.48	812	1.81
B	100.00%	59,200	1.36	59,200	1.36	59,200	1.36	2,277	5.07
C	100.00%	38,800	0.89	38,800	0.89	38,800	0.89	1,492	3.33
D	100.00%	107,300	2.46	107,300	2.46	107,300	2.46	4,127	9.20
E	100.00%	1,300	0.03	1,300	0.03	1,300	0.03	50	0.11
F	100.00%	10,600	0.24	10,600	0.24	10,600	0.24	408	0.91
G	100.00%	7,100	0.16	7,100	0.16	7,100	0.16	273	0.61
H	100.00%	6,500	0.15	6,500	0.15	6,500	0.15	250	0.56
EX1	100.00%	81,900	1.88	81,900	1.88	81,900	1.88	3,150	7.02

Storm Sewer Spreadsheet

Pipe Data					Pipe Capacity DSPS				
Pipe ID	Diameter (FT)	Pipe Qty.	Slope (FT/FT)	Manning's n	Basin ID	Total Flow (cfs)	Total Flow (gpm)	Full Flow Capacity (cfs)	Full Flow Capacity (gpm)
A	1	1	0.010	0.012	A,E	1.92	862	3.87	1,737
B	1.5	1	0.010	0.012	B	5.07	2,277	11.41	5,121
C	2	1	0.009	0.012	A,B,E,G,G	8.21	3,685	23.31	10,463
D	2	1	0.009	0.012	A,B,C,H,G,G	11.98	5,377	23.31	10,463
E	1.25	1	0.010	0.012	C	3.33	1,492	7.02	3,149
F	2	1	0.009	0.012	A,B,C,H,G,G,E,E,E	12.31	5,527	23.31	10,463
G	2.5	1	0.010	0.012	A,B,C,H,G,G,E,E,E,EX1,EX1	26.35	11,827	44.55	19,996
H	2	1	0.020	0.012	A,B,C,H,G,G,H,EX1,EX1,F	27.48	12,335	34.75	15,597
I	1.5	1	0.010	0.012	D	9.20	4,127	11.41	5,121
J	0.5	1	0.010	0.012	E	0.11	50	0.61	274
EX1	1.25	1	0.010	0.012				7.02	3,149

Full Flow Capacity based off Manning's Equation

$$Q = \frac{1.49}{n} R^{2/3} S^{1/2} a$$

Where:

- Q = Full Flow Capacity of Pipe (cfs)
- n = manning's roughness coefficient
- R = hydraulic radius (ft) (D/4)
- s = hydraulic gradient, slope (ft/ft)
- a = flow area (sq. ft.)

Typical Manning's n

- HDPE 0.012
- PVC 0.012
- Concrete 0.013
- CMP 0.024

*Total Flow calculated via TR-55 hydrologic calculations. Reference Storm Pipe Basin Map & TR-55 Calculations

PROJECT INFORMATION

PROPOSED BUILDING ADDITION PHASE ONE FOR:
SHORELAND LUTHERAN HIGH SCHOOL
9026 12TH STREET • KENOSHA, WI 53144

PROFESSIONAL SEAL

PRELIMINARY DATES
AUG. 30, 2024

NOT FOR CONSTRUCTION

JOB NUMBER
240018700

SHEET NUMBER

A1.1

LAB CLASSROOM
1,582 S.F.
(REMODEL)

MUSIC ADDITION
TOTAL AREA
2,548 S.F.
(CONVENTIONAL STEEL)
(LOAD BEARING CMU)

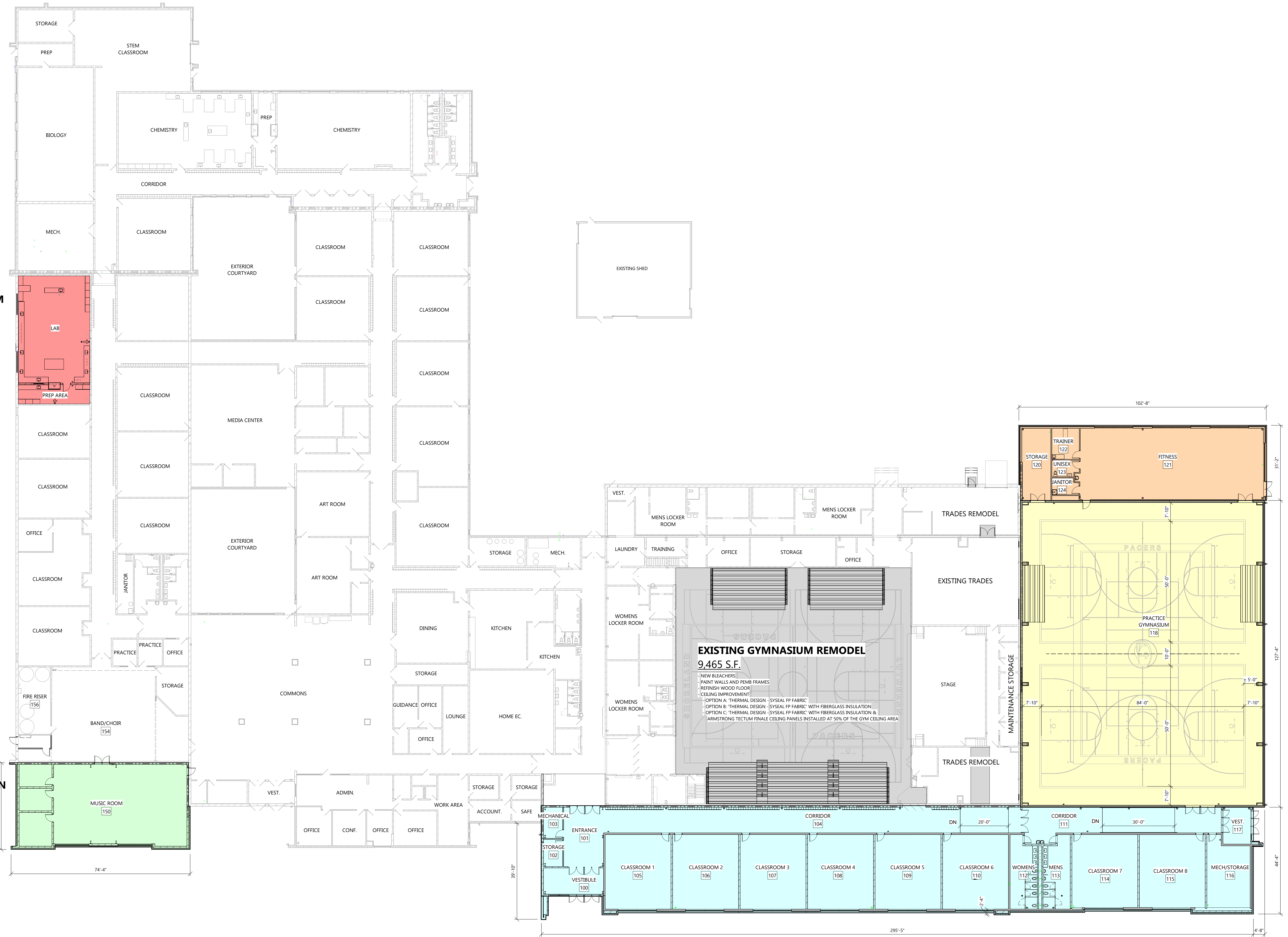
FITNESS CENTER
3,179 S.F.
(CONVENTIONAL STEEL)
(METAL STUD WALLS)

NEW GYMNASIUM
12,903 S.F.
PRECAST WALLS w/
PREMANUFACTURED METAL
BUILDING STRUCTURE
(PEMB)

CLASSROOM ADDITION
12,939 S.F.
(CONVENTIONAL STEEL)
(LOAD BEARING PRECAST)

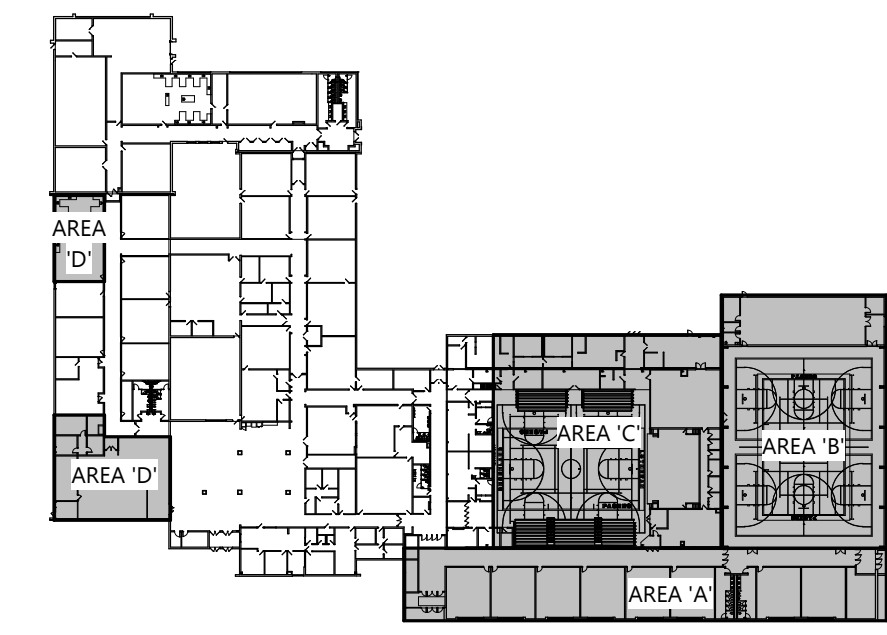
EXISTING GYMNASIUM REMODEL
9,465 S.F.

- NEW BLEACHERS
- PAINT WALLS AND PEMB FRAMES
- REFRESH WOOD FLOOR
- CEILING IMPROVEMENT
- OPTION A: THERMAL DESIGN - SYSEAL FP FABRIC
- OPTION B: THERMAL DESIGN - SYSEAL FP FABRIC WITH FIBERGLASS INSULATION
- OPTION C: THERMAL DESIGN - SYSEAL FP FABRIC WITH FIBERGLASS INSULATION & ARMSTRONG TECTUM FINALE CEILING PANELS INSTALLED AT 50% OF THE GYM CEILING AREA



OVERALL FIRST FLOOR PLAN
SCALE: 1/16" = 1'-0"

BUILDING ADDITION TOTAL AREA
31,569 S.F.



KEY PLAN
ARCHITECTURAL OVERALL FIRST FLOOR PLAN

PROJECT INFORMATION

PROPOSED BUILDING ADDITION PHASE ONE FOR:
SHORELAND LUTHERAN HIGH SCHOOL
 9026 12TH STEET • KENOSHA, WI 53144

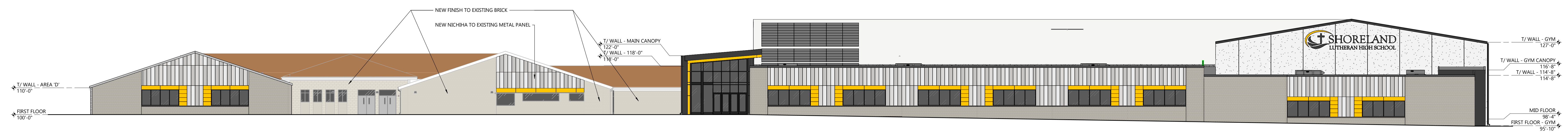
PROFESSIONAL SEAL

PRELIMINARY DATES
 AUG. 30, 2024

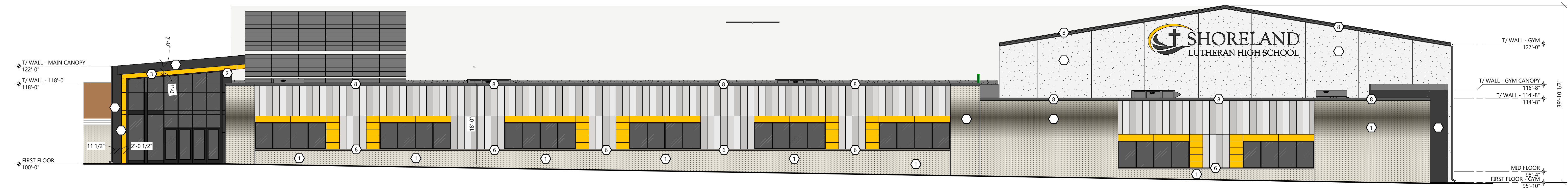
NOT FOR CONSTRUCTION

JOB NUMBER
 240018700

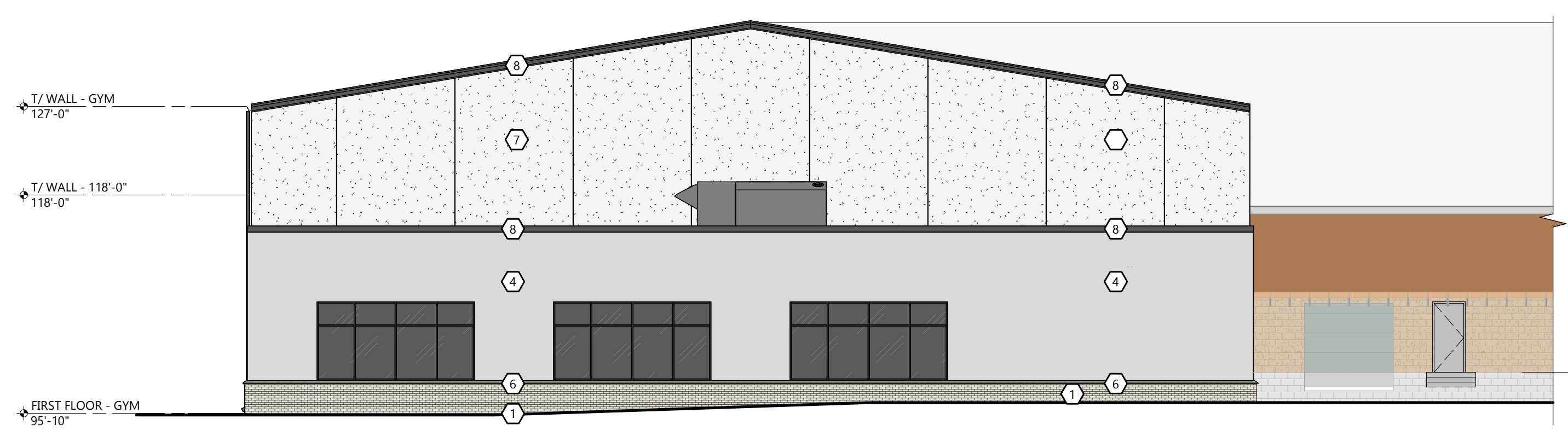
SHEET NUMBER
 A2.0



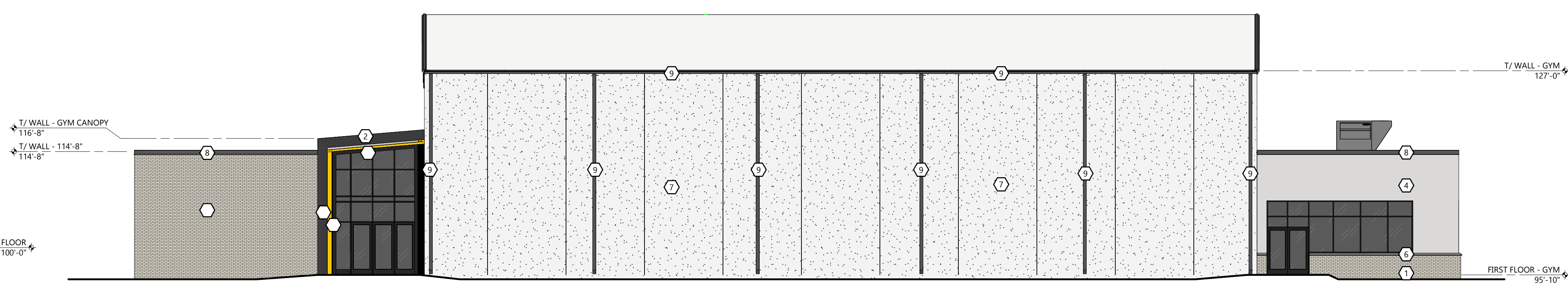
OVERALL SOUTH ELEVATION
 SCALE: 1/16" = 1'-0"



SOUTH ELEVATION
 SCALE: 3/32" = 1'-0"



NORTH ELEVATION
 SCALE: 3/32" = 1'-0"



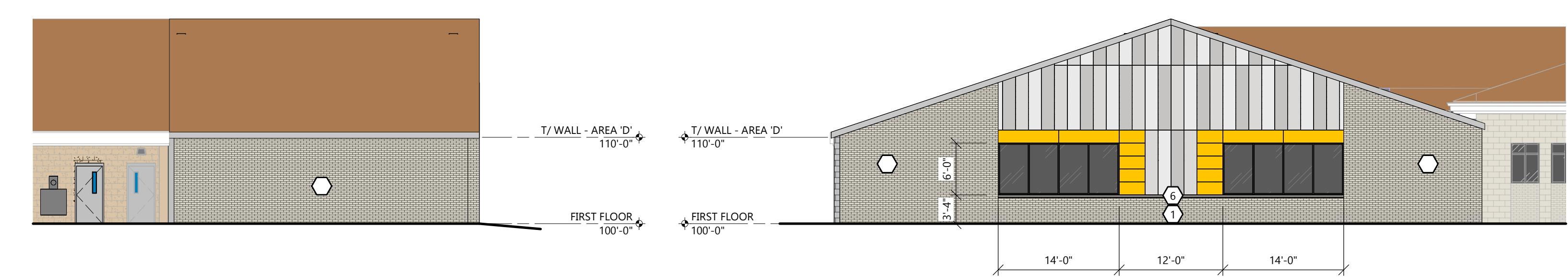
EAST ELEVATION
 SCALE: 3/32" = 1'-0"

GENERAL NOTES

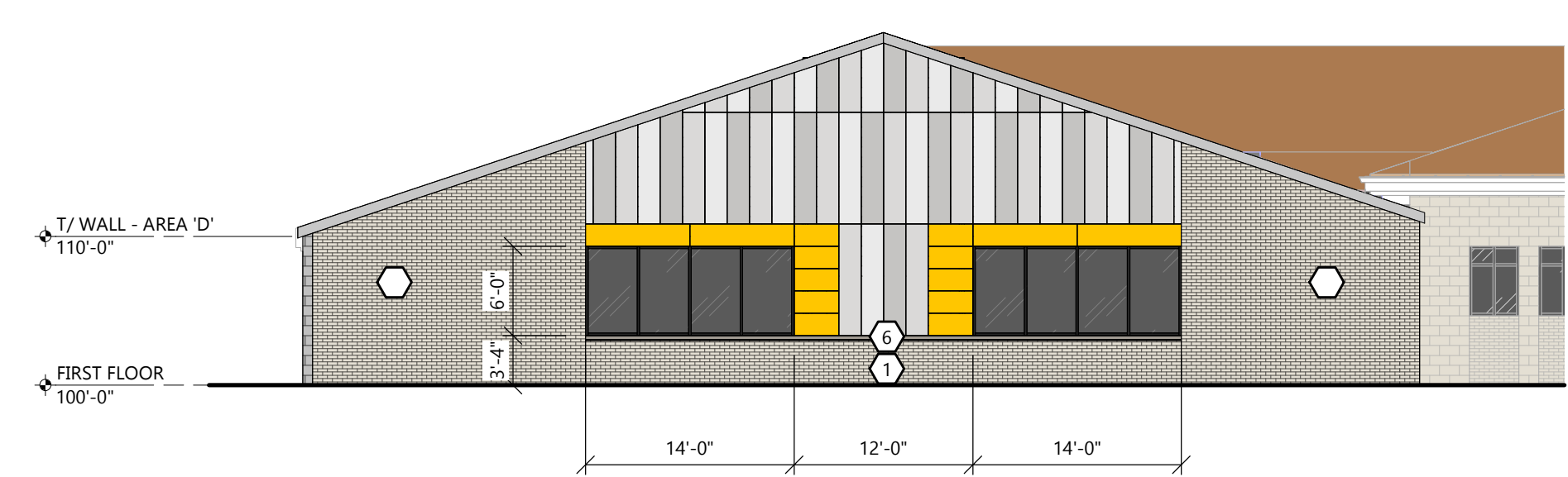
- SEE WALL SECTIONS FOR EXTERIOR WALL CONSTRUCTION
- REFERENCE EXTERIOR ELEVATIONS FOR VENEER CONTROL JOINTS. SEE STRUCTURAL PLANS FOR CMU CONTROL JOINT LOCATIONS. VENEER CONTROL JOINTS AND CMU CONTROL JOINTS ARE NOT REQUIRED TO ALIGN.

EXTERIOR MATERIAL KEY

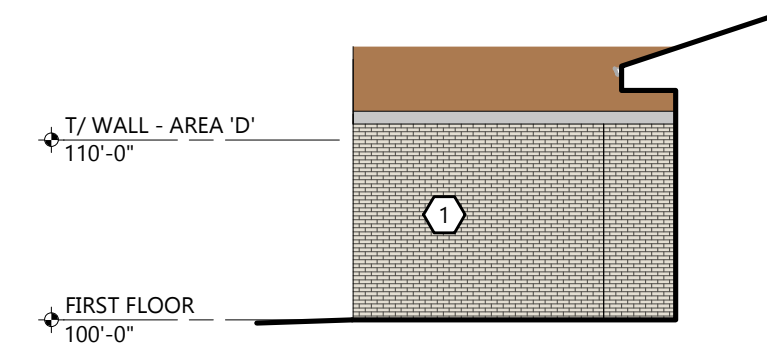
1	BRICK VENEER MFR: T.B.D. PRODUCT: T.B.D. COLOR: SEE RENDERING
2	ALUMINUM COMPOSITE MATERIALS MFR: MUZA PRODUCT: MZ-2000 COLOR: BLACK
3	ALUMINUM COMPOSITE MATERIALS MFR: MUZA PRODUCT: MZ-2000 COLOR: YELLOW
4	FIBER CEMENT PANELS MFR: NICHHA PRODUCT: ILLUMINATION DESIGN SERIES (AWP 3030) VERTICAL COLOR: LIGHT GRAY, DARK GRAY, WHITE PATTERN
5	FIBER CEMENT PANELS MFR: NICHHA PRODUCT: ILLUMINATION DESIGN SERIES (AWP 3030) HORIZONTAL COLOR: YELLOW
6	PRECAST SILL MFR: T.B.D. PROFILE: SEE DETAILS COLOR: SEE RENDERING
7	PRECAST WALL PANEL MFR: T.B.D. FINISH: PAINTED COLOR: WHITE
8	METAL COPING / FLASHING MFR: PAC-CLAD PROFILE: SEE DETAILS COLOR: BLACK
9	GUTTERS / DOWNSPOUTS MFR: T.B.D. PROFILE: SEE ROOF PLAN COLOR: BLACK



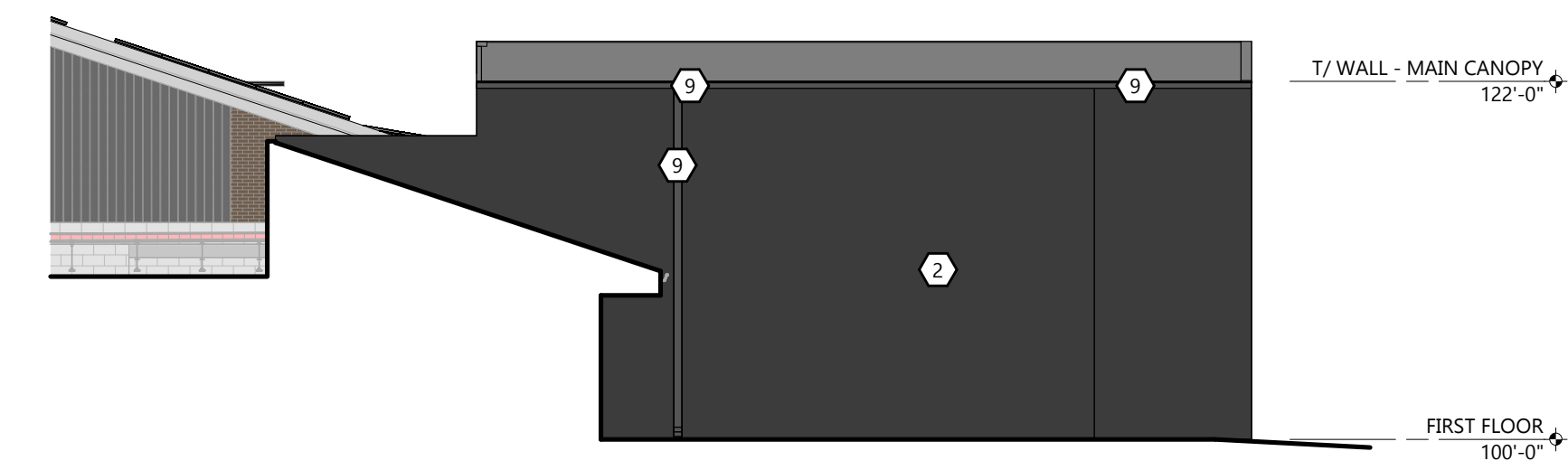
WEST ELEVATION - AREA 'D'
 SCALE: 3/32" = 1'-0"



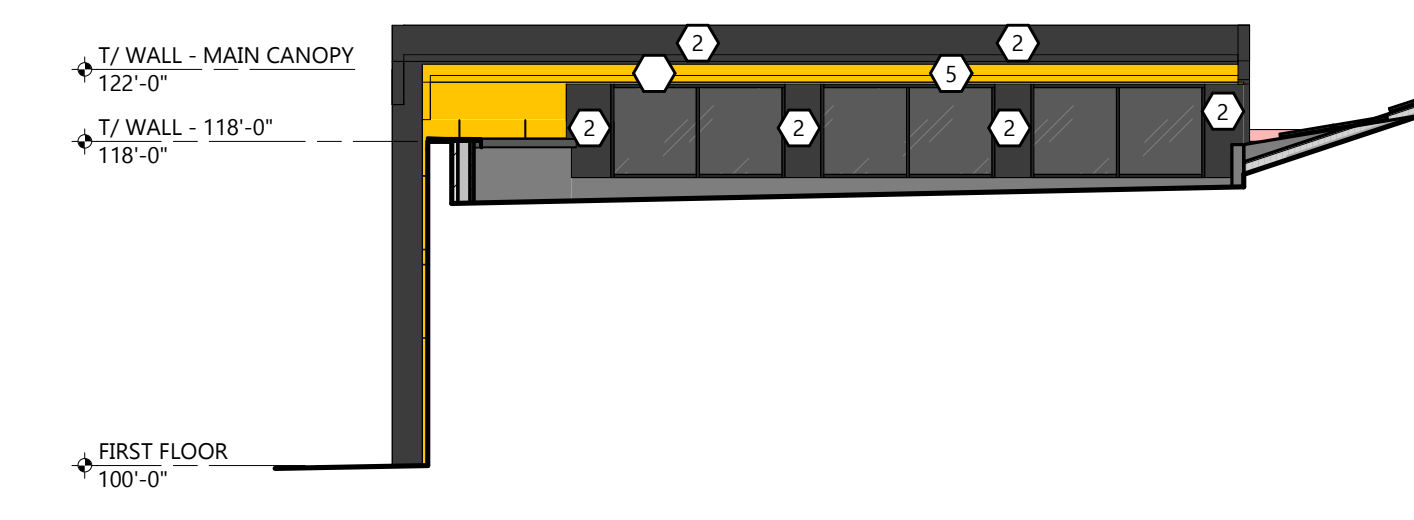
SOUTH ELEVATION - AREA 'D'
 SCALE: 3/32" = 1'-0"



EAST ELEVATION - AREA 'D'
 SCALE: 3/32" = 1'-0"



WEST ELEVATION - ENTRY
 SCALE: 3/32" = 1'-0"



EAST ELEVATION - ENTRY
 SCALE: 3/32" = 1'-0"

DIGGERS HOTLINE
 TO OBTAIN LOCATIONS OF PARTICIPANTS UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN
 CALL DIGGERS HOTLINE
 811 OR 1-800-442-8811
 MILWAUKEE AREA 226-1181
 MSJ STATUTE 192.07(10) (10/04)
 REQUIRES MIN. 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE

HELLER & ASSOCIATES, LLC
 LANDSCAPE ARCHITECTURE
 P.O. Box 1359
 Lake Geneva, Wisconsin 53147-1359
 ph 262.639.9733
 david@wdavidheller.com
 www.wdavidheller.com

PROJECT
SHORELAND LUTHERAN HIGH SCHOOL
 9026 12th Street
 Somers, WI 53144

ISSUANCE AND REVISIONS

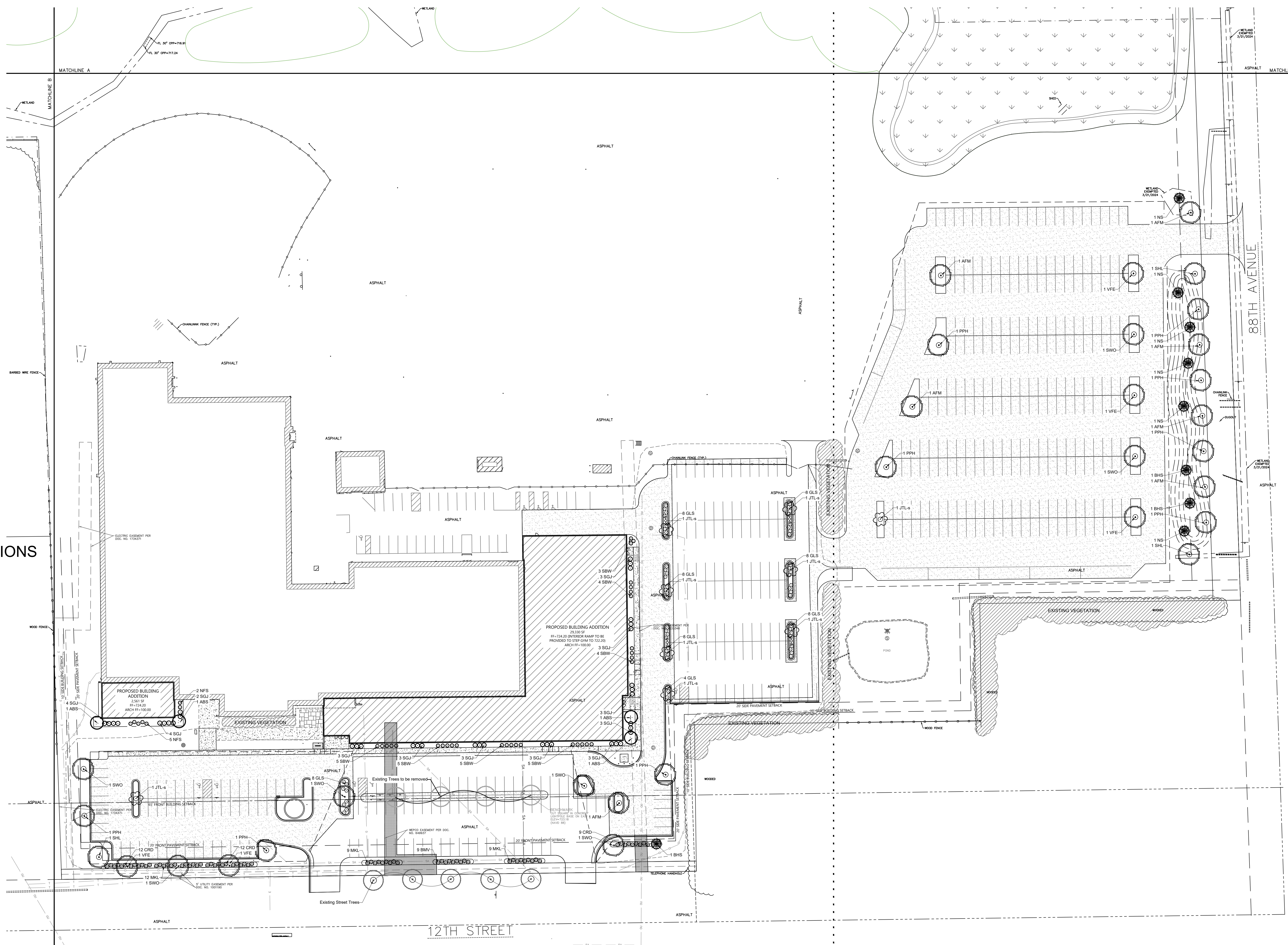
DATE	DESCRIPTION
8.27.24	FIRST ISSUE

Information contained herein is based on Survey Information, Field Inspection, and believed to be accurate.

SHEET TITLE
OVERALL LANDSCAPE PLAN

PROJECT MANAGER WDH
PROJECT NUMBER 24-042
DATE 08.27.24
SHEET NUMBER

L 1.0

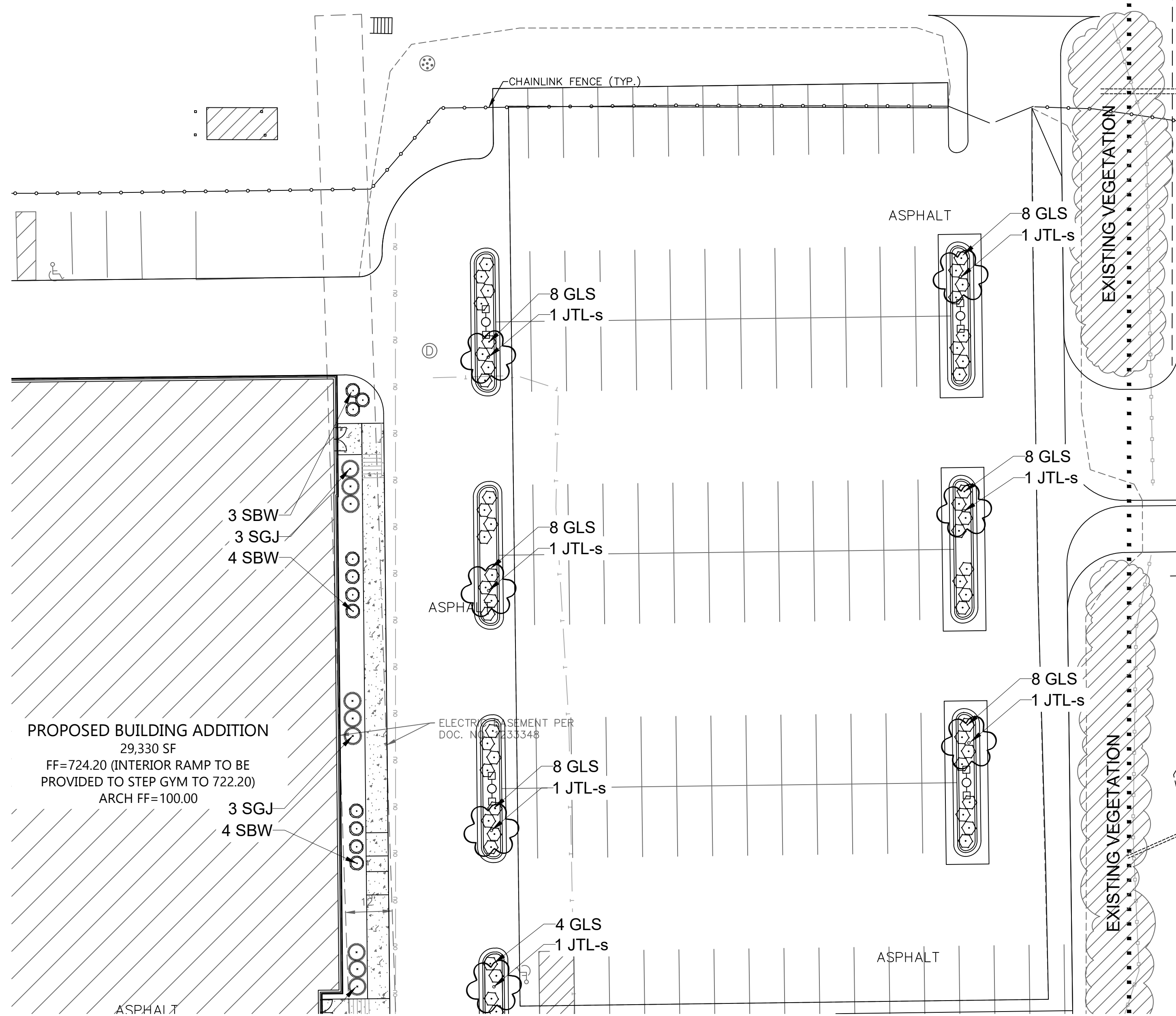


PLANT ABBREVIATIONS

OVERALL LANDSCAPE PLAN
 Scale: 1" = 40'0"
 NORTH



PLANT ABBREVIATIONS



ASPHALT

BASEBALL SCOREBOARD

EXISTING VEGETATION

EXISTING VEGETATION

POND

WOOD RETAINING WALL

EXISTING VEGETATION

WETLAND EXEMPTED 3/21/2024

88TH AVENUE

HIGHWAY JUNCTION SIGN

SHORELAND LUTHERAN SIGN

CHAINLINK FENCE

DUGOUT

WETLAND EXEMPTED 3/21/2024

ASPH

NO ENGR BREAKING

HELLER & ASSOCIATES, LLC
LANDSCAPE ARCHITECTURE
P.O. Box 1359
Lake Geneva, Wisconsin 53147-1359
ph 262.639.9733
david@wdavidheller.com
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PROJECT
SHORELAND LUTHERAN HIGH SCHOOL

9026 12th Street
Somers, WI 53144

ISSUANCE AND REVISIONS

DATE	DESCRIPTION
8.27.24	FIRST ISSUE

Information contained herein is based on Survey Information, Field Inspection, and believed to be accurate.

SHEET TITLE
ENLARGED LANDSCAPE PLAN

PROJECT MANAGER	WDH
PROJECT NUMBER	24-042
DATE	08.27.24
SHEET NUMBER	

ENLARGED LANDSCAPE PLAN
Scale: 1" = 20'0"
NORTH
0 10 20 40

DIGGERS HOTLINE

CALL DIGGERS HOTLINE
811 - 800-4-A-HEAD
M-F 8AM-5PM
NO. 800-4-A-HEAD
REQUIRES MIN. 7-WORK DAY
NOTICE BEFORE YOU EXCAVATE

ELECTRIC EASEMENT PER
DOC. NO. 1734371

PLANT ABBREVIATIONS

PROPOSED BUILDING
ADDITION
2,561 SF
FF=724.20
ARCH FF=100.00

PROPOSED BUILDING ADDITION
29,330 SF
FF=724.20 (INTERIOR RAMP TO BE
PROVIDED TO STEP GYM TO 722.20)
ARCH FF=100.00

2 NFS
2 SGJ
1 ABS
4 SGJ
5 NFS

3 SBW
3 SGJ
4 SBW

3 SGJ
4 SBW

3 SGJ
1 ABS
3 SGJ

8 GLS
1 JTL-s

8 GLS
1 JTL-s

8 GLS
1 JTL-s

4 GLS
1 JTL-s

ELECTRIC EASEMENT PER
DOC. NO. 1734371

1 PPH
1 SHL

12 CRD
1 VFE

12 MKL
1 SWO

5' UTILITY EASEMENT PER
DOC. NO. 1001190

1 PPH

12 CRD
1 VFE

3 SGJ
5 SBW

3 SGJ
5 SBW

3 SGJ
5 SBW

3 SGJ
5 SBW

3 SGJ
1 ABS

1 SWO

1 AFM

9 CRD
1 SWO

9 MKL

9 BMV

9 MKL

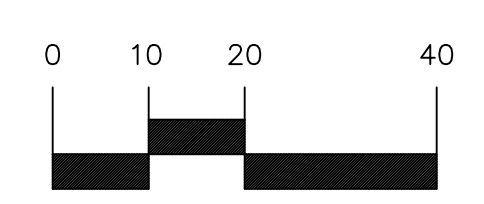
1 BHS

12TH STREET



ENLARGED LANDSCAPE PLAN

Scale: 1" = 20'0"



HELLER & ASSOCIATES, LLC
LANDSCAPE ARCHITECTURE

P.O. Box 1359
Lake Geneva, Wisconsin 53147-1359
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PROJECT

SHORELAND LUTHERAN HIGH SCHOOL

9026 12th Street
Somers, WI 53144

ISSUANCE AND REVISIONS

DATE	DESCRIPTION
8.27.24	FIRST ISSUE

SHEET TITLE

ENLARGED LANDSCAPE PLAN

PROJECT MANAGER	WDH
PROJECT NUMBER	24-042
DATE	08.27.24
SHEET NUMBER	

L 1.2

VILLAGE OF SOMERS

Department of Planning and Development

VILLAGE OF SOMERS SITE PLAN REVIEW PROCEDURES

1. Contact the Kenosha County Department of Planning & Development and check with staff to determine if your proposed zoning change meets the requirements of the Multi-Jurisdictional Comprehensive Plan for Kenosha County: 2035, Village of Somers General Zoning and Shoreland/Floodplain Zoning Ordinance and the Village of Somers Land Division and Platting Control Ordinance.

2. Contact the Kenosha County Department of Planning & Development and schedule a pre-conference meeting, which is required for all site plan review requests.

Meeting Date: _____

3. Contact the Village of Somers to determine if your site plan review application requires concept review by the Village Plan Commission. If so, contact the Village of Somers clerk to schedule a concept meeting with the Village Plan Commission.

Meeting Date: _____

4. Complete and submit to the Kenosha County Department of Planning & Development the Village of Somers Site Plan Review Application by the filing deadline.

Filing Deadline: _____

5. Upon submission you will be given two copies of the date-stamped application. Submit a copy of the date-stamped application to the Village of Somers clerk for placement on the agendas of the Village of Somers Plan Commission and the Village of Somers Board. Keep the other copy for your records.

7. Attend the Village Plan Commission and the Village Board meetings. **NOTE:** You must attend or the Village will not be able to act on your request. At these meetings you will be asked to brief the committee on your request.

Village Plan Commission meeting date (tentative): _____

Village Board meeting date (tentative): _____

8. Village clerk will provide written notice of final action to property owner/applicant.

SITE PLAN REVIEW
CHECKLIST

Owner: Shoreland Lutheran High School Federation, Inc Date 8/30/24

Mailing Address: 9206 21st Street Phone # 262-515-4527

Kenosha, WI 53144 Phone # _____

Agent: Excel Engineering Inc Phone # _____

Mailing Address: 100 Camelot Dr Phone # 920-322-1575

Fond du Lac, WI 54935

Architect/Engineer: Excel Engineering Inc - Ben Warntjes Phone # 920-322-1575

Mailing Address: 100 Camelot Dr Phone # _____

Fond du Lac, WI 54935

Tax Parcel Number(s): 82-4-222-084-0272 Acreage of Project: _____

Existing Zoning: I-1 Institutional Proposed Zoning: I-1 Institutional

Conditional Use Permit: CUP is required for the gym

Description of Project: (include the following when applicable):

Description of project: Gym and classroom addition on the east side of the building , choir room addition to the southwest corner, parking lot modifications and parking lot expansion to the east.

Size of existing building(s): 88,202 SF

Size of new building(s) and/or addition(s): 31,619 SF

Number of current and projected full-time and part-time employees, number of shifts: _____

60 current, 10 proposed. All full-time.

Number of proposed units: _____ Description of units: _____

Density: _____

Plat of Survey Submitted:

Covenants and Restrictions Submitted

A. BUILDING PLANS SUBMITTED? (BUILDING APPEARANCE)

- x No building shall be permitted the design or exterior appearance of which is of such unorthodox or abnormal character in relation to its surroundings as to be unsightly or offensive to generally accepted taste and community standards.
- x No building shall be permitted the design or exterior appearance of which is so identical with those adjoining as to create excessive monotony or drabness.
- x No building shall be permitted where any exposed facade is not constructed or faced with a finished material which is aesthetically compatible with the other facades and presents an attractive appearance to the public and to surrounding properties.
- x Building Scale and Mass. The relative proportion of a building to its neighboring buildings, to pedestrians and observers, or to other existing buildings shall be maintained or enhanced when new buildings are built or when existing buildings are remodeled or altered.
- x Building Rooflines and Roof Shapes. The visual continuity of roofs and their contributing elements (parapet walls, coping, and cornices) shall be maintained in building development and redevelopment.
- x Since the selection of building colors has a significant impact upon the public and neighboring properties, color shall be selected in general harmony with existing neighborhood buildings.
- x No building or sign shall be permitted to be sited on the property in a manner which would unnecessarily destroy or substantially damage the natural beauty of the area, particularly insofar as it would adversely affect values incident to ownership of land in that area, or which would unnecessarily have an adverse effect on the beauty and general enjoyment of existing structures on adjoining properties.
- x The facade of all buildings which face upon a street right-of-way shall be finished with an aesthetically pleasing material. A minimum of 50 percent of a facade facing an existing or future street shall be finished with a combination of brick, decorative masonry material, decorative pre-cast concrete panels, decorative glass panels, wood, or decorative metal or vinyl siding. Such finished material shall extend for a distance of at least 10 feet along the sides of the structure. All buildings on corner lots shall have the required finished facade facing each street.

B. SITE PLAN SUBMITTED?

- x Building locations shall maintain required setbacks from property lines and road rights-of-way.
- x Buildings and uses shall provide for safe traffic circulation and safe driveway locations.
- x Buildings and uses shall be provided with adequate public services as approved by the appropriate utility.

**SITE PLAN REVIEW
CHECKLIST**

- x Buildings and uses shall provide adequate parking and loading areas. No loading dock or overhead doors shall face upon a street right-of-way without approval of the zoning administrator.

- x Parking areas shall maintain required setbacks and parking spaces shall be of minimum required size (9' x 18'). Aisle widths within parking lots shall be a minimum of 24' between the ends of the parking spaces.

- x Each parking space shall be on the same lot or parcel as the principal use, and all parking lots shall have the same zoning district as the principal use. Parking spaces and driveways shall be a minimum of 20 feet from the established highway right-of-way and a minimum of 10 feet from all rear and side lot lines. Curbs or barriers shall be installed so as to prevent vehicles from extending beyond designated parking or driving areas.

- x Parking areas for five or more vehicles adjoining a residential use, shall be screened from such use by a solid wall, fence, evergreen planting of equivalent visual density or other effective means, and built and maintained at a minimum height of four feet at the time of planting or installation.

- x Adequate access to a public or private roadway shall be provided for each parking space. For all non-residential properties, driveways for vehicular ingress and egress shall be a minimum of 10 feet in width and not exceed 35 feet in width at the property line.

- x All off-street parking of more than five vehicles shall be graded and surfaced with asphalt or concrete.

C. LANDSCAPING AND LIGHTING PLAN SUBMITTED?

- x Buildings and uses shall make appropriate use of open spaces. The zoning administrator or Village Board may require appropriate landscaping and planting screens. A landscaping maintenance program, together with appropriate assurances, shall be submitted.

- x Dumpsters and other trash receptacles shall be fenced and/or screened from view from street rights-of-way and adjacent residential uses.

- x Exterior lighting used for parking lots, recreation facilities, product display, and security shall not spill-over on operators of motor vehicles, pedestrians, and/or adjacent residential property (cut-off type luminaries only).

Appropriate buffers shall be provided between dissimilar uses.

D. SEWER VERIFICATION/SANITATION EVALUATION OR APPLICATION SUBMITTED?

E. UTILITY PLANS SUBMITTED?

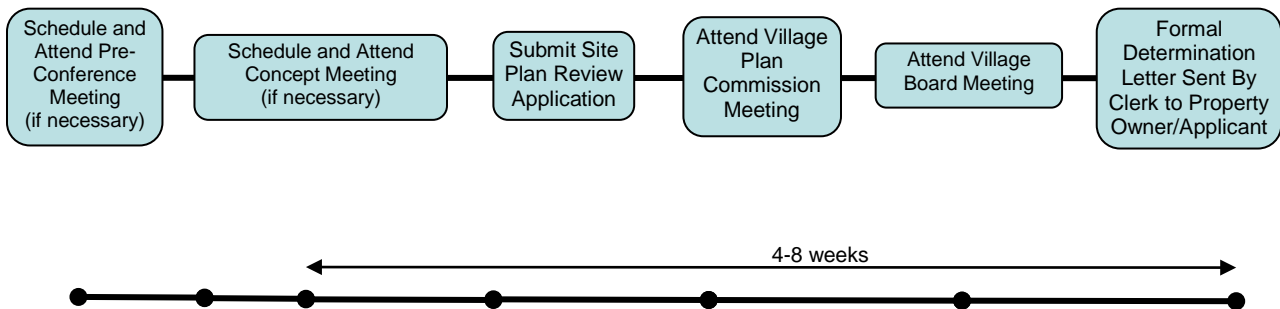
F. STORMWATER DRAINAGE AND EROSION CONTROL PLAN SUBMITTED?

Appropriate erosion control and stormwater management measures shall be utilized in all new development. Buildings and uses shall maintain existing topography, drainage patterns, and vegetative cover insofar as is practical. The zoning administrator or Village Board may require that drainage easements be executed.

IMPORTANT TELEPHONE NUMBERS

Kenosha County Center	
Department of Planning & Development	
19600 - 75 th Street, Post Office, Suite 185-3	
Bristol, Wisconsin 53104-9772	
Division of County Development (including Sanitation & Land Conservation).....	857-1895
Facsimile #.....	857-1920
Public Works Division of Highways	857-1870
Administration Building	
Division of Land Information.....	653-2622
Village of Somers	859-2822
Wisconsin Department of Natural Resources - Sturtevant Office	884-2300
Wisconsin Department of Transportation - Waukesha Office	548-8722

Site Plan Review Procedure Timeline



For Reference Purposes



Shoreland Lutheran High School Traffic Impact Analysis

Village of Somers
Kenosha County, Wisconsin

October 24, 2024



TRAFFIC IMPACT STUDY FOR:

SHORELAND LUTHERAN HIGH SCHOOL

VILLAGE OF SOMERS, KENOSHA COUNTY, WISCONSIN

DATE SUBMITTED: October 24, 2024

PREPARED FOR:

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PREPARED BY:

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Contact Person: Donald J. Lee, P.E. (WisDOT TIA Certification # SE05-804-046)
John Bieberitz, P.E., PTOE (WisDOT TIA Certification # SE05-804-044)

“I certify that this Traffic Impact Analysis has been prepared by me or under my immediate supervision and that I have experience and training in the field of traffic and transportation engineering.”

Donald J. Lee, P.E.
Wisconsin Registration #35214-006
Traffic Analysis & Design, Inc.

**Shoreland Lutheran High School
Traffic Impact Analysis
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CHAPTER I – INTRODUCTION & EXECUTIVE SUMMARY

PART A – PURPOSE OF REPORT AND STUDY OBJECTIVES

Shoreland Lutheran High School is proposing an expansion to their existing high school located on the north side of County Trunk Highway (CTH) E/Somers Road, immediately west of 88th Avenue/CTH H within the Village of Somers, Kenosha County. The initial build includes expansion of the existing gymnasium as well as additional classroom space. Additional parking is also expected under the initial build scenario. As part of the full build-out plans, within the next 5 to 10 years, a football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) are also proposed in the western portion of the site.

TADI conducted this traffic impact analysis (TIA) to determine the expected weekday morning arrival and weekday afternoon dismissal peak hour operating conditions and recommendations at the study area intersections for the Existing, Background (with identified off-site development), Initial Build and Full Build (with on-site and off-site developments) traffic operating conditions. The current student population is expected to increase from the current enrollment of 450 students up to 475 students in the next few years and potentially up to 550 students by 2035.

This report documents the procedures, findings, and conclusions of the TIA. The analysis identifies recommended modifications based on existing intersection geometrics, existing traffic volumes, and additional traffic expected to be generated by the proposed Shoreland Lutheran High School and the previously approved off-site development.

PART B – EXECUTIVE SUMMARY

The executive summary includes a description of the study area, descriptions of the proposed on-site and off-site developments and conclusions based on the findings of the TIA.

B1. Location of Study Site with Respect to Area Roadway Network

A street map illustrating the location of the proposed Shoreland Lutheran High School campus is shown in [Exhibit 1-1](#) at the end of this chapter. As discussed with Kenosha County and the Village of Somers, the study area for the proposed school includes the following existing intersections:

- Somers Road/CTH E & 97th Avenue (one-way stop control)
- Somers Road/CTH E & Existing West School Driveway (one-way stop control)
- Somers Road/CTH E & Existing East School Driveway (one-way stop control)
- Somers Road/CTH E & 88th Avenue/CTH H (all-way stop control)
- 88th Avenue/CTH H & Existing South School Driveway (one-way stop control)

In addition to these existing intersections, the following proposed driveways were also included in the study:

- Somers Road/CTH E & Proposed Sports Field Driveway (one-way stop control)
- 88th Avenue/CTH H & Proposed North School Driveway (one-way stop control)

The Shoreland Lutheran High School expansion site is bordered by residential land uses to the north and northeast. Additional residential properties and neighborhoods exist on both the north and south sides of Somers Road to the east and west of the site. Agriculture/farming land uses

also exist beyond most of the residential areas in all directions. IH-94 exists further to the west with access ramps to the freeway located about 2 miles west of the school.

B2. On-Site Development Description

Shoreland Lutheran High School is proposing expansion of the existing gymnasium as well as additional classroom space. Some parking on the east side of the school is being displaced as part of the expansion plans; however, a new parking lot is proposed as part of the initial build scenario with about 254 new parking spaces as well as 5 bus stalls proposed on the northeast portion of the school site and 57 additional spaces to the southwest, in front of the school. Full buildout plans, within the next 5 to 10 years, include construction of a new football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) proposed to be located on the western portion of the site. Parking is also proposed near the future sports fields. Copies of the conceptual site plans for the initial build and future full build are included in [Exhibits 1-2A&B](#), respectively.

B3. Off-Site Development Description

One off-site development has been identified within the immediate study area. A residential development, the Beta Terra 45 Residential Neighborhood development, including 45 single family lots, has previously been approved for a parcel of land on the south side of Somers Road. Access to the site is proposed via a new access drive onto Somers Road, immediately east of 93rd Avenue.

B4. On-Site Generated Traffic

The hourly vehicular traffic volumes expected to be generated by the Shoreland Lutheran High School expansion are estimated based on trip rates derived using the existing driveway trips counted as part of this project. Using the existing student population and the traffic counts conducted for this study during a typical weekday in mid-September of 2024, weekday morning arrival and weekday afternoon discharge peak hour rates were calculated. To calculate the expected new daily trips, the daily rate for a private high school was taken from the Institute of Transportation Engineer's (ITE) *Trip Generation Manual, 11th Edition*.

Based on the assumptions above, the Shoreland Lutheran High School expansion is expected to generate about 20 new vehicular trips (15 in/5 out) during the weekday morning arrival peak hour and 10 new vehicular trips (5 in/5 out) during the weekday afternoon dismissal peak hour under Initial Build conditions. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 50 new vehicle trips (25 in/25 out) under initial build conditions.

Under the future year projected full build out, the High School expansion is expected to generate about 80 new vehicular trips (55 in/25 out) during the weekday morning arrival peak hour and 40 new vehicular trips (10 in/30 out) during the weekday afternoon dismissal peak hour. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 220 new vehicle trips (110 in/110 out) under the future year projected full build out.

B5. Off-Site Generated Traffic

The Beta Terra 45 Residential Neighborhood off-site development is expected to generate about 35 vehicular trips (10 in/25 out) during the weekday morning arrival peak hour, 45 vehicular trips (30 in/15 out) during the weekday afternoon dismissal peak hour and 500 new trips over a typical weekday (24-hour period) under full build out conditions. Trip generation and traffic

assignment information for the Beta Terra 45 Residential Neighborhood off-site developments was taken from the previously completed Beta Terra 45 Residential Neighborhood Development TIA dated June 24, 2021.

B6. Proposed Access to Shoreland Lutheran High School

Two new access points are proposed for the Shoreland Lutheran High School site. A new full access driveway, referred to as “North Driveway” is proposed along 88th Avenue/CTH H, about 350 feet north of the existing South Driveway onto CTH H. This new driveway, which is expected to provide access to a new northeast parking lot area on the east side of the school, is proposed as part of the initial build phase of the project. A second access driveway, referred to as “Sports Field Drive” is proposed under the full build conditions. This driveway is proposed to provide access to the sports fields proposed on the western portion of the site. Cross access between the school and the sports field is also proposed as part of the full build development plans; however, because of the proximity of the connection and the curved nature of the connection roadway, the Sports Field Drive is not expected to be utilized as a primary access for typical school day traffic. This second access is proposed as a full access driveway and is proposed to be located about 220 feet east of 97th Avenue.

B7. Recommended Modifications

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual (HCM) 6th Edition*. Intersection operation is defined by “level of service.” Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A,’ to very poor, represented by LOS ‘F.’ For the purpose of this study, LOS D or better was used to define acceptable peak hour operating conditions.

Modifications to address traffic impacts are included for the Existing, Background, and Build traffic conditions and have been shown for the following five scenarios:

- “Existing Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions, without the proposed school.
- “Year 2025 Background Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions plus full build out of the identified off-site development, without the proposed school.
- “Year 2025 Initial Build Traffic” – These modifications are expected to be necessary to accommodate the Initial Build traffic volumes, which includes the initial build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development.
- “Year 2035 Background Traffic” – These modifications are expected to be necessary to accommodate the future background traffic volume conditions which include typical background growth along the highways plus full build out of the identified off-site development, without the proposed school.
- “Year 2035 Full Build Traffic” – These modifications are expected to be necessary to accommodate the Full Build traffic volumes, which includes the full build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development, including typical background growth along the highways.

The analysis was conducted using existing intersection geometrics and traffic control. The following modifications, as shown in Exhibits 1-3 & 1-4, are recommended to accommodate the respective Background and Build traffic volume scenarios for the years 2025 and 2035, respectively. *Modifications are for jurisdictional consideration and are not legally binding. Kenosha County and the Village of Somers reserve the right to determine alternative solutions.*

Node 100: Somers Road/CTH E intersection with 97th Avenue

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 200: Somers Road/CTH E intersection with :Proposed Sports Field Drive

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:*
 - Provide a new full access drive on the north approach as shown on the site plan.
 - Provide stop sign control on the north approach.
 - Provide an eastbound bypass lane on the south side of CTH E at the new intersection.
 - Connect the existing westbound bypass lanes on the north side of CTH E between 93rd Avenue and 97th Avenue.

Node 300: Somers Road/CTH E intersection with West School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 400: Somers Road/CTH E intersection with East School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 500: Somers Road/CTH E intersection with 88th Avenue/CTH H

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:*
 - Consider providing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2025 Initial Build Traffic:* No additional modifications.
- *Year 2035 Background Traffic:*
 - If not previously constructed, consider constructing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
 - Consider providing an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection (see sensitivity analysis discussion below). If implemented, the additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2035 Full Build Traffic:* No additional modifications.

Node 600: 88th Avenue/CTH H intersection with North Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:*
 - Provide a new full access drive on the west approach as shown on the site plan.
 - Provide stop sign control on the west approach.
- *Year 2035 Background Traffic:* No additional modifications.
- *Year 2035 Full Build Traffic:* No additional modifications.

Node 700: Somers Road intersection with South School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

With a new north access driveway onto CTH H proposed, two access scenarios were evaluated as part of this study to analyze the operation of the two driveways along CTH H with and without the existing south school driveway operational. Regardless of the access scenario, both the north and south driveways are expected to operate acceptably at LOS B or better under both

weekday peak periods; therefore, since providing the south driveway is not expected to cause an unsafe condition, allowing the south access driveway is recommended to allow for additional access options to/from the site. However, if it is decided to remove the driveway, the north driveway is expected to operate acceptably as the sole access onto CTH H.

All intersections are expected to operate acceptably with the proposed initial build out of the Shoreland Lutheran High School campus with the recommended modifications. Without the recommended modifications under the current year 2025 background traffic conditions, longer delays and queues are expected for the eastbound and westbound movements at the all-way stop-controlled Somers Road intersection with 88th Avenue during the weekday morning arrival and weekday afternoon discharge peak periods, with school in session. The higher delays and queues increase under the initial build conditions. However, the increases in delay under the background and initial build conditions are expected to be slightly higher (15 seconds for westbound vehicles and 7 seconds for eastbound vehicle) during the typical weekday morning (for westbound) and afternoon (for eastbound) discharge peak period under the initial build traffic conditions with the existing lane configurations on all approaches, that is, with no additional eastbound/westbound lanes along Somers Road. The increases in queues under the existing lane configurations are expected to be 2 to 3 vehicles. It is noted that these delays and increases in queue lengths would only be expected for less than 30 minutes during the typical weekday morning and afternoon school “surge” peak periods with lower delays and queues all other hours of the day.

In addition, with the recommended modifications under future year 2035 background traffic conditions; that is, with additional lanes on the eastbound and westbound approaches but without additional lanes on the northbound and southbound approaches at the Somers Road intersection with 88th Avenue, slightly higher delays (2 seconds for northbound vehicles and 8 seconds for southbound vehicle) would be expected on the north and south approaches during the typical weekday afternoon discharge peak period under the full build traffic conditions with single lanes on the north and south approaches. Queue lengths on the north and south approaches would be expected to be 7 to 9 vehicles; however, these queue lengths would only be expected for less than 30 minutes. To alleviate the expected longer delays and queue lengths, additional northbound and southbound lanes would be required; therefore, a sensitivity analysis was completed to document this geometric condition, as described below.

Because modifications are recommended at the all-way stop-controlled Somers Road intersection with 88th Avenue under current and background conditions and because the modifications are further driven by projected future year volumes during peak “surge” traffic periods during the typical weekday morning school arrival and afternoon school discharge peak periods, a sensitivity analysis was completed for this project to document the operations if dual lanes were not only constructed along Somers Road (eastbound/westbound lanes) but also along 88th Avenue (northbound/southbound lanes) at the all-way stop controlled intersection. With the modifications, all movements are expected to operate safely with moderate queue lengths. Therefore, the following modifications could be considered as optional under the design year background and full build conditions.

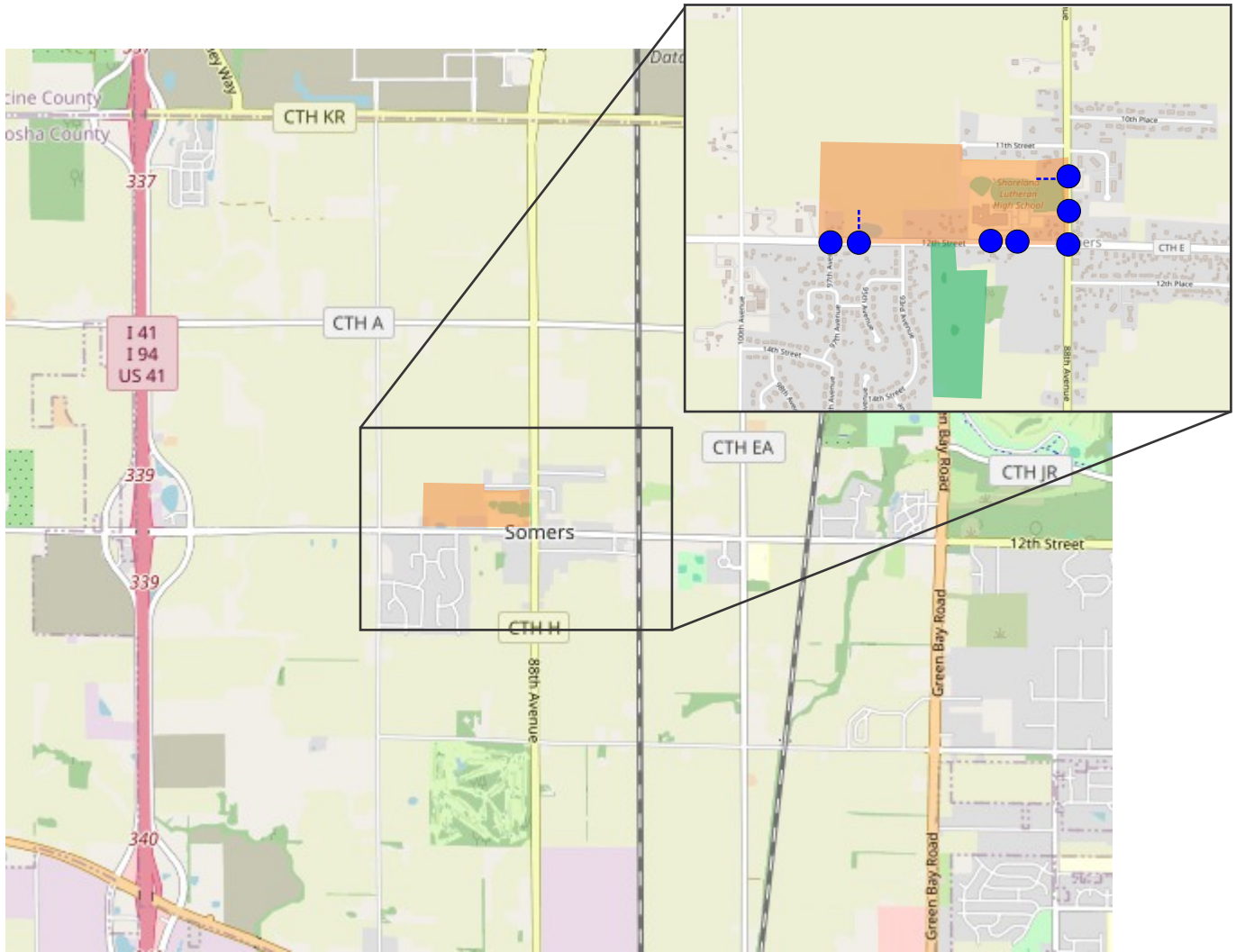
Node 500: Somers Road/CTH E intersection with 88th Avenue/CTH H

- *Year 2035 Background Traffic:*
 - Provide an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.

- *Year 2035 Full Build Traffic:* No additional modifications.

B8. Conclusion

All movements at the study area intersections are expected to operate safely and efficiently with the development assumptions outlined in this TIA and with the identified recommended modifications if properly designed and implemented through the design year of the development.



LEGEND

- Study Area Intersection
- School Expansion Site Location
- Beta Terra 45 Residential Site Location





PROJECT INFORMATION

**PROPOSED BUILDING ADDITION PHASE ONE FOR:
SHORELAND LUTHERAN HIGH SCHOOL**
9026 12TH STREET • KENOSHA, WI 53144

PRELIMINARY DATES
JULY 8, 2024
JULY 18, 2024

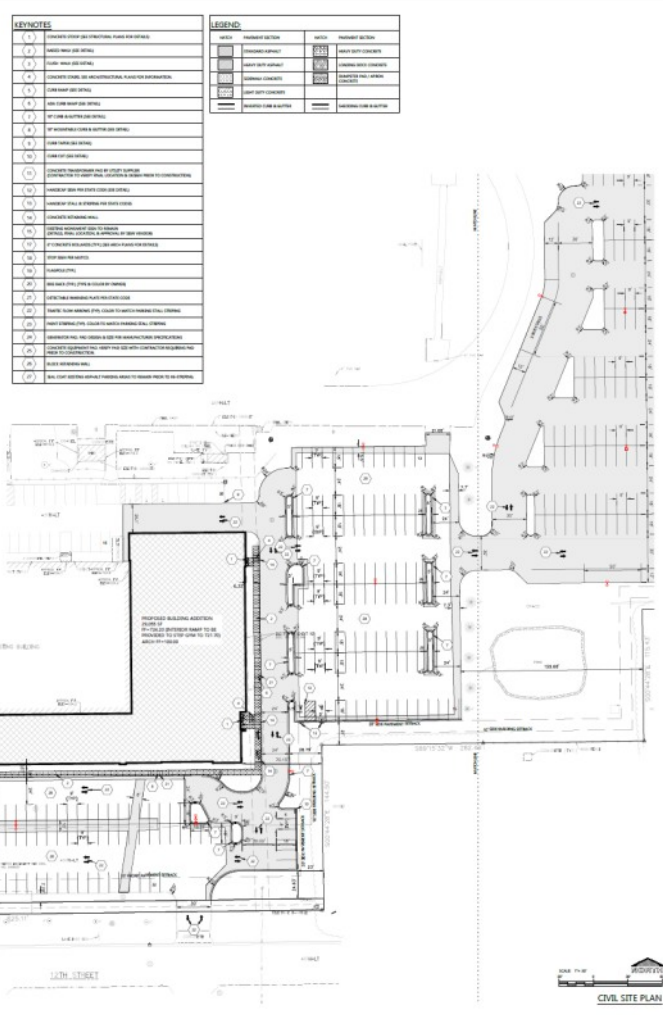
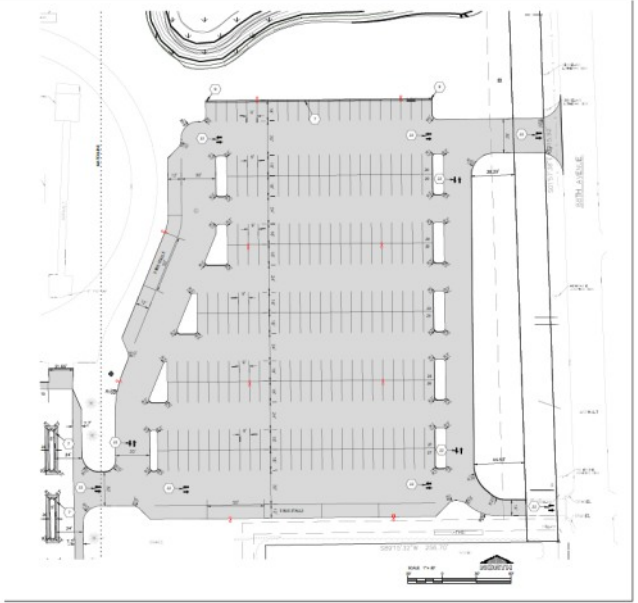
NOT FOR CONSTRUCTION

JOB NUMBER
240018700

DATE REVISION
C1.1



KEYNOTES		LEGEND	
(1)	CONCRETE FOOTING AND FOUNDATION (BASE FOR DRIVE)	(1)	CONCRETE DRIVEWAY
(2)	ASPHALT DRIVE AND DRIVEWAY	(2)	ASPHALT DRIVE DRIVEWAY
(3)	GRAVEL DRIVE AND DRIVEWAY	(3)	GRAVEL DRIVE DRIVEWAY
(4)	CONCRETE DRIVE AND DRIVEWAY (BASE FOR DRIVEWAY)	(4)	CONCRETE DRIVE DRIVEWAY
(5)	CONCRETE DRIVE AND DRIVEWAY	(5)	CONCRETE DRIVE DRIVEWAY
(6)	GRAVEL DRIVE AND DRIVEWAY	(6)	GRAVEL DRIVE DRIVEWAY
(7)	GRAVEL DRIVE AND DRIVEWAY	(7)	GRAVEL DRIVE DRIVEWAY
(8)	GRAVEL DRIVE AND DRIVEWAY	(8)	GRAVEL DRIVE DRIVEWAY
(9)	GRAVEL DRIVE AND DRIVEWAY	(9)	GRAVEL DRIVE DRIVEWAY
(10)	GRAVEL DRIVE AND DRIVEWAY	(10)	GRAVEL DRIVE DRIVEWAY
(11)	GRAVEL DRIVE AND DRIVEWAY	(11)	GRAVEL DRIVE DRIVEWAY
(12)	GRAVEL DRIVE AND DRIVEWAY	(12)	GRAVEL DRIVE DRIVEWAY
(13)	GRAVEL DRIVE AND DRIVEWAY	(13)	GRAVEL DRIVE DRIVEWAY
(14)	GRAVEL DRIVE AND DRIVEWAY	(14)	GRAVEL DRIVE DRIVEWAY
(15)	GRAVEL DRIVE AND DRIVEWAY	(15)	GRAVEL DRIVE DRIVEWAY
(16)	GRAVEL DRIVE AND DRIVEWAY	(16)	GRAVEL DRIVE DRIVEWAY
(17)	GRAVEL DRIVE AND DRIVEWAY	(17)	GRAVEL DRIVE DRIVEWAY
(18)	GRAVEL DRIVE AND DRIVEWAY	(18)	GRAVEL DRIVE DRIVEWAY
(19)	GRAVEL DRIVE AND DRIVEWAY	(19)	GRAVEL DRIVE DRIVEWAY
(20)	GRAVEL DRIVE AND DRIVEWAY	(20)	GRAVEL DRIVE DRIVEWAY
(21)	GRAVEL DRIVE AND DRIVEWAY	(21)	GRAVEL DRIVE DRIVEWAY
(22)	GRAVEL DRIVE AND DRIVEWAY	(22)	GRAVEL DRIVE DRIVEWAY
(23)	GRAVEL DRIVE AND DRIVEWAY	(23)	GRAVEL DRIVE DRIVEWAY
(24)	GRAVEL DRIVE AND DRIVEWAY	(24)	GRAVEL DRIVE DRIVEWAY
(25)	GRAVEL DRIVE AND DRIVEWAY	(25)	GRAVEL DRIVE DRIVEWAY
(26)	GRAVEL DRIVE AND DRIVEWAY	(26)	GRAVEL DRIVE DRIVEWAY
(27)	GRAVEL DRIVE AND DRIVEWAY	(27)	GRAVEL DRIVE DRIVEWAY
(28)	GRAVEL DRIVE AND DRIVEWAY	(28)	GRAVEL DRIVE DRIVEWAY
(29)	GRAVEL DRIVE AND DRIVEWAY	(29)	GRAVEL DRIVE DRIVEWAY
(30)	GRAVEL DRIVE AND DRIVEWAY	(30)	GRAVEL DRIVE DRIVEWAY
(31)	GRAVEL DRIVE AND DRIVEWAY	(31)	GRAVEL DRIVE DRIVEWAY
(32)	GRAVEL DRIVE AND DRIVEWAY	(32)	GRAVEL DRIVE DRIVEWAY
(33)	GRAVEL DRIVE AND DRIVEWAY	(33)	GRAVEL DRIVE DRIVEWAY
(34)	GRAVEL DRIVE AND DRIVEWAY	(34)	GRAVEL DRIVE DRIVEWAY
(35)	GRAVEL DRIVE AND DRIVEWAY	(35)	GRAVEL DRIVE DRIVEWAY
(36)	GRAVEL DRIVE AND DRIVEWAY	(36)	GRAVEL DRIVE DRIVEWAY
(37)	GRAVEL DRIVE AND DRIVEWAY	(37)	GRAVEL DRIVE DRIVEWAY
(38)	GRAVEL DRIVE AND DRIVEWAY	(38)	GRAVEL DRIVE DRIVEWAY
(39)	GRAVEL DRIVE AND DRIVEWAY	(39)	GRAVEL DRIVE DRIVEWAY
(40)	GRAVEL DRIVE AND DRIVEWAY	(40)	GRAVEL DRIVE DRIVEWAY
(41)	GRAVEL DRIVE AND DRIVEWAY	(41)	GRAVEL DRIVE DRIVEWAY
(42)	GRAVEL DRIVE AND DRIVEWAY	(42)	GRAVEL DRIVE DRIVEWAY
(43)	GRAVEL DRIVE AND DRIVEWAY	(43)	GRAVEL DRIVE DRIVEWAY
(44)	GRAVEL DRIVE AND DRIVEWAY	(44)	GRAVEL DRIVE DRIVEWAY
(45)	GRAVEL DRIVE AND DRIVEWAY	(45)	GRAVEL DRIVE DRIVEWAY
(46)	GRAVEL DRIVE AND DRIVEWAY	(46)	GRAVEL DRIVE DRIVEWAY
(47)	GRAVEL DRIVE AND DRIVEWAY	(47)	GRAVEL DRIVE DRIVEWAY
(48)	GRAVEL DRIVE AND DRIVEWAY	(48)	GRAVEL DRIVE DRIVEWAY
(49)	GRAVEL DRIVE AND DRIVEWAY	(49)	GRAVEL DRIVE DRIVEWAY
(50)	GRAVEL DRIVE AND DRIVEWAY	(50)	GRAVEL DRIVE DRIVEWAY



**EXHIBIT 1-2A
CONCEPTUAL SITE PLAN
INITIAL BUILD**

SOMERS, WISCONSIN



PROJECT INFORMATION

PROPOSED MASTERPLAN FOR:
SHORELAND LUTHERAN HIGH SCHOOL
9026 12TH STREET • KENOSHA, WI 53144

REVISIONS
NOV. 18, 2022
NOV. 18, 2022
NOV. 18, 2022
NOV. 18, 2022
NOV. 18, 2022
NOV. 18, 2022

NOT FOR CONSTRUCTION

JOB NUMBER
2187400

SHEET NUMBER
C100

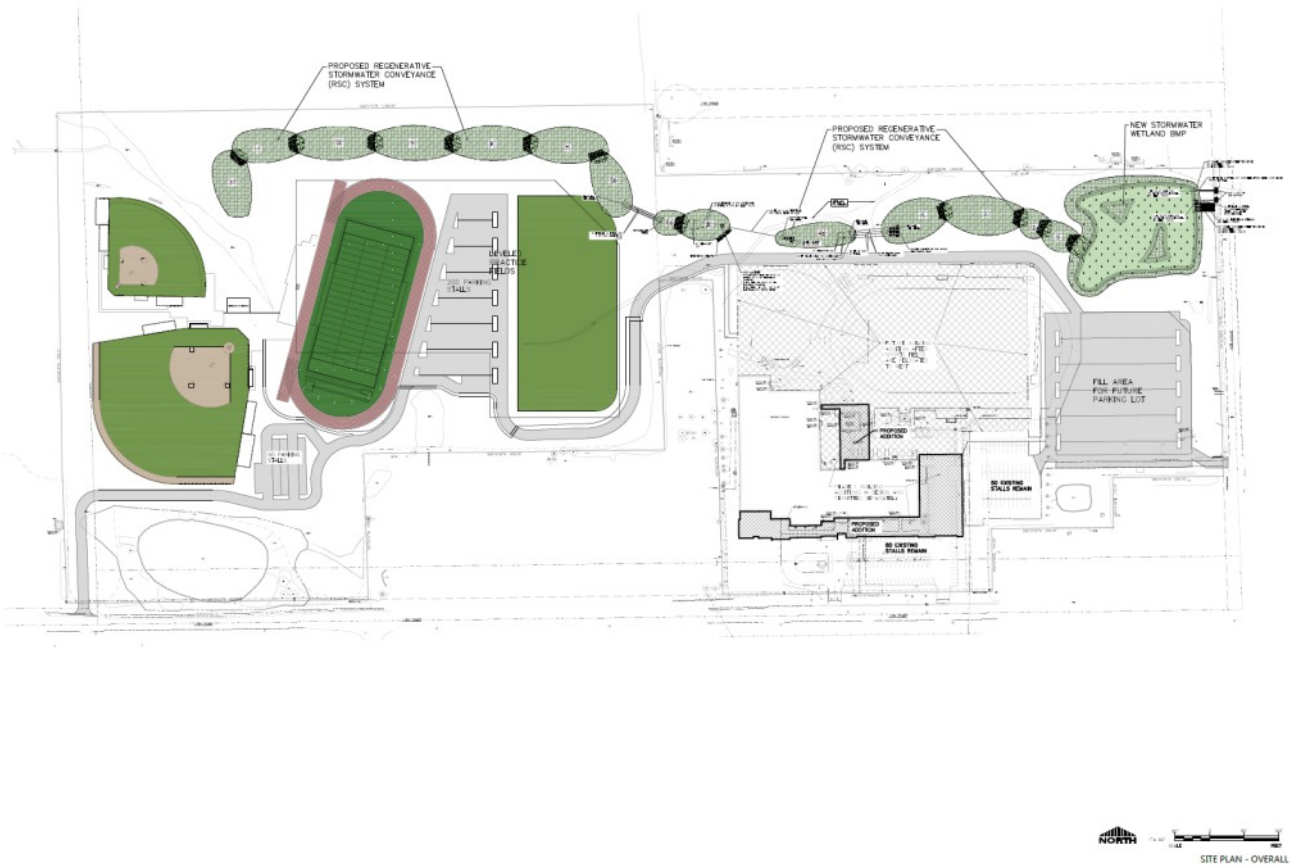

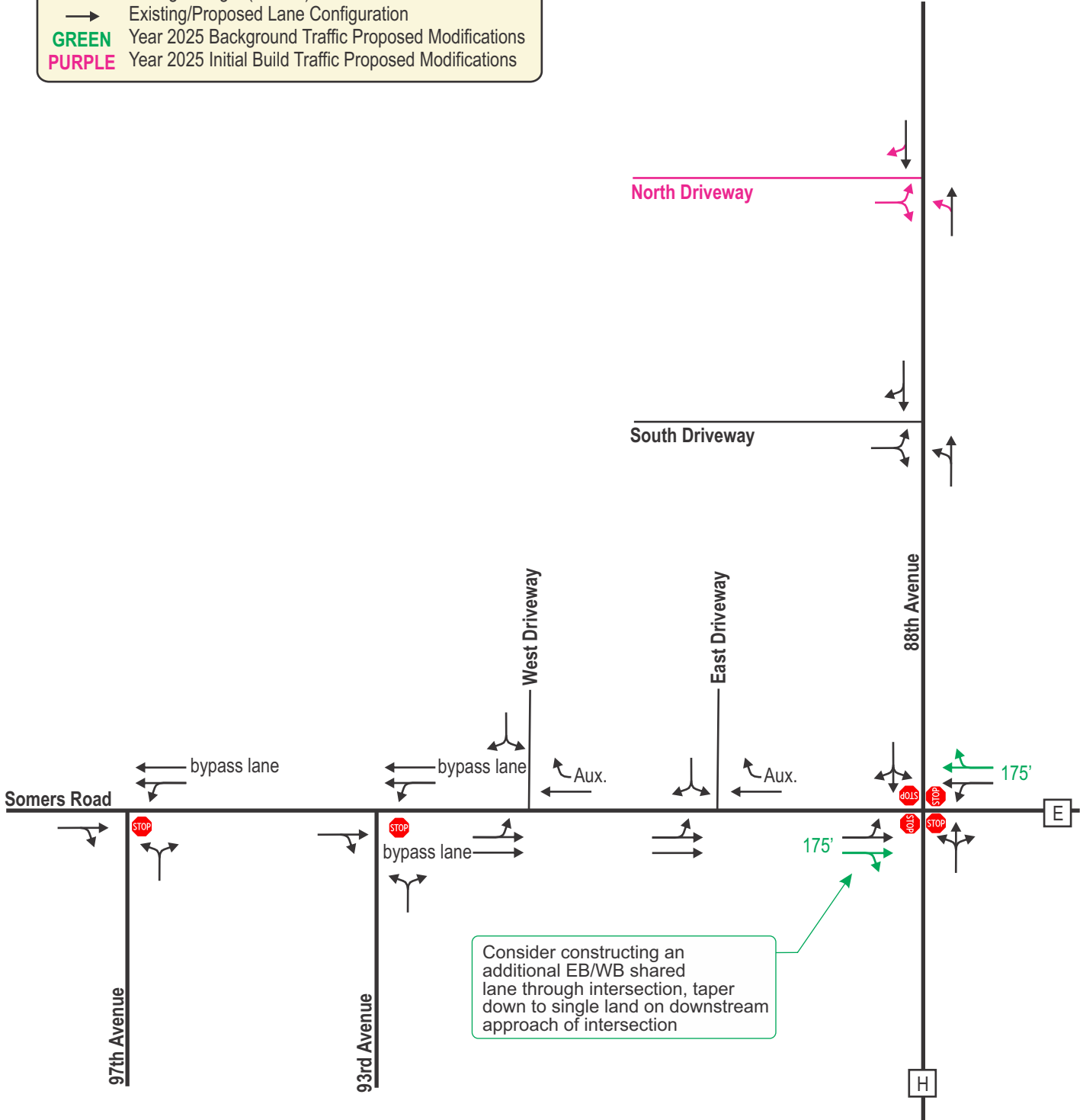


EXHIBIT 1-2B
CONCEPTUAL SITE PLAN
FULL BUILD



SOMERS, WISCONSIN

LEGEND

-  Stop Sign Control
- XX' Storage Length (In Feet)
- Existing/Proposed Lane Configuration
- GREEN** Year 2025 Background Traffic Proposed Modifications
- PURPLE** Year 2025 Initial Build Traffic Proposed Modifications

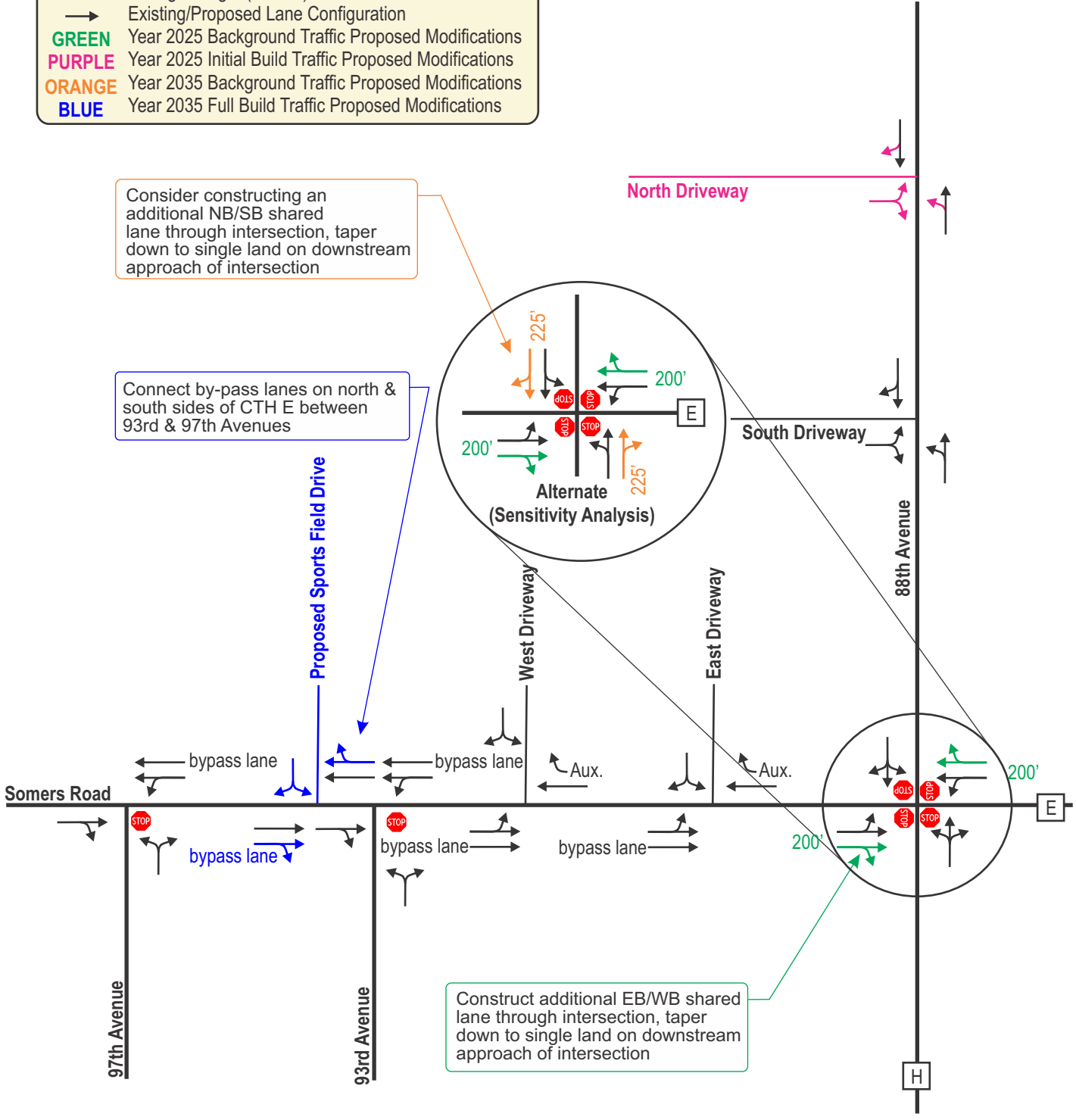


LEGEND

-  Stop Sign Control
- XX'** Storage Length (In Feet)
-  Existing/Proposed Lane Configuration
- GREEN** Year 2025 Background Traffic Proposed Modifications
- PURPLE** Year 2025 Initial Build Traffic Proposed Modifications
- ORANGE** Year 2035 Background Traffic Proposed Modifications
- BLUE** Year 2035 Full Build Traffic Proposed Modifications

Consider constructing an additional NB/SB shared lane through intersection, taper down to single land on downstream approach of intersection

Connect by-pass lanes on north & south sides of CTH E between 93rd & 97th Avenues



**EXHIBIT 1-4
YEAR 2035 RECOMMENDED MODIFICATIONS**

SOMERS, WISCONSIN

CHAPTER II – PROPOSED DEVELOPMENT

PART A – ON-SITE DEVELOPMENT

A1. Development Site Location

Shoreland Lutheran High School is proposing an expansion to their existing high school located on the north side of County Trunk Highway (CTH) E/Somers Road, immediately west of 88th Avenue/CTH H within the Village of Somers, Kenosha County. The initial build includes expansion of the existing gymnasium as well as additional classroom space. Additional parking is also expected under the initial build scenario. As part of the full build-out plans, within the next 5 to 10 years, a football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) are also proposed in the western portion of the site. A street map illustrating the location of the proposed Shoreland Lutheran High School is shown in [Exhibit 2-1](#) at the end of this chapter. Copies of the conceptual site plans for the initial build and future full build are included in [Exhibits 2-2A&B](#), respectively.

A2. Surrounding Land Use

The Shoreland Lutheran High School expansion site is bordered by residential land uses to the north and northeast. Additional residential properties and neighborhoods exist on both the north and south sides of Somers Road to the east and west of the site. Agriculture/farming land uses also exist beyond most of the residential areas in all directions. IH-94 exists further to the west with access ramps to the freeway located about 2 miles west of the school.

A3. On-Site Development Description

Shoreland Lutheran High School is proposing expansion of the existing gymnasium as well as additional classroom space. Some parking on the east side of the school is being displaced as part of the expansion plans; however, a new parking lot is proposed as part of the initial build scenario with about 254 new parking spaces as well as 5 bus stalls proposed on the northeast portion of the school site and 57 additional spaces to the southwest, in front of the school. Full buildout plans, within the next 5 to 10 years, include construction of a new football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) proposed to be located on the western portion of the site. Parking is also proposed near the future sports fields.

A4. On-Site Development Access

Two new access points are proposed for the Shoreland Lutheran High School site. A new full access driveway, referred to as “North Driveway” is proposed along 88th Avenue/CTH H, about 350 feet north of the existing South Driveway onto CTH H. This new driveway, which is expected to provide access to a new northeast parking lot area on the east side of the school, is proposed as part of the initial build phase of the project. A second access driveway, referred to as “Sports Field Drive” is proposed under the full build conditions. This driveway is proposed to provide access to the sports fields proposed on the western portion of the site. Cross access between the school and the sports field is also proposed as part of the development plans; however, because of the proximity of the connection and the curved nature of the connection roadway, the Sports Field Drive is not expected to be utilized as a primary access for typical school day traffic. This second access is proposed as a full access driveway and is proposed to be located about 220 feet east of 97th Avenue.

PART B – STUDY AREA

B1. Influence Area

The campus is expected to draw from the greater Milwaukee, Racine, and Kenosha County areas. Statistics and a map showing the location of the feeder families, by existing school district, are provided in the appendix of this report.

B2. Area of Significant Traffic Impact

As discussed with Kenosha County and the Village of Somers, the study area for the proposed school includes the following existing intersections:

- Somers Road/CTH E & 97th Avenue (one-way stop control)
- Somers Road/CTH E & Existing West School Driveway (one-way stop control)
- Somers Road/CTH E & Existing East School Driveway (one-way stop control)
- Somers Road/CTH E & 88th Avenue/CTH H (all-way stop control)
- 88th Avenue/CTH H & Existing South School Driveway (one-way stop control)

In addition to these existing intersections, the following proposed driveways were also included in the study:

- Somers Road/CTH E & Proposed Sports Field Driveway (one-way stop control)
- 88th Avenue/CTH H & Proposed North School Driveway (one-way stop control)

PART C – OFF-SITE LAND USE AND DEVELOPMENT

One off-site development has been identified within the immediate study area. A residential development, the Beta Terra 45 Residential Neighborhood development, including 45 single family lots, has previously been approved for a parcel of land on the south side of Somers Road. Access to the site is proposed via a new access drive onto Somers Road, immediately east of 93rd Avenue. The off-site development traffic was included in the Year 2025 and Year 2035 Background Traffic scenarios.

PART D – SITE ACCESSIBILITY

D1. Study Area Roadways

The study area roadways are discussed below:

Somers Road (CTH E) is an east/west two-lane undivided Major Collector with a posted speed limit of 35-mph through the project limits. The Year 2021 WisDOT annual average daily traffic (AADT) volumes on Somers Road were approximately 6,100 vehicles per day (vpd) west of 88th Avenue, and 7,000-vpd east of 58th Avenue. Sidewalks are not currently provided along either side of Somers Road within the limits of the study area.

88th Avenue (CTH H) is a north-south two-lane undivided Minor Arterial with a posted speed limit of 45-mph through the project limits. The Year 2021 WisDOT AADT volumes on 88th Avenue were approximately 2,100-vpd to the north of Somers Road and 3,200-vpd to the south. Sidewalks are not currently provided along either side of 88th Avenue within the limits of the study area.

97th Avenue is a north-south two-lane undivided local street with a posted speed limit of 25-mph that intersects Somers Road as the south approach of a conventional three-legged stop sign

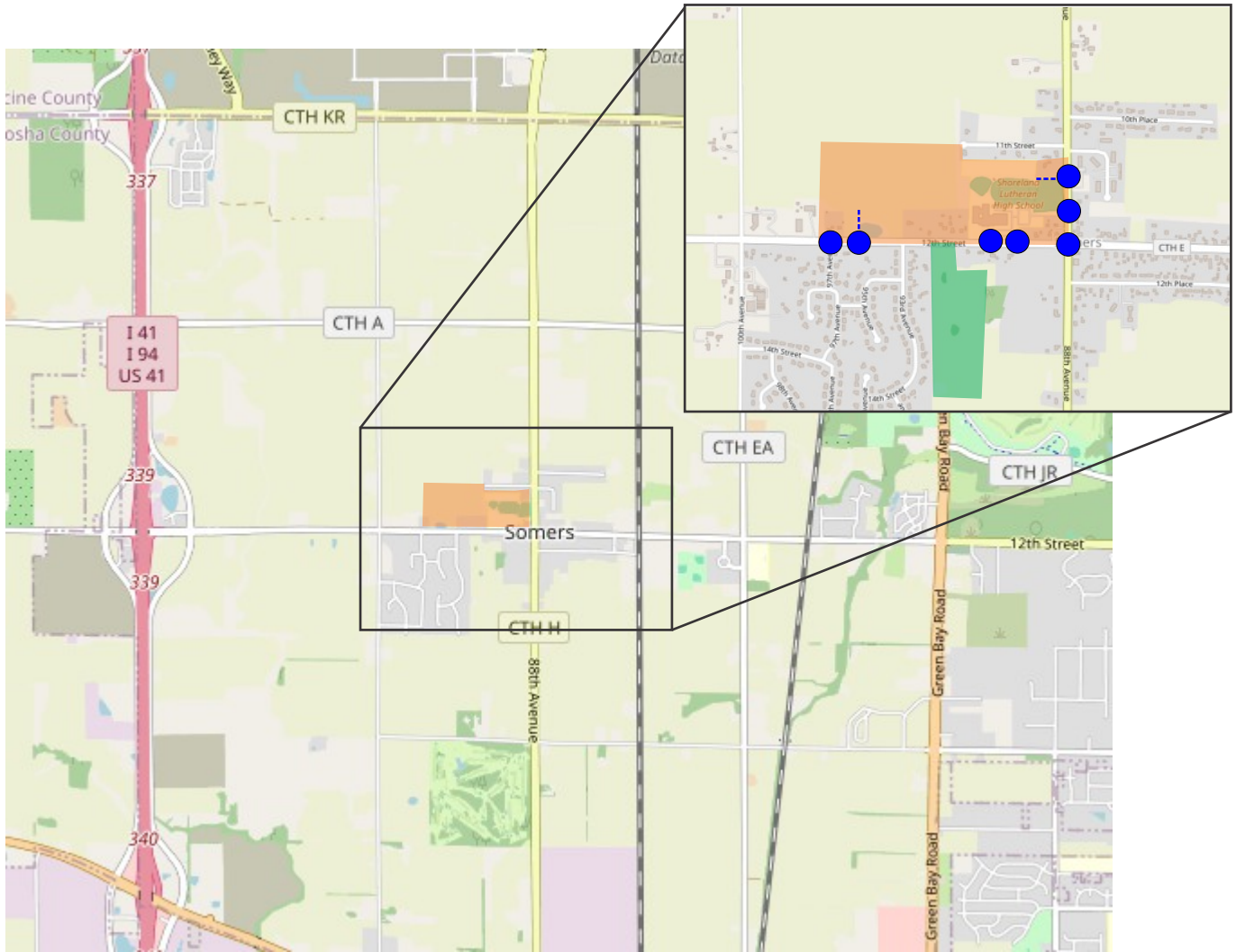
controlled intersection. There are currently no WisDOT AADT volumes available along 97th Avenue. Sidewalks are not currently provided along either side of 97th Avenue within the limits of the study area.

D2. Alternative Modes of Transportation

Sidewalks do not currently exist along any of the roadways within the limits of the study area. No designated on-street or off-street bicycle facilities were identified.

Pedestrians and bicyclists may use their respective modes to access the proposed development. However, to allow for a conservative (highest vehicular volume) analysis, these modes were assumed to make up a relatively small portion of the overall trips to/from the study area. For the purpose of this TIA, all trips to/from the proposed development site were assumed to occur via motor vehicle.

Transit is not present within the community; however, up to 5 existing private buses are expected to provide service for some students within southeast Wisconsin.



LEGEND

- Study Area Intersection
- School Expansion Site Location
- Beta Terra 45 Residential Site Location





Always a Better Plan

PROPOSED MASTERPLAN FOR:

SHORELAND LUTHERAN HIGH SCHOOL

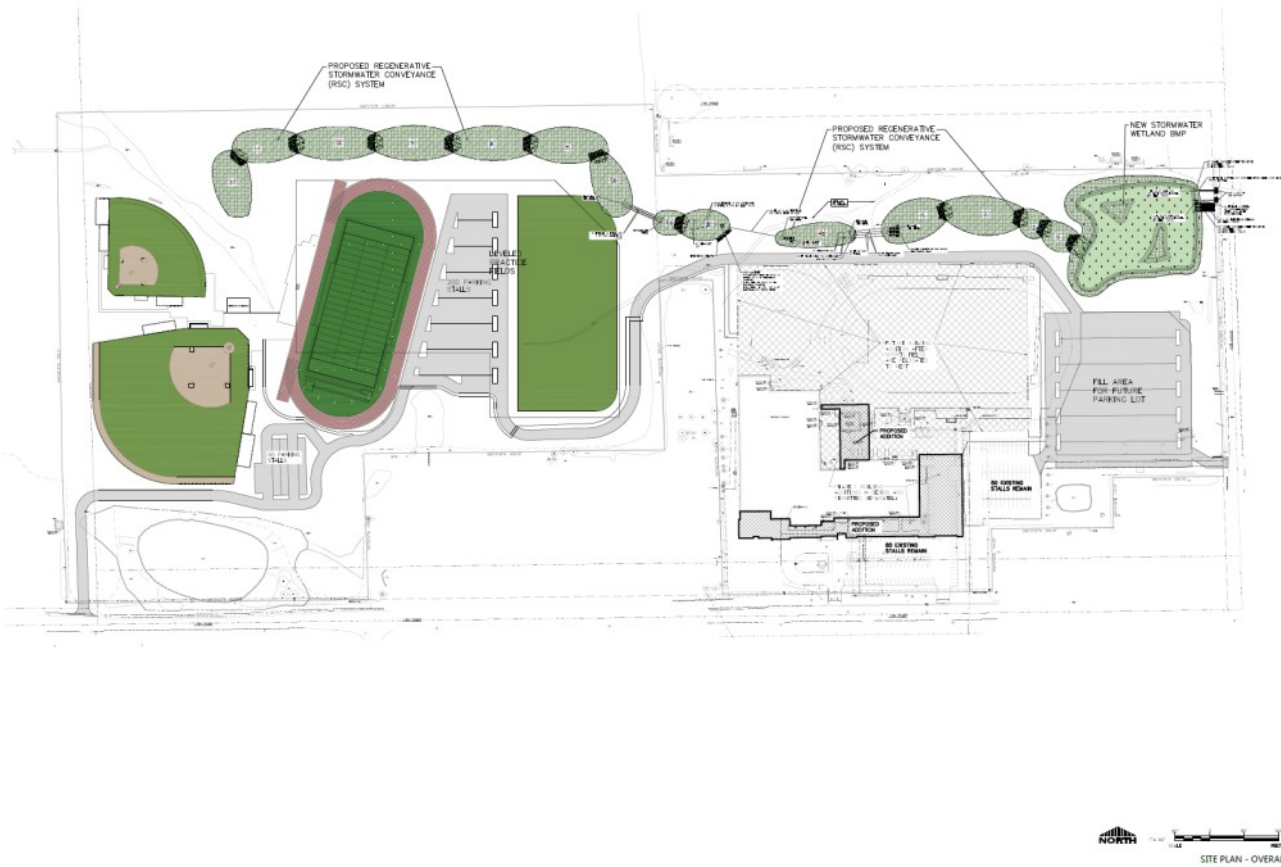
9026 12TH STREET • KENOSHA, WI 53144

REVISIONS
AUG. 25, 2022
MAR. 23, 2022
MAR. 15, 2022
APR. 19, 2022
NOV. 8, 2022
NOV. 18, 2022

NOT FOR CONSTRUCTION

JOB NUMBER
2187400

SHEET NUMBER
C100



TRAFFIC ANALYSIS & DESIGN, INC.
3341 ~ 10/24/24



EXHIBIT 2-2B CONCEPTUAL SITE PLAN FULL BUILD

SOMERS, WISCONSIN

CHAPTER III – ANALYSIS OF EXISTING CONDITIONS

PART A – EXISTING PHYSICAL CHARACTERISTICS

[Exhibit 3-1](#) shows the existing transportation detail for the study area intersections. More specifically, the exhibit illustrates intersection lane configurations, intersection traffic controls, posted speed limits, and approximate intersection spacing.

PART B – EXISTING TRAFFIC VOLUMES

The weekday morning school arrival and weekday afternoon school discharge peak hours are expected to drive the improvements needed to adequately accommodate the proposed school, as they represent the highest trip generation for the site. Therefore, in mid-September of 2024, TADI conducted weekday morning arrival (7:00 to 8:00 am) and weekday afternoon (2:30 to 4:30 pm) turning movement traffic counts at the four Somers Road study area intersections with 97th Avenue, West school driveway, East school driveway and 88th Avenue as well as at the 88th Avenue intersection with the south school driveway.

Based on the turning movement counts and the expected school bell schedules, the weekday morning and weekday afternoon peak school hours were identified as being 7:00 to 8:00am and 3:00 to 4:00pm; respectively. These peak hours coincide with the school start and end times at the school of 7:45am and 3:00pm (closing prayer); respectively. Copies of the existing traffic counts and related calculations are included in [Appendix A](#). The Existing peak hour traffic volumes, balanced along the Somers Road and 88th Avenue corridors, are shown in [Exhibit 3-2](#).

PART C – EXISTING CAPACITY LEVEL OF SERVICE

C1. Level of Service Definitions

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual (HCM) 6th Edition*. Intersection operation is defined by “level of service.” Level of service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A,’ to very poor, represented by LOS ‘F.’ For the purpose of this study, LOS D was used to define acceptable peak hour operating conditions. Descriptions of the various levels of service are as follows:

LOS A is the highest level of service that can be achieved. Under this condition, intersection approaches appear quite open, turning movements are easily made, and nearly all drivers find freedom of operation. At unsignalized intersections, average delays are less than **10** seconds.

LOS B represents stable operation. At unsignalized intersections, average delays are **10 to 15** seconds.

LOS C still represents stable operation, but periodic backups of a few vehicles may develop behind turning vehicles. Most drivers begin to feel restricted, but not objectionably so. At unsignalized intersections, average delays are **15 to 25** seconds.

LOS D represents increasing traffic restrictions as the intersection approaches instability. Delays to approaching vehicles may be substantial during short peaks within the peak period, but periodic clearance of long lines occurs, thus preventing excessive backups. At unsignalized intersections, average delays are **25 to 35** seconds.

LOS E represents the capacity of the intersection. At unsignalized intersections, average delays are **35 to 50** seconds.

LOS F represents jammed conditions where the intersection is over capacity and acceptable gaps for unsignalized intersections in the mainline traffic flow are minimal. At unsignalized intersections, average delays exceed 50 seconds.

C2. Existing Traffic Operations

[Exhibit 3-3](#) show the Existing traffic peak hour operating conditions at the study area intersections for the typical school day. The Existing traffic analysis was conducted using the existing lane configurations shown in [Exhibit 3-1](#) and the Existing traffic volumes shown in [Exhibit 3-2](#).

As shown in [Exhibit 3-3](#), all movements at the study area intersections are currently operating acceptably with LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours except the westbound movements at the Somers Road intersection with 88th Avenue which are currently operating at LOS E during the typical weekday morning arrival peak hour.



PART D – SOURCES OF DATA

The following sources of data were obtained for use in conducting this traffic study:

- Turning movement traffic counts – TADI
- Historic AADT information to calculate background growth rates – WisDOT
- Existing transportation detail – TADI along with Google Earth
- On-site development information – Excel Engineering and Shoreland Lutheran High School
- Off-site development information – TADI

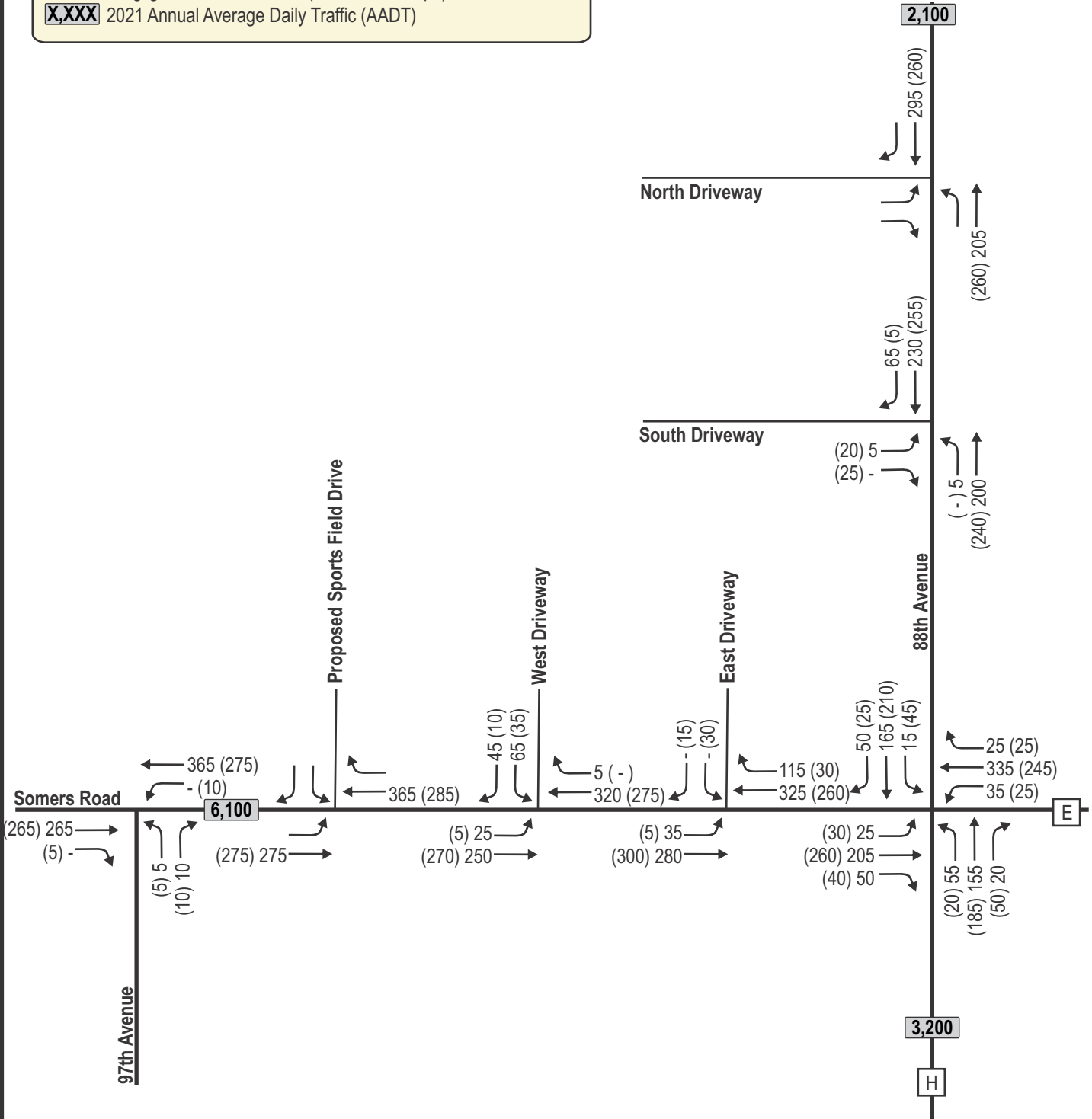


LEGEND

-  Stop Sign Control
-  Existing Lane Configuration
- XX'** Existing Storage Length (in Feet)
- XX'** Distance Between Roadways (in Feet)

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)
- X,XXX 2021 Annual Average Daily Traffic (AADT)



**EXHIBIT 3-2
EXISTING TRAFFIC VOLUMES**

SOMERS, WISCONSIN

Exhibit 3-3
Year 2024 Existing Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	Lanes->		-	1		1	-		1					
	AM	LOS	-	*		A	-		B					
		Delay	-	*		7.8	-		11.5					
		Queue	-	*		25'	-		25'					
	PM	LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.2					
Queue		-	*		25'	-		25'						
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	Lanes->		1	-	-	1			-		1			
	AM	LOS	A	-	-	*			-		C			
		Delay	8.3	-	-	*			-		17.9			
		Queue	25'	-	-	*			-		35'			
	PM	LOS	A	-	-	*			-		B			
		Delay	7.9	-	-	*			-		13.3			
Queue		25'	-	-	*			-		25'				
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	Lanes->		1	-	-	1			-		1			
	AM	LOS	A	-	-	*			-		C			
		Delay	8.9	-	-	*			-		15.0			
		Queue	25'	-	-	*			-		25'			
	PM	LOS	A	-	-	*			-		B			
		Delay	8.0	-	-	*			-		13.3			
Queue		25'	-	-	*			-		25'				
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	Lanes->		1			1			1		1			
	AM	LOS	C			E			C		C			
		Delay	23.5			42.8			20.8		20.4			
		Queue	120'			245'			90'		90'			
	PM	LOS	D			D			C		D			
		Delay	31.6			26.9			22.6		25.3			
Queue		170'			140'			105'		125'				
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	Lanes->		1			-		1	-	-	1			
	AM	LOS	B			-		A	*	-	*			
		Delay	12.3			-		8.1	*	-	*			
		Queue	25'			-		25'	*	-	*			
	PM	LOS	B			-		A	*	-	*			
		Delay	12.2			-		8.0	*	-	*			
Queue		25'			-		25'	*	-	*				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

CHAPTER IV – DEVELOPMENT TRAFFIC

PART A – DEVELOPMENT TRAFFIC FORECASTING

To determine the background traffic growth rate to use when calculating the design year traffic volumes, historic WisDOT AADT volumes from the year 1990 through year 2023 were collected along CTH E and CTH H at the WisDOT count stations located along the corridors in close proximity to the study area. Using this data, historical AADT trend lines were developed to determine the expected annual growth of traffic that may occur on the corridor between the present and design year 2035. Based on the traffic projections (1990 – 2023) and assuming the historic trends will continue at a linear rate, traffic volumes along the CTH E and CTH H corridors would be expected to increase at a rate of 1.5-percent per year. Therefore, this growth rate was applied to the existing traffic volumes collected as part of this study. The Year 2035 Projected traffic volumes, which account for this yearly growth rate, are shown in [Exhibit 4-2A](#). The growth rate calculations have been included in the [Appendix](#) of this study.

A1. Trip Generation & Distribution

To address future traffic impacts along study area roadways and at the intersections adjacent to the development, it is necessary to identify the hourly and daily volume of traffic generated by the development. The hourly vehicular traffic volumes expected to be generated by the Shoreland Lutheran High School expansion are estimated based on trip rates derived using the existing driveway trips counted as part of this project. Using the existing student population and the traffic counts conducted for this study during a typical weekday in mid-September of 2024, weekday morning arrival and weekday afternoon discharge peak hour rates were calculated. Based on the calculations, the weekday morning arrival and weekday afternoon discharge peak hour rates were calculated at 0.81 vehicles/student and 0.40 vehicles/student, respectively. To calculate the expected new daily trips, the daily rate for a private high school was taken from the Institute of Transportation Engineer's (ITE) *Trip Generation Manual, 11th Edition*.

The trip generation and distribution for the Shoreland Lutheran High School expansion typical weekday school day Initial Build and Full Build scenarios are shown in [Exhibits 4-3A&B](#). The trip generation and distribution for the Beta Terra 45 Residential Neighborhood off-site development is shown in [Exhibit 4-3C](#). The trip distribution for the school expansion, which is listed below, and shown graphically in [Exhibit 4-4](#), was determined based on the location of anticipated school feeder families as well as the existing turning movement counts conducted as part of this study.

- 50-percent to/from the east on Somers Road
- 35-percent to/from the west on Somers Road
- 10-percent to/from the north on CTH H
- 5-percent to/from the south on CTH H

As shown in [Exhibit 4-3A](#), the Shoreland Lutheran High School expansion is expected to generate about 20 new vehicular trips (15 in/5 out) during the weekday morning arrival peak hour and 10 new vehicular trips (5 in/5 out) during the weekday afternoon dismissal peak hour under Initial Build conditions. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 50 new vehicle trips (25 in/25 out) under Initial Build conditions. Note that the ITE trip generation peak hour volumes do take into account teacher and staff trips which generally fall outside the peak hour.

As shown in [Exhibit 4-3B](#), the High School expansion is expected to generate about 80 new vehicular trips (55 in/25 out) during the weekday morning arrival peak hour and 40 new vehicular trips (10 in/30 out) during the weekday afternoon dismissal peak hour. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 220 new vehicle trips (110 in/110 out) under the future year projected full build out.

The Beta Terra 45 Residential Neighborhood off-site development is expected to generate about 35 vehicular trips (10 in/25 out) during the weekday morning arrival peak hour, 45 vehicular trips (30 in/15 out) during the weekday afternoon dismissal peak hour and 500 new trips over a typical weekday (24-hour period) under full build out conditions. Trip generation and traffic assignment information for the Beta Terra 45 Residential Neighborhood off-site developments were taken from the previously completed Beta Terra 45 Residential Neighborhood Development TIA dated June 24, 2021. All relevant traffic information for this off-site development area is provided in [Appendix B](#).

A2. Mode Split

The trip generation above provides estimates of vehicular traffic based on the student population assumptions provided by Shoreland Lutheran High School as well as the trip generation rates derived using the existing driveway trips counted as part of this project. All trips to/from the proposed development site were assumed to occur via motor vehicle.

A3. Determination of Linked and Pass-by Trip Traffic

Linked trips occur when a patron or employee visits more than one land use within a site without leaving the site. Since the school is the only use on-site, no linked trips are expected.

Pass-by trips occur when motorists on the adjacent roadway system stop off at a development prior to continuing on their intended route. For example, a motorist eastbound on Somers Road stops at Shoreland Lutheran High School prior to continuing eastbound on Somers Road. Due to the proposed land use, no pass-by trips were assumed as part of this study.

A4. Trip Assignment

New trips expected to be generated by Shoreland Lutheran High School initial build and full build were assigned based on the trip distribution described above, shown in tabular format in [Exhibits 4-3A&B](#) and graphically in [Exhibit 4-4](#). As shown in the table at the bottom of the trip generation exhibit, new trips were assigned and broken out for the typical school day. Per direction from Kenosha County, two access scenarios were evaluated as part of this study to analyze the operation of the driveways along CTH H with and without the existing south school driveway operational. The following scenarios were evaluated:

Scenario 1 – Both driveways along CTH H operational. Initial build and Full build new trips for this scenario are shown on [Exhibits 4-5A and 4-8A](#), respectively.

Scenario 2 – Only the north driveway along CTH H operational with the south driveway removed. Initial build and Full build new trips for this scenario are shown on [Exhibit 4-5B and 4-8B](#), respectively.

The Beta Terra 45 Residential Neighborhood off-site development new trips are shown in [Exhibit 4-9](#).

Under the traffic scenarios with and without the south school driveway operational, existing traffic patterns will change with vehicles expecting to divert to the north driveway, if the south

driveway were eliminated. Therefore, [Exhibits 4-10A and 4-10B](#) show the expected diverted traffic under the two scenarios listed above, respectively.

PART B – DETERMINATION OF BACKGROUND AND BUILD TRAFFIC

B1. Background Traffic

Background traffic volumes include the full build-out of the Beta Terra 45 Residential Neighborhood off-site development.

The Year 2025 Background traffic volumes were determined by adding the Existing traffic volumes ([Exhibit 3-2](#)) to the Beta Terra 45 Residential Neighborhood new trips ([Exhibit 4-9](#)). The Year 2025 Background traffic volumes are shown in [Exhibit 4-1](#).

The Year 2035 Background traffic volumes were determined by adding the Year 2035 Projected traffic volumes ([Exhibit 4-2A](#)) to the Beta Terra 45 Residential Neighborhood new trips ([Exhibit 4-9](#)). The Year 2035 Background traffic volumes are shown in [Exhibit 4-2B](#).

B2. Initial Build Traffic

The Year 2025 Initial Build (North and South Driveways on CTH H) traffic volumes assume the initial build-out of the Shoreland Lutheran High School under access scenario 1. Year 2025 Initial Build (North and South Driveways on CTH H) traffic volumes were determined by adding the Year 2025 Background traffic volumes ([Exhibit 4-1](#)) to the Initial Build (North and South Driveways on CTH H) new trips ([Exhibit 4-5A](#)) and the Redistributed (North and South Driveways on CTH H) trips ([Exhibit 4-10A](#)). The Year 2025 Initial Build (North and South Driveways on CTH H) traffic volumes are shown in [Exhibit 4-11A](#).

The Year 2025 Initial Build (North Driveway Only on CTH H) traffic volumes assume the initial build-out of the Shoreland Lutheran High School under access scenario 2. Year 2025 Initial Build (North Driveway Only on CTH H) traffic volumes were determined by adding the Year 2025 Background traffic volumes ([Exhibit 4-1](#)) to the Initial Build (North Driveway Only on CTH H) new trips ([Exhibit 4-5B](#)) and the Redistributed (North Driveway Only on CTH H) trips ([Exhibit 4-10B](#)). The Year 2025 Initial Build (North Driveway Only on CTH H) traffic volumes are shown in [Exhibit 4-11B](#).

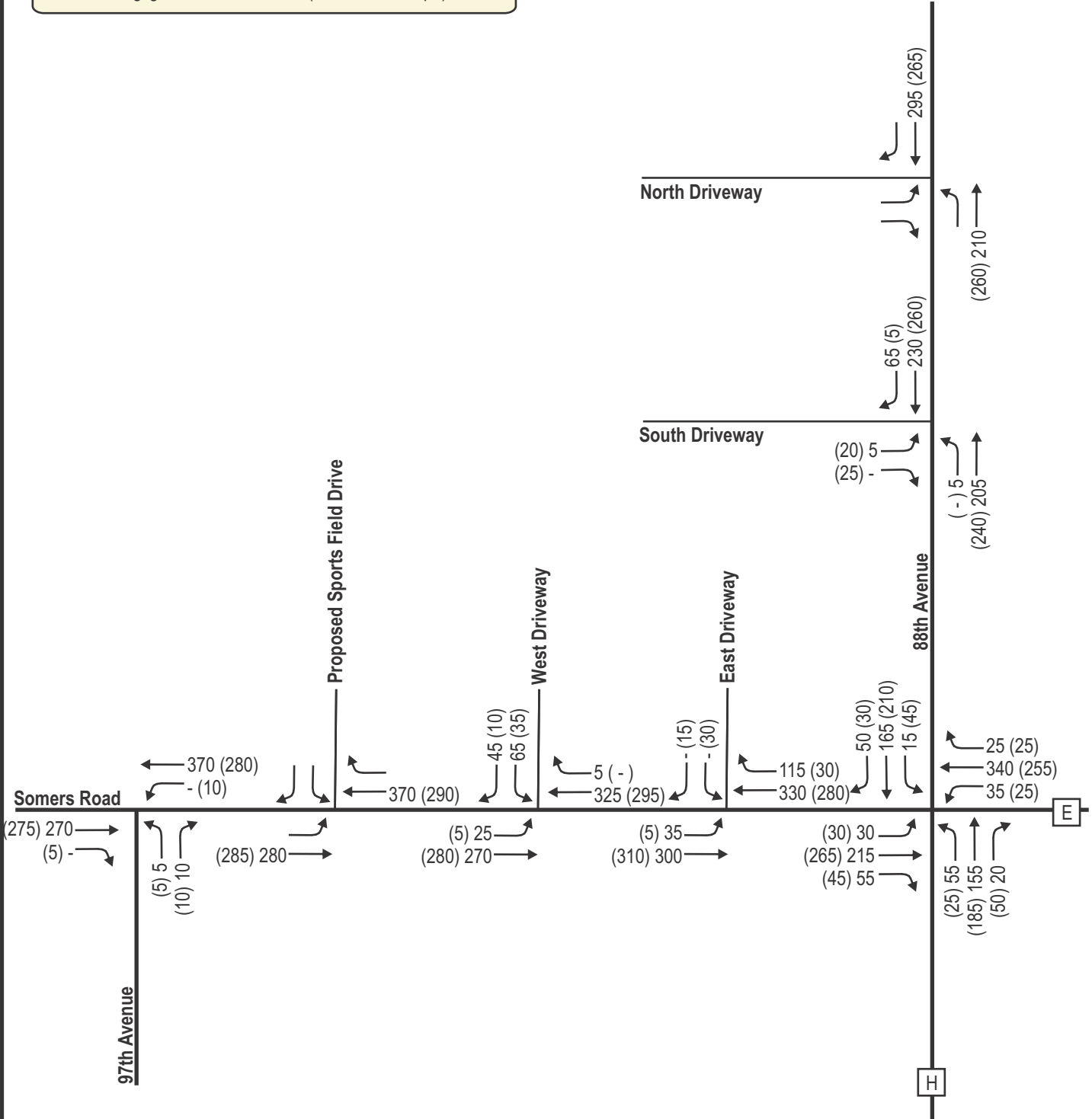
B3. Full Build Traffic

The Year 2035 Full Build (North and South Driveways on CTH H) traffic volumes assume the full build-out of the Shoreland Lutheran High School under access scenario 1. Year 2035 Full Build (North and South Driveways on CTH H) traffic volumes were determined by adding the Year 2035 Background traffic volumes ([Exhibit 4-2B](#)) to the Full Build (North and South Driveways on CTH H) new trips ([Exhibit 4-8A](#)) and the Redistributed (North and South Driveways on CTH H) trips ([Exhibit 4-10A](#)). The Year 2035 Full Build (North and South Driveways on CTH H) traffic volumes are shown in [Exhibit 4-14A](#).

The Year 2035 Full Build (North Driveway Only on CTH H) traffic volumes assume the full build-out of the Shoreland Lutheran High School under access scenario 2. Year 2035 Full Build (North Driveway Only on CTH H) traffic volumes were determined by adding the Year 2035 Background traffic volumes ([Exhibit 4-2B](#)) to the Full Build (North Driveway Only on CTH H) new trips ([Exhibit 4-8B](#)) and the Redistributed (North Driveway Only on CTH H) trips ([Exhibit 4-10B](#)). The Year 2035 Full Build (North Driveway Only on CTH H) traffic volumes are shown in [Exhibit 4-14B](#).

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

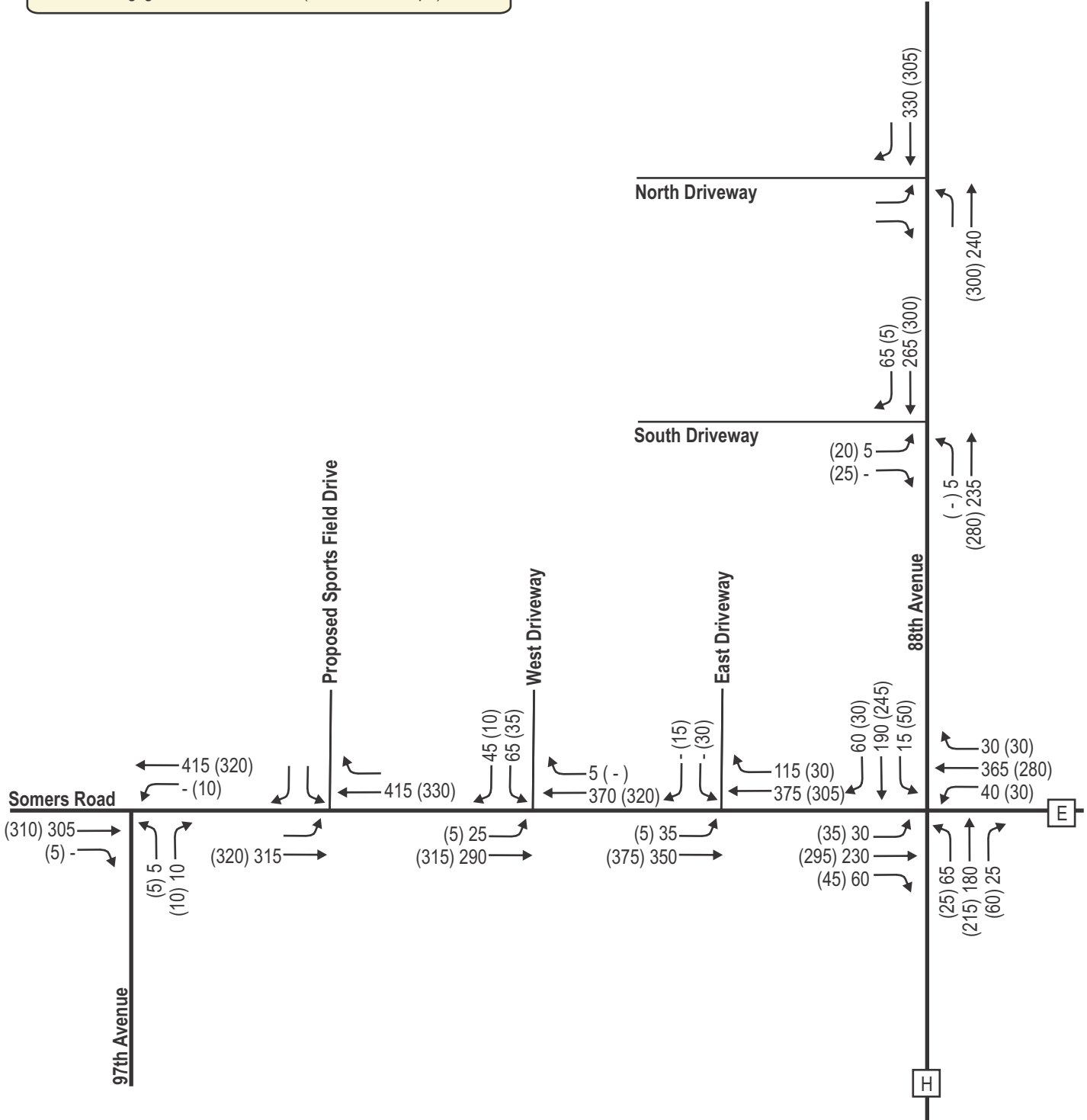


**EXHIBIT 4-1
YEAR 2025 BACKGROUND TRAFFIC VOLUMES
INCLUDES OFFSITE DEVELOPMENT**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

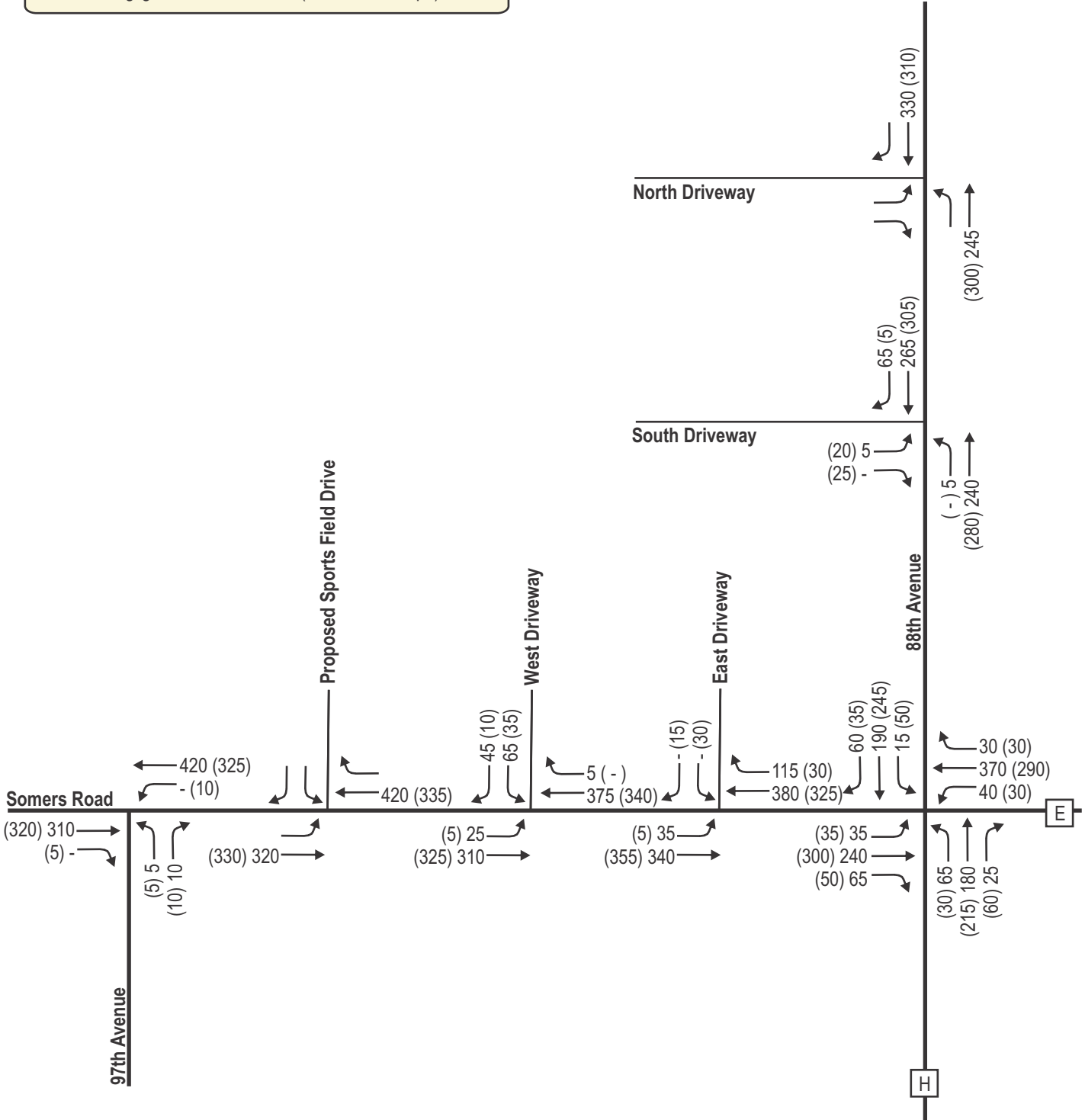


**EXHIBIT 4-2A
YEAR 2035 PROJECTED TRAFFIC VOLUMES
BACKGROUND GROWTH ONLY**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



**EXHIBIT 4-2B
YEAR 2035 BACKGROUND TRAFFIC VOLUMES
INCLUDES OFFSITE DEVELOPMENT**

SOMERS, WISCONSIN

Exhibit 4-3A
Opening Year (Initial Build) Trip Generation Table¹

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Private High School (Additional Students)	TADI	25 Students	50 (2.17)	15 (68%)	5 (32%)	20 (0.81)	5 (25%)	5 (75%)	10 (0.40)
Total New Trips			50	15	5	20	5	5	10

¹ Daily volumes calculated using ITE Trip Rates (X.XX) from the ITE Trip Generation Manual, 11th Edition. Peak hour volumes based on calculated rates using actual driveway counts conducted in early September of 2024.

TRIP DISTRIBUTION (New Trips)

East on CTH E	50%	25	10	5	5	5
West on CTH E	35%	15	5	0	0	0
North on CTH H	10%	10	0	0	0	0
South on CTH H	5%	0	0	0	0	0
	100%	50	15	5	5	5

Exhibit 4-3B
Design Year (Full Build) Trip Generation Table¹

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Private High School (Additional Students)	TADI	100 Students	220 (2.17)	55 (68%)	25 (32%)	80 (0.81)	10 (25%)	30 (75%)	40 (0.40)
Total New Trips			220	55	25	80	10	30	40

¹ Daily volumes calculated using ITE Trip Rates (X.XX) from the ITE Trip Generation Manual, 11th Edition. Peak hour volumes based on calculated rates using actual driveway counts conducted in early September of 2024.

TRIP DISTRIBUTION (New Trips)

East on CTH E	50%	110	25	10	5	15
West on CTH E	35%	80	20	10	5	10
North on CTH H	10%	20	5	5	0	5
South on CTH H	5%	10	5	0	0	0
	100%	220	55	25	10	30

Exhibit 4-3C

Off-Site (Beta Terra 45 Residential) Trip Generation Table ¹

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Single-Family Housing	210	45 Units	500 FCE	10 (25%)	25 (75%)	35 FCE	30 (63%)	15 (37%)	45 FCE
Total New Trips			500	10	25	35	30	15	45

¹ ITE Trip Rates (X.XX) and/or Fitted Curve Equations (FCE) are from the ITE Trip Generation Manual, 11th Edition.

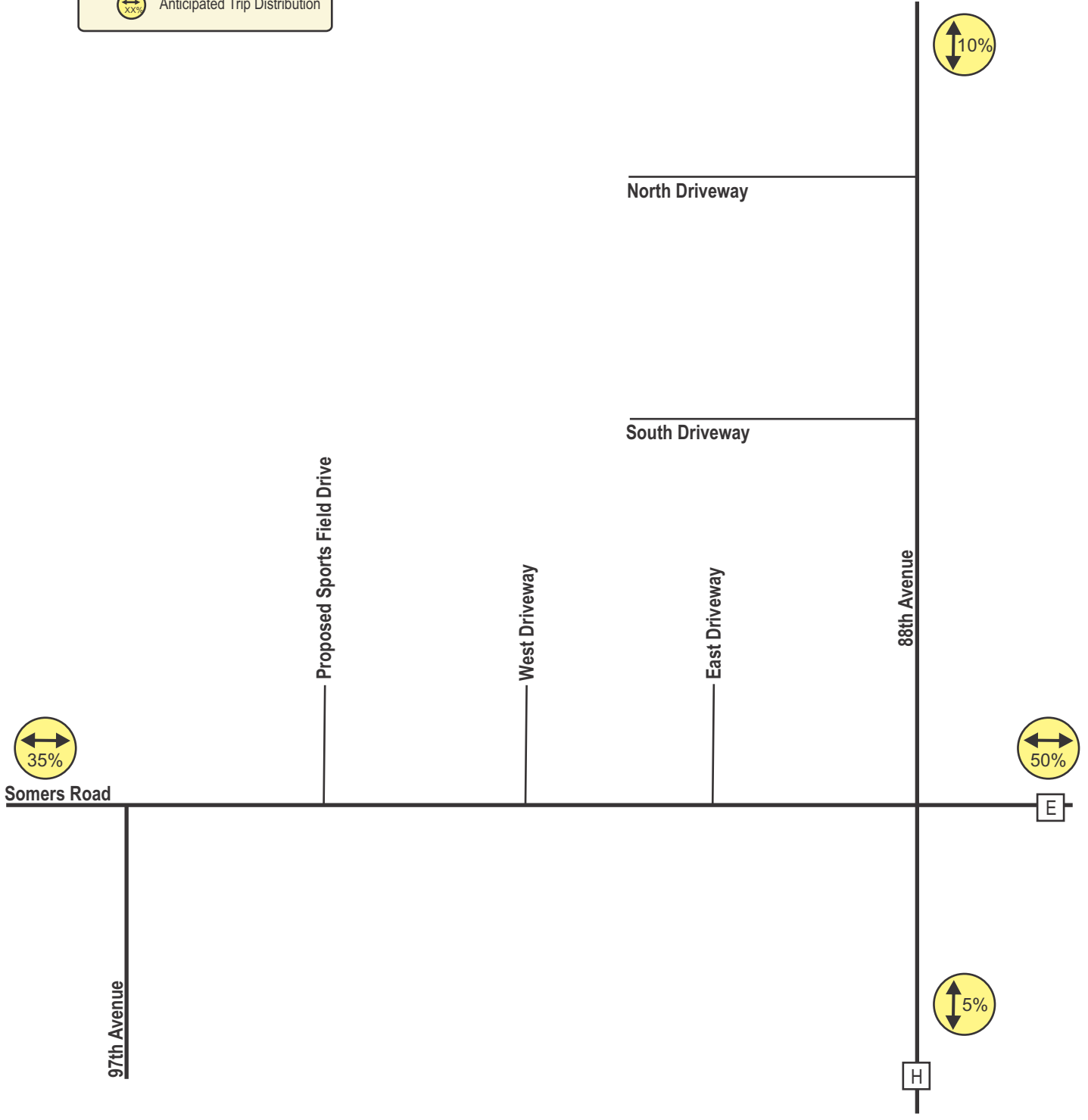
TRIP DISTRIBUTION (New Trips)

East on CTH E	30%	150	5	10	10	5
West on CTH E	30%	150	5	5	10	5
North on CTH H	15%	80	0	5	5	0
South on CTH H	25%	130	0	5	5	5
	100%	510	10	25	30	15

LEGEND

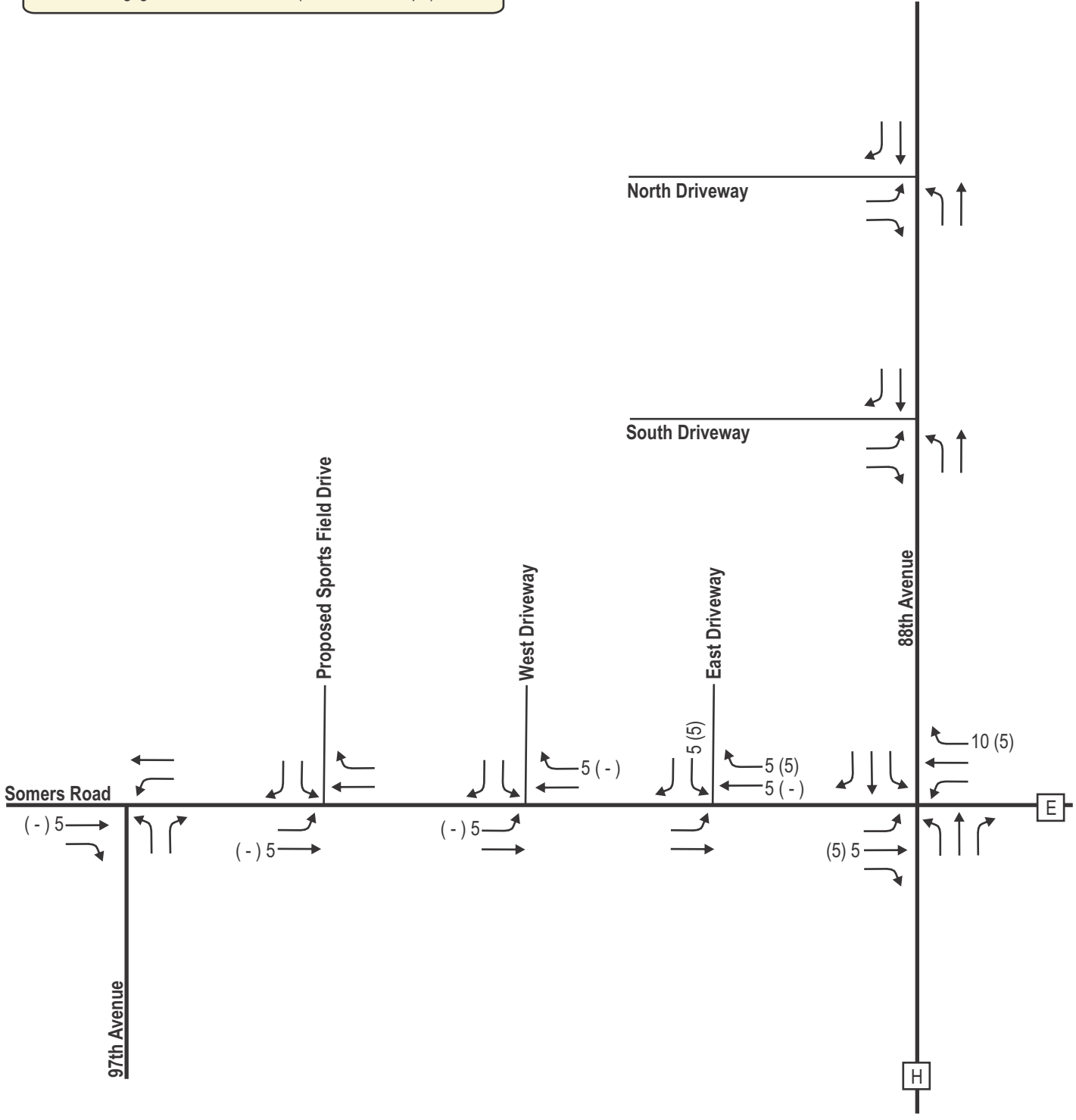


Anticipated Trip Distribution



LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

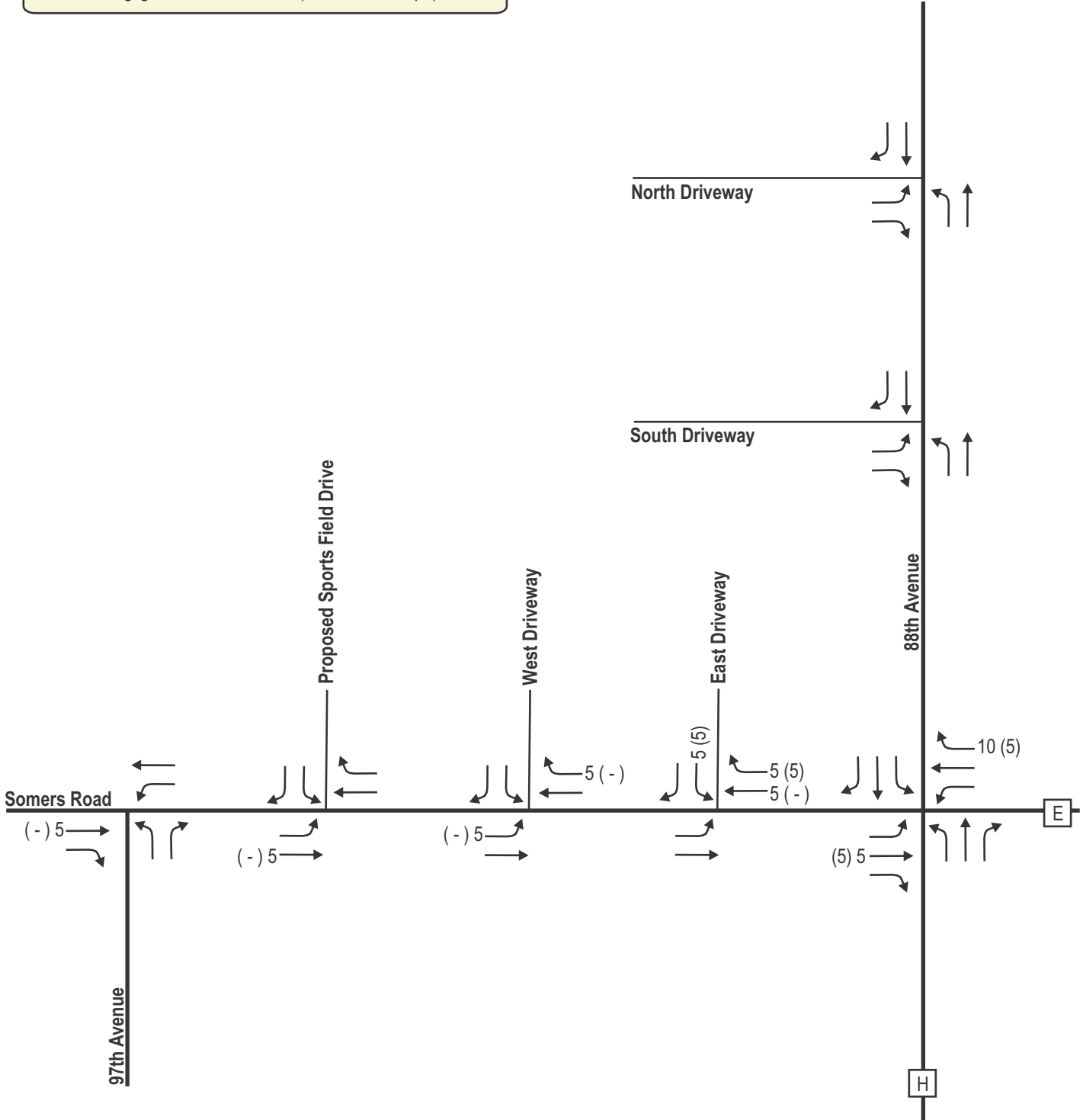


**EXHIBIT 4-5A
INITIAL BUILD NEW TRIPS
NORTH AND SOUTH DRIVEWAYS ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

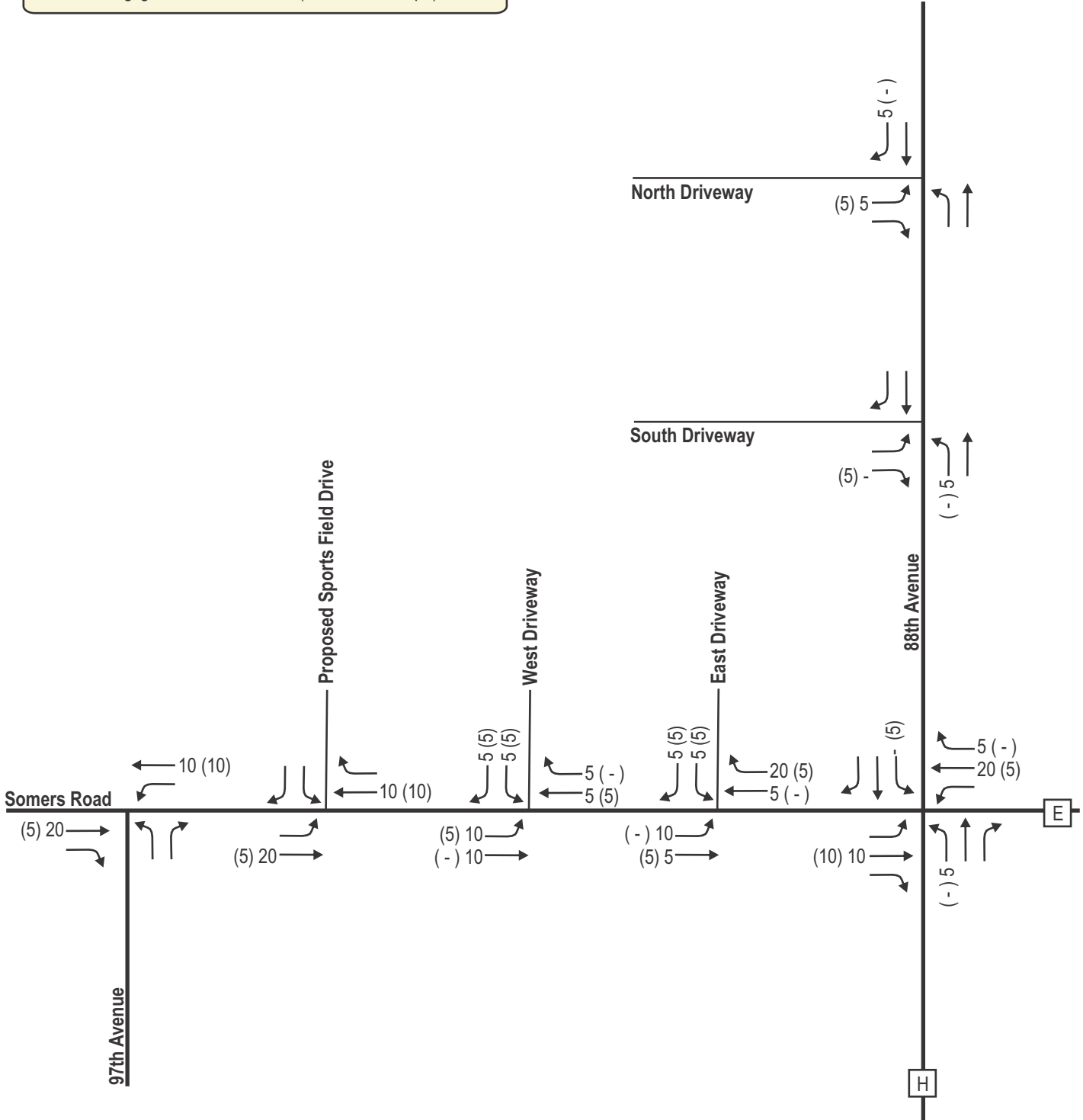


**EXHIBIT 4-5B
INITIAL BUILD NEW TRIPS
NORTH DRIVEWAY ONLY ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

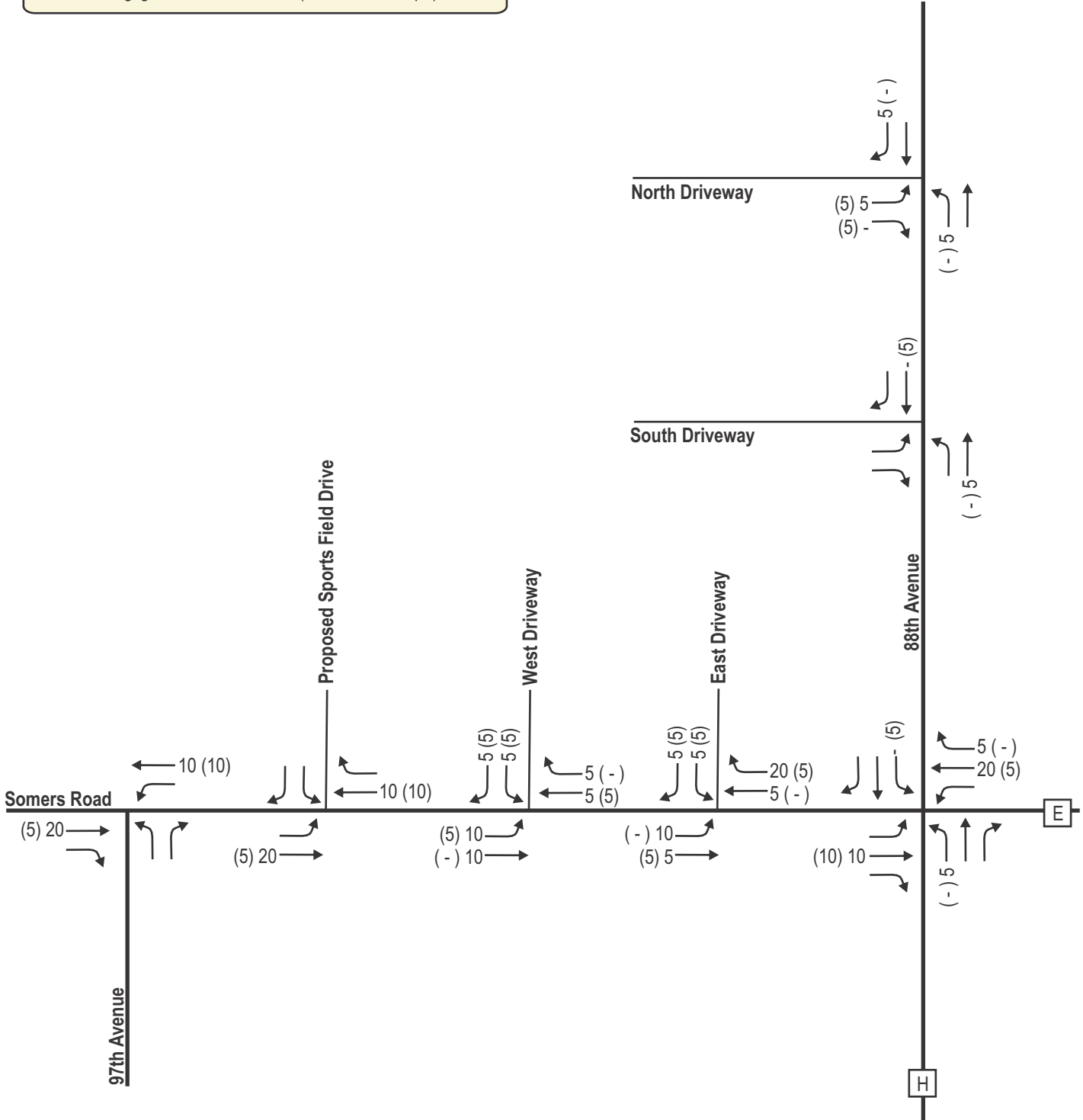


**EXHIBIT 4-8A
FULL BUILD NEW TRIPS
NORTH AND SOUTH DRIVEWAYS ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

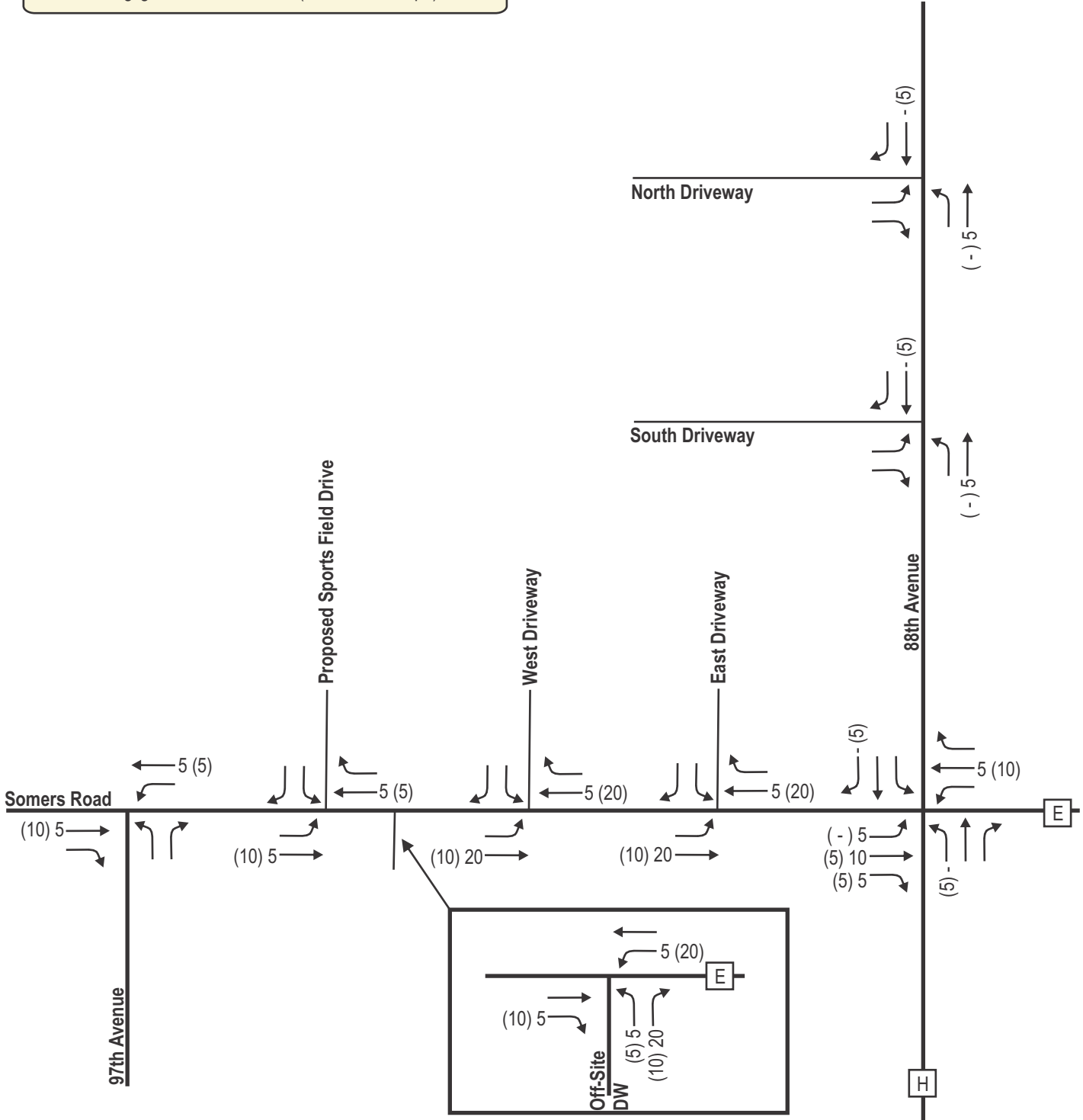


**EXHIBIT 4-8B
FULL BUILD NEW TRIPS
NORTH DRIVEWAY ONLY ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

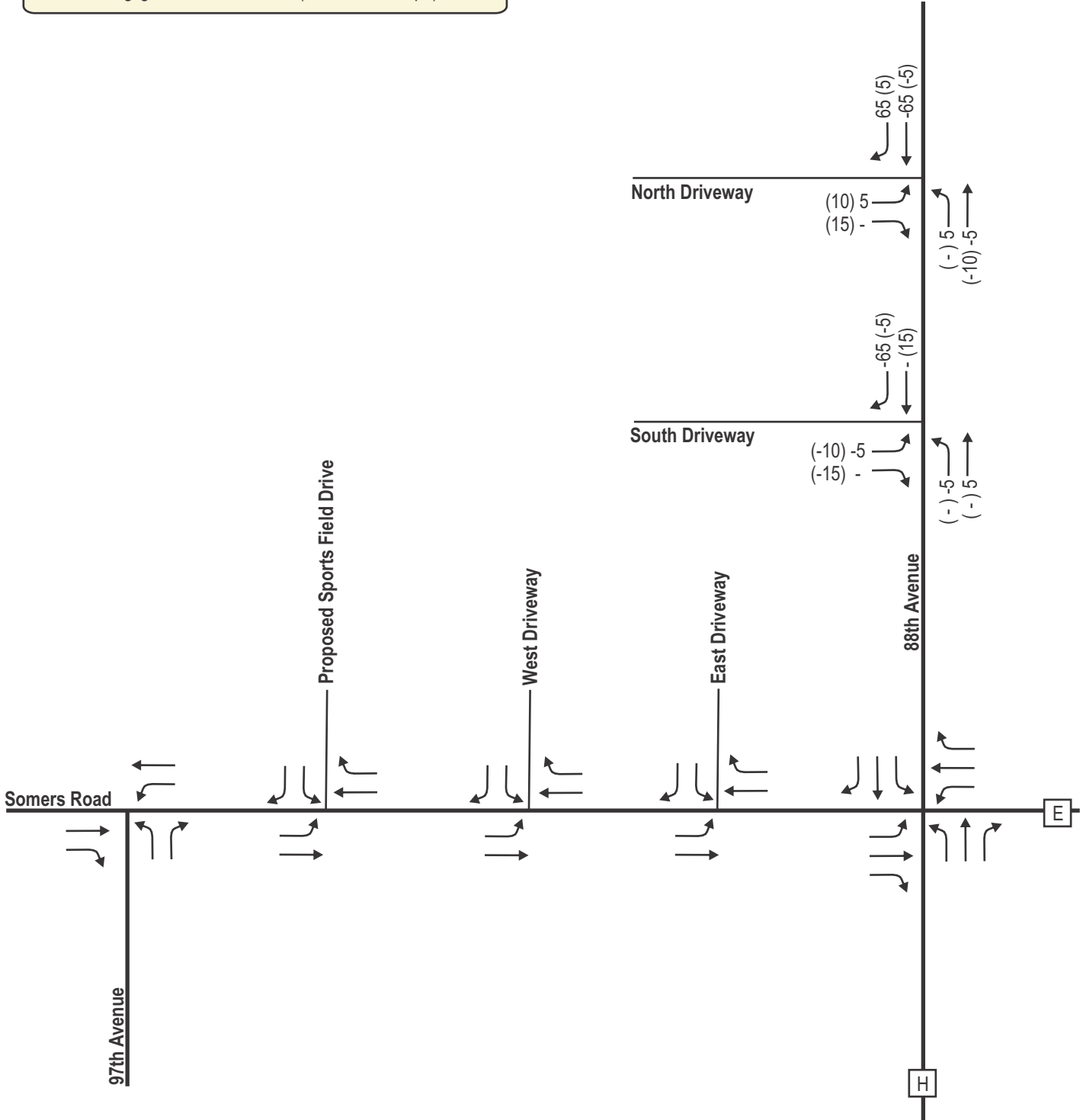


**EXHIBIT 4-9
OFF-SITE NEW TRIPS**

SOMERS, WISCONSIN

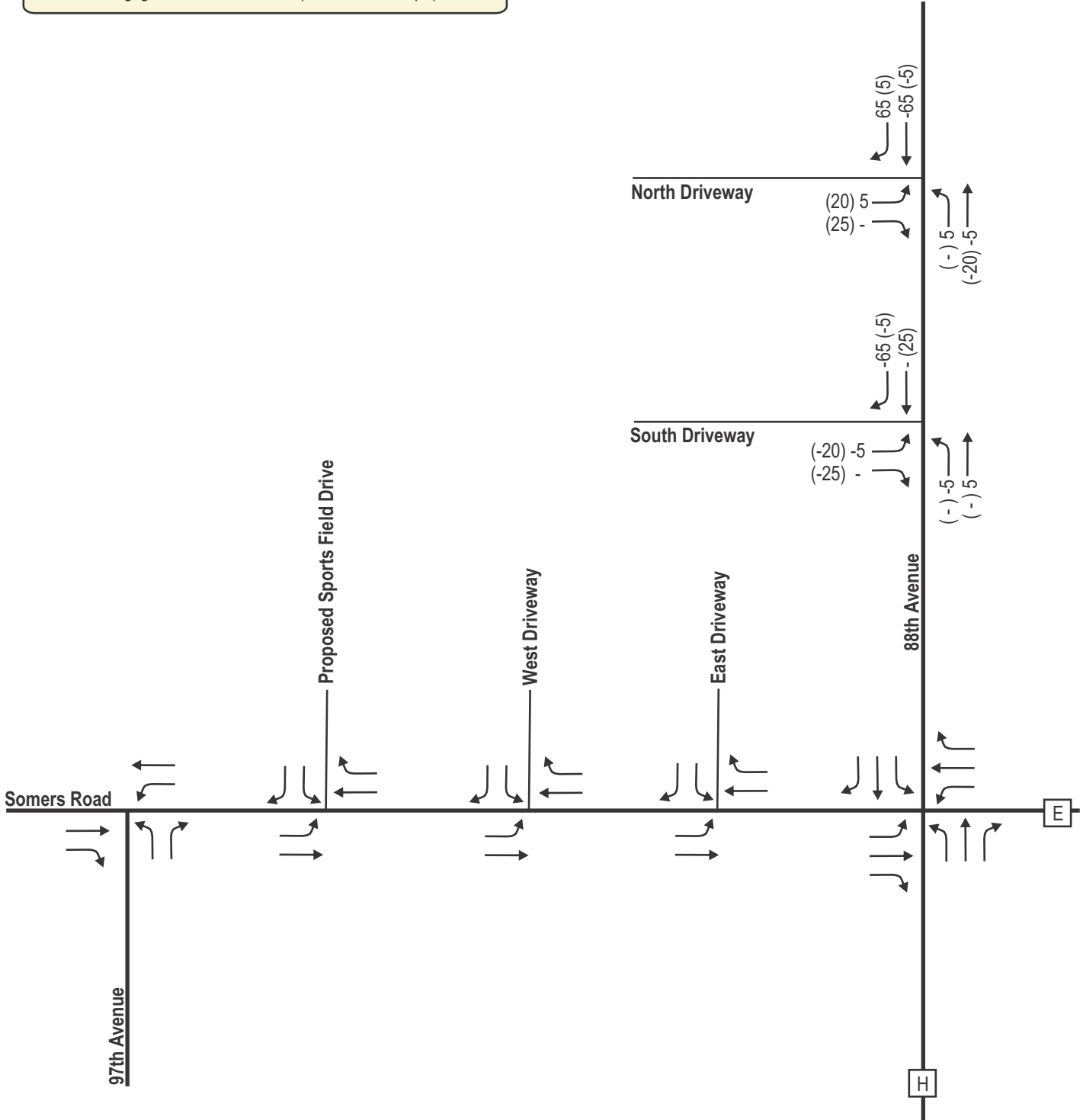
LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

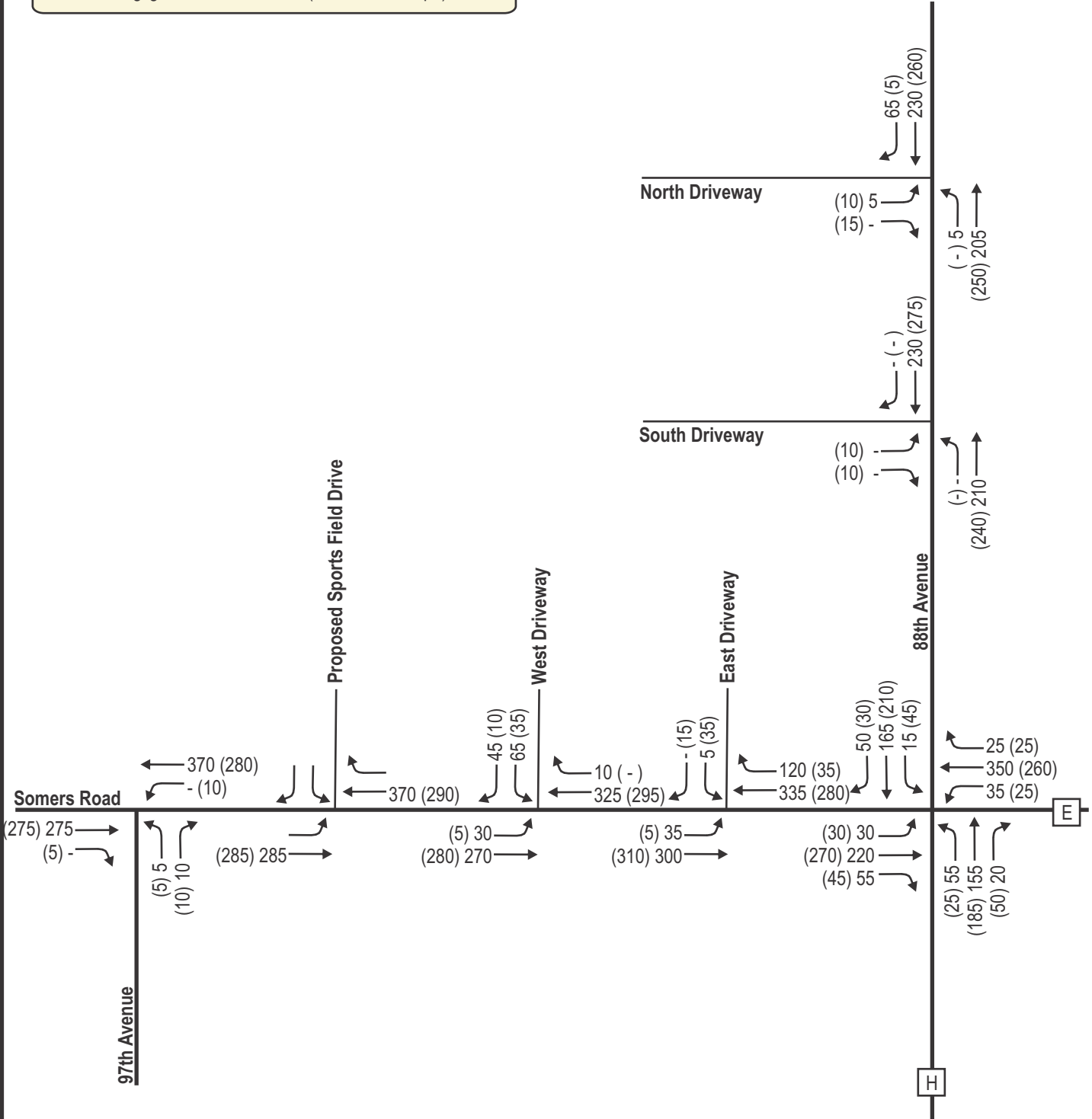


**EXHIBIT 4-10B
REDISTRIBUTED TRIPS
NORTH DRIVEWAY ONLY ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

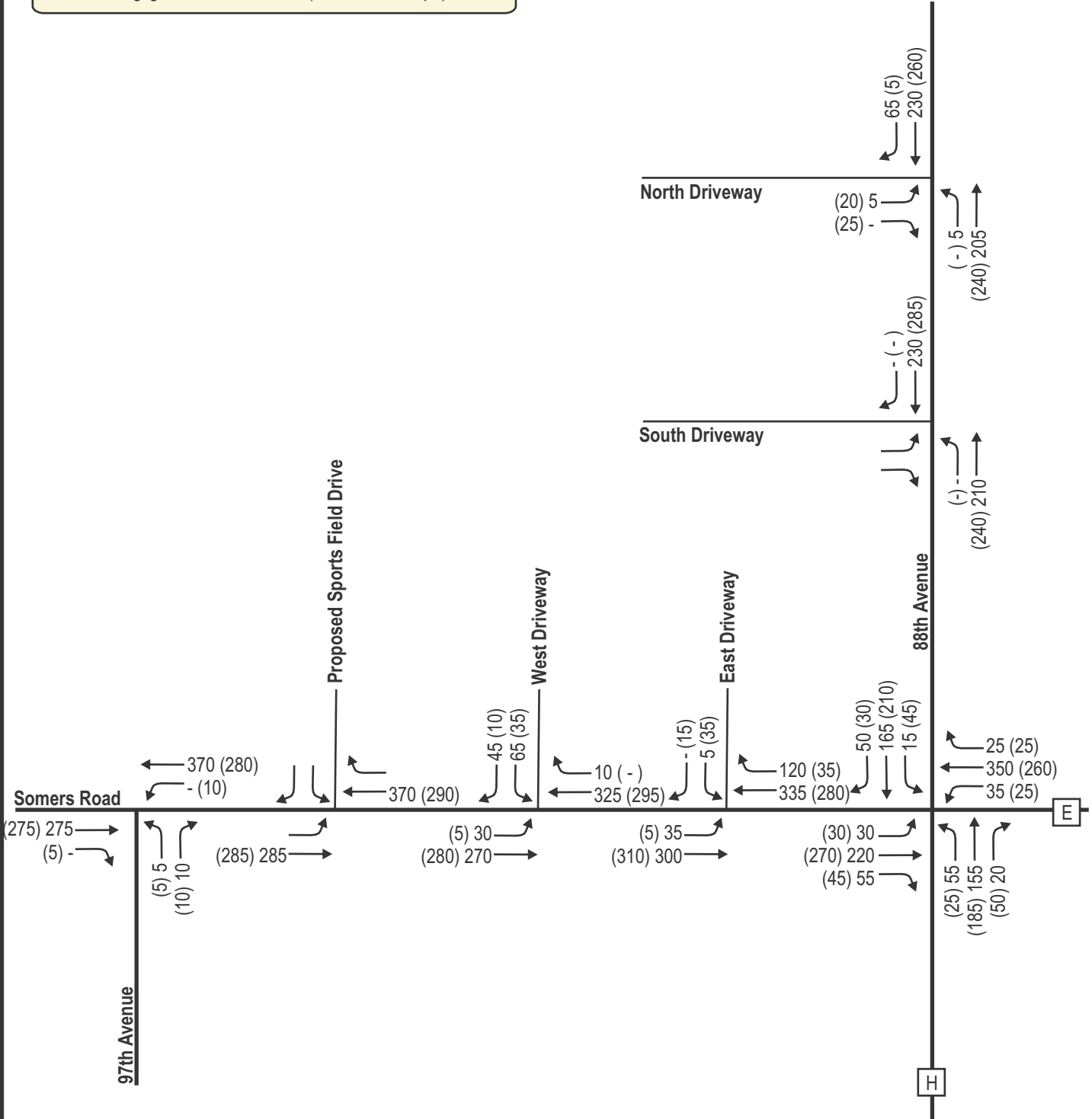


**EXHIBIT 4-11A
YEAR 2025 INITIAL BUILD TRAFFIC VOLUMES
NORTH AND SOUTH DRIVEWAYS ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



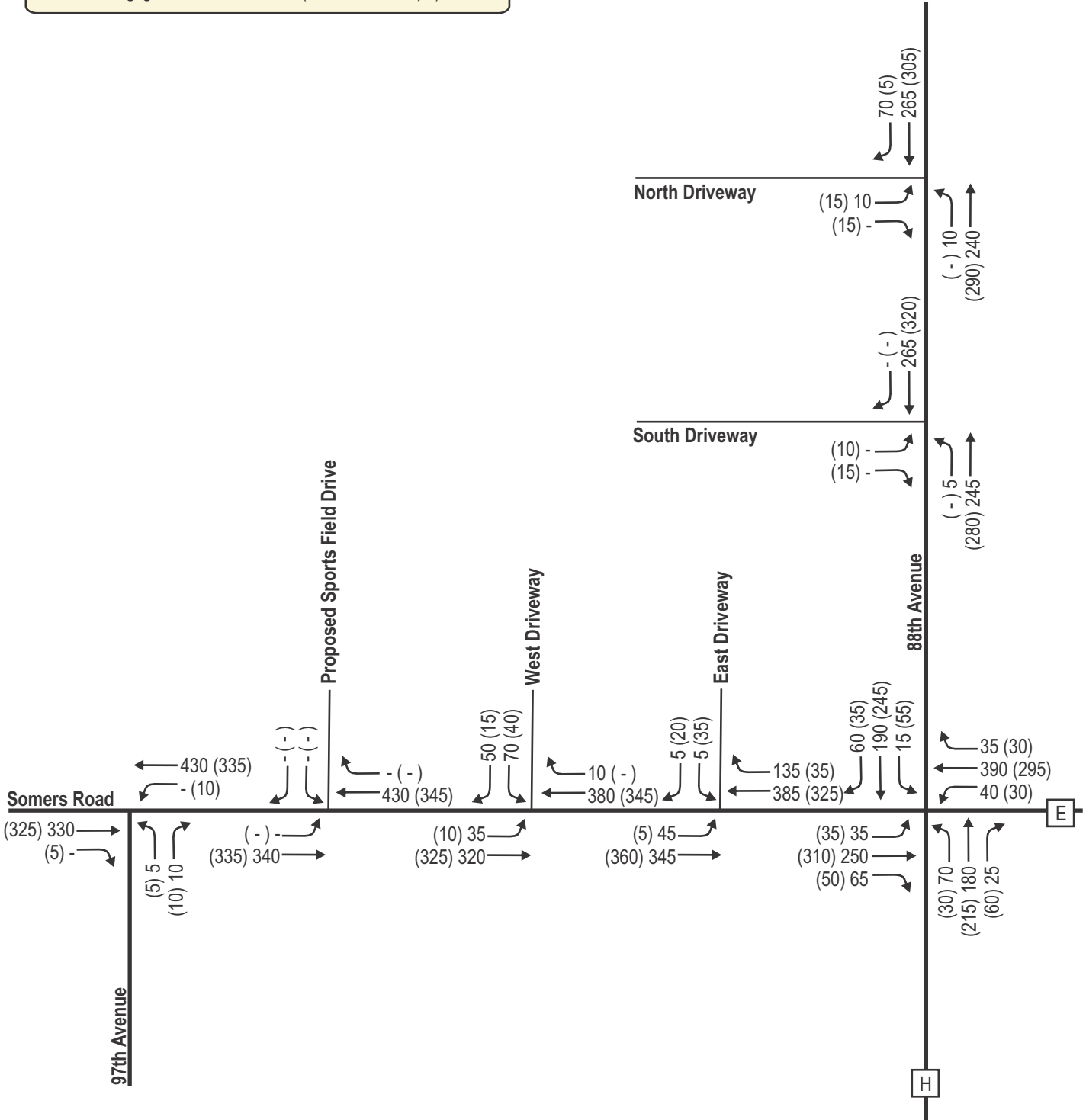
NOT TO SCALE

**EXHIBIT 4-11B
YEAR 2025 INITIAL BUILD TRAFFIC VOLUMES
NORTH DRIVEWAY ONLY ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

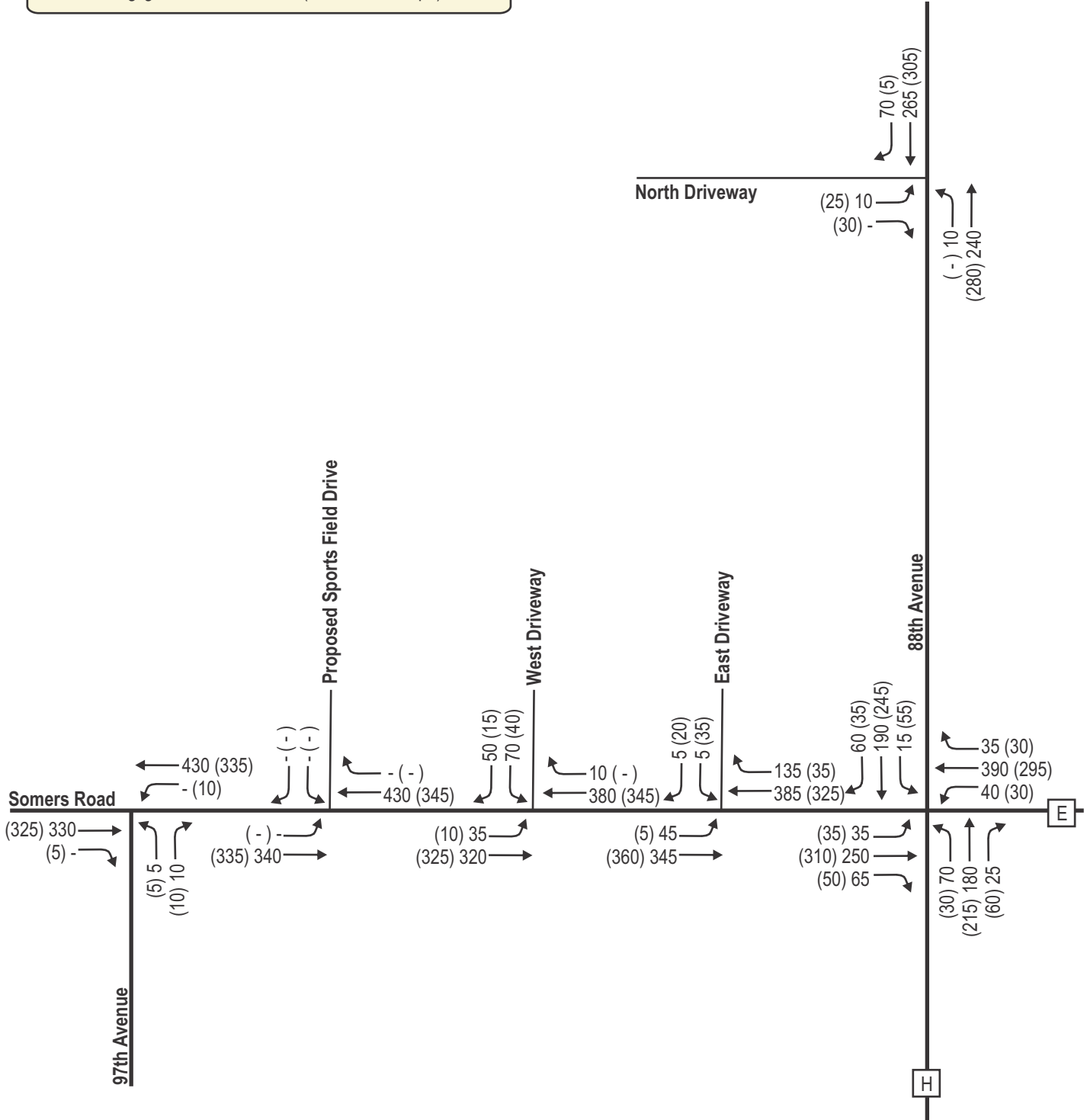


**EXHIBIT 4-14A
YEAR 2035 FULL BUILD TRAFFIC VOLUMES
NORTH AND SOUTH DRIVEWAYS ON CTH H**

SOMERS, WISCONSIN

LEGEND

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



**EXHIBIT 4-14B
YEAR 2035 FULL BUILD TRAFFIC VOLUMES
NORTH DRIVEWAY ONLY ON CTH H**

SOMERS, WISCONSIN

CHAPTER V – TRAFFIC AND IMPROVEMENT ANALYSIS

PART A – FUTURE CAPACITY LEVEL OF SERVICE

A1. Year 2025 Background Traffic Operating Conditions

[Exhibit 5-1](#) shows the Year 2025 Background traffic peak hour operating conditions at the study area intersections without modifications.

Based on the analysis, the study area intersections are expected to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2025 Background traffic peak hour operating conditions, which includes the identified off-site development, except the following:

- The westbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS F during the typical weekday morning arrival peak hour.
- The eastbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS E during the typical weekday afternoon discharge peak hour.

A2. Year 2035 Background Traffic Operating Conditions

[Exhibit 5-2](#) shows the Year 2035 Background traffic peak hour operating conditions at the study area intersections without modifications.

Based on the analysis, the study area intersections are expected to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2035 Background traffic peak hour operating conditions, which includes typical background growth as well as the identified off-site development, except the following:

- The eastbound, westbound, northbound, and southbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS F during the typical weekday morning and afternoon discharge peak hours.

A3. Year 2025 Initial Build Traffic Operating Conditions

As previously discussed, two access scenarios were evaluated as part of this study to analyze the operation of the two potential driveways along CTH H. [Exhibits 5-3A&B](#) show the Year 2025 Initial Build traffic peak hour operating conditions at the study area intersections for the two respective access scenarios along CTH H, without additional modifications. Based on the analysis, *regardless of the access scenario evaluated*, the study area intersections are expected to continue to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2025 Initial Build traffic peak hour operating conditions except:

- The westbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS F during the typical weekday morning arrival peak hour.
- The eastbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS E during the typical weekday afternoon discharge peak hour.

A4. Year 2035 Full Build Traffic Operating Conditions

[Exhibits 5-5A&B](#) show the Year 2035 Full Build traffic peak hour operating conditions at the study area intersections for the two respective access scenarios along CTH H, without additional

modifications. Based on the analysis, *regardless of the access scenario evaluated*, the study area intersections are expected to continue to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2035 Full Build traffic peak hour operating conditions except:

- The eastbound, westbound, northbound, and southbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS F during the typical weekday morning and afternoon discharge peak hours.

A5. Traffic Operating Conditions – With Recommended Modifications

Modifications to accommodate the background and build traffic volumes are summarized in *Chapter VI – Recommendations and Conclusion*. The following outlines the location of the expected peak hour operating conditions with modifications at the study area intersections. The analysis outputs are included in the [Appendix](#) of this report.

- [Exhibit 5-9](#) – Year 2025 Background Traffic Operations – *With Modifications*
- [Exhibit 5-11A](#) – Year 2035 Background Traffic Operations – *With Modifications*
- [Exhibit 5-12A](#) – Year 2025 Initial Build (North and South Driveways on CTH H) Traffic Operations – *With Modifications*
- [Exhibit 5-12B](#) – Year 2025 Initial Build (North Driveway Only on CTH H) Traffic Operations – *With Modifications*
- [Exhibit 5-14A](#) – Year 2035 Full Build (North and South Driveways on CTH H) Traffic Operations – *With Modifications*
- [Exhibit 5-14B](#) – Year 2035 Full Build (North Driveway Only on CTH H) Traffic Operations – *With Modifications*

As shown, all movements are expected to operate desirably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours with recommended modifications with the modified geometry except the southbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS E (5 seconds over threshold) during the typical weekday afternoon discharge peak hours under background traffic conditions and the northbound and southbound movements at the Somers Road intersection with 88th Avenue which are expected to operate at LOS E (2 seconds and 8 seconds over threshold, respectively) during the typical weekday afternoon discharge peak hours under full build traffic conditions.

A sensitivity analysis was also completed at the Somers Road intersection with 88th Avenue looking at the operations if dual lanes were not only constructed along Somers Road (eastbound/westbound lanes) but also along 88th Avenue (northbound/southbound lanes) at the all-way stop-controlled intersection. With the modifications, all movements are expected to operate safely with moderate queue lengths as shown on the following exhibits.

- [Exhibit 5-11B](#) – Year 2035 Background Traffic Operations – *With Modifications, Sensitivity Analysis*
- [Exhibit 5-14C](#) – Year 2035 Full Build (North and South Driveways on CTH H) Traffic Operations – *With Modifications, Sensitivity Analysis*
- [Exhibit 5-14D](#) – Year 2035 Full Build (North Driveway Only on CTH H) Traffic Operations – *With Modifications, Sensitivity Analysis*

As shown, all movements are expected to operate desirably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours with recommended modifications under future year background and full build traffic conditions.

PART B – QUEUEING ANALYSIS

To estimate storage length requirements for turn bays at the study area intersections with modifications, a queuing analysis has been conducted. Note that the 95th percentile probable queue lengths were used for the design of turn bay storage at stop sign and traffic signal-controlled intersections. The following is a list of where the results of the queuing analysis can be found.

- Existing Traffic – [Exhibit 3-3](#)
- Year 2025 Background Traffic – [Exhibits 5-9 & 5-18](#)
- Year 2035 Background Traffic – [Exhibits 5-11A & 5-20A](#)
- Year 2035 Background Traffic, Sensitivity Analysis – [Exhibits 5-11B & 5-20B](#)
- Year 2025 Initial Build (North and South Driveways on CTH H) Traffic – [Exhibits 5-12A & 5-21A](#)
- Year 2025 Initial Build (North Driveway Only on CTH H) Traffic – [Exhibits 5-12B & 5-21B](#)
- Year 2035 Full Build (North and South Driveways on CTH H) Traffic – [Exhibits 5-14A & 5-23A](#)
- Year 2035 Full Build (North Driveway Only on CTH H) Traffic – [Exhibits 5-14B & 5-23B](#)
- Year 2035 Full Build (North and South Driveways on CTH H) Traffic, Sensitivity Analysis – [Exhibits 5-14C & 5-23C](#)
- Year 2035 Full Build (North Driveway Only on CTH H) Traffic, Sensitivity Analysis – [Exhibits 5-14D & 5-23D](#)

Exhibit 5-1
Year 2025 Background Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	Lanes->		-	1		1	-		1					
	AM	LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.6					
		Queue	-	*		25'	-		25'					
	PM	LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.3					
Queue		-	*		25'	-		25'						
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	Lanes->		1	-	-	1			-			1		
	AM	LOS	A	-	-	*			-			C		
		Delay	8.3	-	-	*			-			18.5		
		Queue	25'	-	-	*			-			40'		
	PM	LOS	A	-	-	*			-			B		
		Delay	8.0	-	-	*			-			13.7		
Queue		25'	-	-	*			-			25'			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	Lanes->		1	-	-	1			-			1		
	AM	LOS	A	-	-	*			-			C		
		Delay	8.9	-	-	*			-			15.4		
		Queue	25'	-	-	*			-			25'		
	PM	LOS	A	-	-	*			-			B		
		Delay	8.1	-	-	*			-			13.7		
Queue		25'	-	-	*			-			25'			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	Lanes->		1			1			1			1		
	AM	LOS	D			F			C			C		
		Delay	28.1			52.2			22.4			21.9		
		Queue	145'			285'			95'			95'		
	PM	LOS	E			D			C			D		
		Delay	36.6			30.7			24.9			28.1		
Queue		195'			155'			115'			140'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	Lanes->		1			-			1	-	-	1		
	AM	LOS	B			-			A	*	-	*		
		Delay	12.4			-			8.1	*	-	*		
		Queue	25'			-			25'	*	-	*		
	PM	LOS	B			-			A	*	-	*		
		Delay	12.3			-			8.0	*	-	*		
Queue		25'			-			25'	*	-	*			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-2
Year 2035 Background Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>		Lanes->	-	1		1	-		1		-			
	AM	LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.3		-			
		Queue	-	*		25'	-		25'		-			
	PM	LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		11.9		-			
Queue		-	*		25'	-		25'		-				
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	A	-	-	*	-	-	C					
		Delay	8.5	-	-	*	-	-	21.8					
		Queue	25'	-	-	*	-	-	45'					
	PM	LOS	A	-	-	*	-	-	C					
		Delay	8.1	-	-	*	-	-	15.1					
Queue		25'	-	-	*	-	-	25'						
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	A	-	-	*	-	-	C					
		Delay	9.1	-	-	*	-	-	17.0					
		Queue	25'	-	-	*	-	-	25'					
	PM	LOS	A	-	-	*	-	-	C					
		Delay	8.2	-	-	*	-	-	15.0					
Queue		25'	-	-	*	-	-	25'						
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	F	-	-	F	-	-	E	-	-	E		
		Delay	55.5	-	-	131.6	-	-	37.5	-	-	35.5		
		Queue	245'	-	-	495'	-	-	160'	-	-	150'		
	PM	LOS	F	-	-	F	-	-	F	-	-	F		
		Delay	120.4	-	-	88.4	-	-	61.6	-	-	74.5		
Queue		410'	-	-	315'	-	-	235'	-	-	275'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	B	-	-	A	*	-	*	-	-	*		
		Delay	13.2	-	-	8.2	*	-	*	-	-	*		
		Queue	25'	-	-	25'	*	-	*	-	-	*		
	PM	LOS	B	-	-	A	*	-	*	-	-	*		
		Delay	13.3	-	-	8.2	*	-	*	-	-	*		
Queue		25'	-	-	25'	*	-	*	-	-	*			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-3A
Year 2025 Initial Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*	A	-	B	-						
		Delay	-	*	7.9	-	11.6	-						
	PM	Queue	-	*	25'	-	25'	-						
		LOS	-	*	A	-	B	-						
		Delay	-	*	7.9	-	11.3	-						
Queue	-	*	25'	-	25'	-								
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		1		-			
		LOS	A	-	-	*	-	B	-					
		Delay	8.2	-	-	*	-	12.7	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	B	-					
		Delay	7.9	-	-	*	-	11.5	-					
Queue	25'	-	-	*	-	25'	-							
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		1		-			
		LOS	A	-	-	*	-	C	-					
		Delay	8.3	-	-	*	-	19.0	-					
	PM	Queue	25'	-	-	*	-	40'	-					
		LOS	A	-	-	*	-	B	-					
		Delay	8.0	-	-	*	-	13.7	-					
Queue	25'	-	-	*	-	25'	-							
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		1		-			
		LOS	A	-	-	*	-	C	-					
		Delay	9.0	-	-	*	-	18.5	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	B	-					
		Delay	8.1	-	-	*	-	14.0	-					
Queue	25'	-	-	*	-	25'	-							
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	AM	Lanes->	1			1		1		1				
		LOS	D			F		C		C				
		Delay	30.1			58.0		23.1		22.6				
	PM	Queue	160'			305'		100'		95'				
		LOS	E			D		D		D				
		Delay	39.1			32.5		25.6		28.9				
Queue	205'			165'		120'		140'						
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1				1				1			
		LOS	B			-		A	*	-	*			
		Delay	12.4			-		8.1	*	-	*			
	PM	Queue	25'			-		25'	*	-	*			
		LOS	B			-		A	*	-	*			
		Delay	11.8			-		8.0	*	-	*			
Queue	25'			-		25'	*	-	*					
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1				1				1			
		LOS	B			-		A	*	-	*			
		Delay	11.0			-		7.9	*	-	*			
	PM	Queue	25'			-		25'	*	-	*			
		LOS	B			-		A	*	-	*			
		Delay	12.2			-		8.1	*	-	*			
Queue	25'			-		25'	*	-	*					

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-3B
Year 2025 Initial Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1					
		LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.6					
	PM	Queue	-	*		25'	-		25'					
		LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.3					
Queue	-	*		25'	-		25'							
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1			-			1		
		LOS	A	-	-	*			-			B		
		Delay	8.2	-	-	*			-			12.7		
	PM	Queue	25'	-	-	*			-			25'		
		LOS	A	-	-	*			-			B		
		Delay	7.9	-	-	*			-			11.5		
Queue	25'	-	-	*			-			25'				
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1			-			1		
		LOS	A	-	-	*			-			C		
		Delay	8.3	-	-	*			-			19.0		
	PM	Queue	25'	-	-	*			-			40'		
		LOS	A	-	-	*			-			B		
		Delay	8.0	-	-	*			-			13.7		
Queue	25'	-	-	*			-			25'				
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1			-			1		
		LOS	A	-	-	*			-			C		
		Delay	9.0	-	-	*			-			18.5		
	PM	Queue	25'	-	-	*			-			25'		
		LOS	A	-	-	*			-			B		
		Delay	8.1	-	-	*			-			14.0		
Queue	25'	-	-	*			-			25'				
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	AM	Lanes->	1			1			1			1		
		LOS	D			F			C			C		
		Delay	30.1			58.0			23.1			22.6		
	PM	Queue	160'			305'			100'			95'		
		LOS	E			D			D			D		
		Delay	39.1			32.5			25.6			28.9		
Queue	205'			165'			120'			140'				
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	1		
		LOS	B			-			A	*	-	*		
		Delay	12.4			-			8.1	*	-	*		
	PM	Queue	25'			-			25'	*	-	*		
		LOS	B			-			A	*	-	*		
		Delay	12.3			-			8.0	*	-	*		
Queue	25'			-			25'	*	-	*				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-5A
Year 2035 Full Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.5		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.0		-			
Queue	-	*		25'	-		25'		-					
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		B			
		Delay	8.4	-	-	*	-		-		13.8			
	PM	Queue	25'	-	-	*	-		-		25'			
		LOS	A	-	-	*	-		-		B			
		Delay	8.1	-	-	*	-		-		12.3			
Queue	25'	-	-	*	-		-		25'					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	8.6	-	-	*	-		-		24.8			
	PM	Queue	25'	-	-	*	-		-		60'			
		LOS	A	-	-	*	-		-		C			
		Delay	8.2	-	-	*	-		-		15.3			
Queue	25'	-	-	*	-		-		25'					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	9.3	-	-	*	-		-		18.4			
	PM	Queue	25'	-	-	*	-		-		25'			
		LOS	A	-	-	*	-		-		C			
		Delay	8.2	-	-	*	-		-		15.2			
Queue	25'	-	-	*	-		-		25'					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	AM	Lanes->	1			1			1			1		
		LOS	F			F			E		E			
		Delay	64.2			167.5			41.1		37.7			
	PM	Queue	265'			585'			170'		155'			
		LOS	F			F			F		F			
		Delay	130.1			95.4			61.4		77.0			
Queue	430'			330'			230'		280'					
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	1		
		LOS	B			-			A	*	-	*		
		Delay	13.6			-			8.2	*	-	*		
	PM	Queue	25'			-			25'	*	-	*		
		LOS	B			-			A	*	-	*		
		Delay	13.3			-			8.2	*	-	*		
Queue	25'			-			25'	*	-	*				
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	1		
		LOS	B			-			A	*	-	*		
		Delay	11.7			-			8.0	*	-	*		
	PM	Queue	25'			-			25'	*	-	*		
		LOS	B			-			A	*	-	*		
		Delay	12.8			-			8.2	*	-	*		
Queue	25'			-			25'	*	-	*				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-5B
Year 2035 Full Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.5		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.0		-			
Queue	-	*		25'	-		25'		-					
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	B		-			
		Delay	8.4	-	-	*	-	-	13.8		-			
	PM	Queue	25'	-	-	*	-	-	25'		-			
		LOS	A	-	-	*	-	-	B		-			
		Delay	8.1	-	-	*	-	-	12.3		-			
Queue	25'	-	-	*	-	-	25'		-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	C		-			
		Delay	8.6	-	-	*	-	-	24.8		-			
	PM	Queue	25'	-	-	*	-	-	60'		-			
		LOS	A	-	-	*	-	-	C		-			
		Delay	8.2	-	-	*	-	-	15.3		-			
Queue	25'	-	-	*	-	-	25'		-					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	C		-			
		Delay	9.3	-	-	*	-	-	18.4		-			
	PM	Queue	25'	-	-	*	-	-	25'		-			
		LOS	A	-	-	*	-	-	C		-			
		Delay	8.2	-	-	*	-	-	15.2		-			
Queue	25'	-	-	*	-	-	25'		-					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	AM	Lanes->	1			1			1			1		
		LOS	F			F			E			E		
		Delay	64.2			167.5			41.1			37.7		
	PM	Queue	265'			585'			170'			155'		
		LOS	F			F			F			F		
		Delay	130.1			95.4			61.4			77.0		
Queue	430'			330'			230'			280'				
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	-	1	
		LOS	B			-			A	*	-	-	*	
		Delay	13.9			-			8.3	*	-	-	*	
	PM	Queue	25'			-			25'	*	-	-	*	
		LOS	B			-			A	*	-	-	*	
		Delay	13.6			-			8.2	*	-	-	*	
Queue	25'			-			25'	*	-	-	*			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

Exhibit 5-9
Year 2025 Background Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.6		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.3		-			
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	8.3	-	-	*	-		-		18.5			
	PM	Queue	25'	-	-	*	-		-		40'			
		LOS	A	-	-	*	-		-		B			
		Delay	8.0	-	-	*	-		-		13.7			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	8.9	-	-	*	-		-		15.4			
	PM	Queue	25'	-	-	*	-		-		25'			
		LOS	A	-	-	*	-		-		B			
		Delay	8.1	-	-	*	-		-		13.7			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1	1		1		
		LOS	B		B	C		C	C	C		C		
		Delay	14.2		14.4	16.8		15.6	18.1	17.7		17.7		
	PM	Queue	40'		45'	65'		60'	80'	75'		75'		
		LOS	C		C	C		B	C	C		C		
		Delay	15.4		15.4	15.2		14.7	19.7	21.6		21.6		
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-		1	-	-		1		
		LOS	B			-		A	*	-		*		
		Delay	12.4			-		8.1	*	-		*		
	PM	Queue	25'			-		25'	*	-		*		
		LOS	B			-		A	*	-		*		
		Delay	12.3			-		8.0	*	-		*		
Queue	25'			-		25'	*	-		*				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes consist of a shared left/through lane and a shared right/through lane

Exhibit 5-11A
Year 2035 Background Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.3		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		11.9		-			
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		C		-			
		Delay	8.5	-	-	*	-		21.8		-			
	PM	Queue	25'	-	-	*	-		45'		-			
		LOS	A	-	-	*	-		C		-			
		Delay	8.1	-	-	*	-		15.1		-			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		C		-			
		Delay	9.1	-	-	*	-		17.0		-			
	PM	Queue	25'	-	-	*	-		25'		-			
		LOS	A	-	-	*	-		C		-			
		Delay	8.2	-	-	*	-		15.0		-			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1		1			
		LOS	C		C	C		C	D		D			
		Delay	17.3		18.1	22.0		20.1	26.8		25.5			
	PM	Queue	55'		65'	95'		85'	130'		120'			
		LOS	C		C	C		C	D		E			
		Delay	20.3		20.5	19.7		18.9	34.0		39.8			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-		1	-		-	1		
		LOS	B			-		A	*		-	*		
		Delay	13.2			-		8.2	*		-	*		
	PM	Queue	25'			-		25'	*		-	*		
		LOS	B			-		A	*		-	*		
		Delay	13.3			-		8.2	*		-	*		
Queue	25'			-		25'	*		-	*				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane

Exhibit 5-11B
Year 2035 Background Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control - Sensitivity Analysis

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.3		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		11.9		-			
Queue	-	*		25'	-		25'		-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1		-	-		1		-		1		
		LOS	A		-	-		*		-		C		
		Delay	8.5		-	-		*		-		21.8		
	PM	Queue	25'		-	-		*		-		45'		
		LOS	A		-	-		*		-		C		
		Delay	8.1		-	-		*		-		15.1		
Queue	25'		-	-		*		-		25'				
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1		-	-		1		-		1		
		LOS	A		-	-		*		-		C		
		Delay	9.1		-	-		*		-		17.0		
	PM	Queue	25'		-	-		*		-		25'		
		LOS	A		-	-		*		-		C		
		Delay	8.2		-	-		*		-		15.0		
Queue	25'		-	-		*		-		25'				
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1		1	1	1	
		LOS	C		C	C		C	C		B	B	C	
		Delay	16.2		16.9	20.1		18.4	17.3		14.3	14.6	16.0	
	PM	Queue	50'		60'	85'		75'	55'		30'	30'	50'	
		LOS	C		C	C		C	C		C	C	C	
		Delay	18.1		18.1	17.7		17.0	15.9		16.6	17.9	16.0	
Queue	65'		70'	60'		55'	40'		55'	60'	50'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1		-	-	1	
		LOS	B			-			A		*	-	*	
		Delay	13.2			-			8.2		*	-	*	
	PM	Queue	25'			-			25'		*	-	*	
		LOS	B			-			A		*	-	*	
		Delay	13.3			-			8.2		*	-	*	
Queue	25'			-			25'		*	-	*			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane

Exhibit 5-12A
Year 2025 Initial Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach										
			Eastbound			Westbound			Northbound		Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-
		LOS	-	*	A	-	B	-	-				
		Delay	-	*	7.9	-	11.6	-	-				
	PM	Queue	-	*	25'	-	25'	-	-				
		LOS	-	*	A	-	B	-	-				
		Delay	-	*	7.9	-	11.3	-	-				
Queue	-	*	25'	-	25'	-	-						
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1	
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.2	-	-	*	-	12.7	-	-			
	PM	Queue	25'	-	-	*	-	25'	-	-			
		LOS	A	-	-	*	-	B	-	-			
		Delay	7.9	-	-	*	-	11.5	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1	
		LOS	A	-	-	*	-	C	-	-			
		Delay	8.3	-	-	*	-	19.0	-	-			
	PM	Queue	25'	-	-	*	-	40'	-	-			
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.0	-	-	*	-	13.7	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1	
		LOS	A	-	-	*	-	C	-	-			
		Delay	9.0	-	-	*	-	18.5	-	-			
	PM	Queue	25'	-	-	*	-	25'	-	-			
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.1	-	-	*	-	14.0	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1	1	1	1	1	1	1	1	1		
		LOS	B	B	C	C	C	C	C				
		Delay	14.4	14.7	17.2	16.0	18.3	18.0	18.0				
	PM	Queue	40'	45'	65'	60'	80'	80'	80'				
		LOS	C	C	C	B	C	C	C				
		Delay	15.7	15.6	15.4	14.9	19.9	21.9	21.9				
Queue	50'	55'	45'	40'	95'	110'	110'						
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	1		
		LOS	B	-	-	A	*	-	*				
		Delay	12.4	-	-	8.1	*	-	*				
	PM	Queue	25'	-	-	25'	*	-	*				
		LOS	B	-	-	A	*	-	*				
		Delay	11.8	-	-	8.0	*	-	*				
Queue	25'	-	-	25'	*	-	*						
Node 700: 88th Avenue/CTH H & South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	1		
		LOS	B	-	-	A	*	-	*				
		Delay	11.0	-	-	7.9	*	-	*				
	PM	Queue	25'	-	-	25'	*	-	*				
		LOS	B	-	-	A	*	-	*				
		Delay	12.2	-	-	8.1	*	-	*				
Queue	25'	-	-	25'	*	-	*						

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes consist of a shared left/through lane and a shared right/through lane



EXHIBIT 5-12A
INITIAL BUILD TRAFFIC OPERATIONS
WITH MODIFICATIONS
NORTH AND SOUTH DRIVEWAYS ON CTH H

SOMERS, WISCONSIN

Exhibit 5-12B
Year 2025 Initial Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-	-	-	-	-	
		Delay	-	*	7.9	-	11.6	-	-	-	-	-	-	
	PM	Queue	-	*	25'	-	25'	-	-	-	-	-	-	
		LOS	-	*	A	-	B	-	-	-	-	-	-	
		Delay	-	*	7.9	-	11.3	-	-	-	-	-	-	
Queue	-	*	25'	-	25'	-	-	-	-	-	-	-		
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	8.2	-	-	*	-	-	-	-	-	12.7		
	PM	Queue	25'	-	-	*	-	-	-	-	-	25'		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	7.9	-	-	*	-	-	-	-	-	11.5		
Queue	25'	-	-	*	-	-	-	-	-	25'	-			
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	-	C		
		Delay	8.3	-	-	*	-	-	-	-	-	19.0		
	PM	Queue	25'	-	-	*	-	-	-	-	-	40'		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	8.0	-	-	*	-	-	-	-	-	13.7		
Queue	25'	-	-	*	-	-	-	-	-	25'	-			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	-	C		
		Delay	9.0	-	-	*	-	-	-	-	-	18.5		
	PM	Queue	25'	-	-	*	-	-	-	-	-	25'		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	8.1	-	-	*	-	-	-	-	-	14.0		
Queue	25'	-	-	*	-	-	-	-	-	25'	-			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1	1	1	1		
		LOS	B		B	C		C	C	C	C	C		
		Delay	14.4		14.7	17.2		16.0	18.3	18.0	18.0	18.0		
	PM	Queue	40'		45'	65'		60'	80'	80'	80'	80'		
		LOS	C		C	C		B	C	C	C	C		
		Delay	15.7		15.6	15.4		14.9	19.9	21.9	21.9	21.9		
Queue	50'		55'	45'		40'	95'	110'	110'	110'				
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1		-			1	-	-	-	1		
		LOS	B		-			A	*	-	-	*		
		Delay	12.4		-			8.1	*	-	-	*		
	PM	Queue	25'		-			25'	*	-	-	*		
		LOS	B		-			A	*	-	-	*		
		Delay	12.3		-			8.0	*	-	-	*		
Queue	25'		-			25'	*	-	-	*				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.

Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

¹ EB/WB lanes consist of a shared left/through lane and a shared right/through lane

Exhibit 5-14A
Year 2035 Full Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach										
			Eastbound			Westbound			Northbound		Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-
		LOS	-	*	A	-	B	-	-				
		Delay	-	*	8.0	-	12.5	-	-				
	PM	Queue	-	*	25'	-	25'	-	-				
		LOS	-	*	A	-	B	-	-				
		Delay	-	*	8.0	-	12.0	-	-				
Queue	-	*	25'	-	25'	-	-						
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	-	
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.4	-	-	*	-	13.8	-	-			
	PM	Queue	25'	-	-	*	-	25'	-	-			
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.1	-	-	*	-	12.3	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-		
		LOS	A	-	-	*	-	C	-	-			
		Delay	8.6	-	-	*	-	24.8	-	-			
	PM	Queue	25'	-	-	*	-	60'	-	-			
		LOS	A	-	-	*	-	C	-	-			
		Delay	8.2	-	-	*	-	15.3	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-		
		LOS	A	-	-	*	-	C	-	-			
		Delay	9.3	-	-	*	-	18.4	-	-			
	PM	Queue	25'	-	-	*	-	25'	-	-			
		LOS	A	-	-	*	-	C	-	-			
		Delay	8.2	-	-	*	-	15.2	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1	-	1	1	1	1	1	1	1		
		LOS	C	-	C	C	C	D	D	D			
		Delay	18.0	-	19.1	23.9	22.2	29.1	26.9	26.9			
	PM	Queue	55'	-	70'	105'	95'	140'	105'	105'			
		LOS	C	-	C	C	C	E	E	E			
		Delay	21.1	-	21.4	20.3	19.5	35.5	43.1	43.1			
Queue	75'	-	80'	70'	65'	175'	215'	215'					
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	1		
		LOS	B	-	-	A	*	-	*				
		Delay	13.6	-	-	8.2	*	-	*				
	PM	Queue	25'	-	-	25'	*	-	*				
		LOS	B	-	-	A	*	-	*				
		Delay	13.3	-	-	8.2	*	-	*				
Queue	25'	-	-	25'	*	-	*						
Node 700: 88th Avenue/CTH H & South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	1		
		LOS	B	-	-	A	*	-	*				
		Delay	11.7	-	-	8.0	*	-	*				
	PM	Queue	25'	-	-	25'	*	-	*				
		LOS	B	-	-	A	*	-	*				
		Delay	12.8	-	-	8.2	*	-	*				
Queue	25'	-	-	25'	*	-	*						

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane



EXHIBIT 5-14A
FULL BUILD TRAFFIC OPERATIONS
WITH MODIFICATIONS
NORTH AND SOUTH DRIVEWAYS ON CTH H

SOMERS, WISCONSIN

Exhibit 5-14B
Year 2035 Full Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.5	-	-					
	PM	Queue	-	*	25'	-	25'	-	-					
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.0	-	-					
Queue	-	*	25'	-	25'	-	-							
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1		
		LOS	A	-	-	*	-	B	-	-				
		Delay	8.4	-	-	*	-	13.8	-	-				
	PM	Queue	25'	-	-	*	-	25'	-	-				
		LOS	A	-	-	*	-	B	-	-				
		Delay	8.1	-	-	*	-	12.3	-	-				
Queue	25'	-	-	*	-	25'	-	-						
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1		
		LOS	A	-	-	*	-	C	-	-				
		Delay	8.6	-	-	*	-	24.8	-	-				
	PM	Queue	25'	-	-	*	-	60'	-	-				
		LOS	A	-	-	*	-	C	-	-				
		Delay	8.2	-	-	*	-	15.3	-	-				
Queue	25'	-	-	*	-	25'	-	-						
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1		
		LOS	A	-	-	*	-	C	-	-				
		Delay	9.3	-	-	*	-	18.4	-	-				
	PM	Queue	25'	-	-	*	-	25'	-	-				
		LOS	A	-	-	*	-	C	-	-				
		Delay	8.2	-	-	*	-	15.2	-	-				
Queue	25'	-	-	*	-	25'	-	-						
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1		1			
		LOS	C		C	C		C	D		D			
		Delay	18.0		19.1	23.9		22.2	29.1		26.9			
	PM	Queue	55'		70'	105'		95'	140'		105'			
		LOS	C		C	C		C	E		E			
		Delay	21.1		21.4	20.3		19.5	35.5		43.1			
Queue	75'		80'	70'		65'	175'		215'					
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1		-			1	-	-	-	1		
		LOS	B		-			A	*	-	-	*		
		Delay	13.9		-			8.3	*	-	-	*		
	PM	Queue	25'		-			25'	*	-	-	*		
		LOS	B		-			A	*	-	-	*		
		Delay	13.6		-			8.2	*	-	-	*		
Queue	25'		-			25'	*	-	-	*				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane



EXHIBIT 5-14B
FULL BUILD TRAFFIC OPERATIONS
WITH MODIFICATIONS
NORTH DRIVEWAY ONLY ON CTH H

SOMERS, WISCONSIN

Exhibit 5-14C
Year 2035 Full Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control - Sensitivity Analysis

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.5	-	-					
	PM	Queue	-	*	25'	-	25'	-	-					
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.0	-	-					
Queue	-	*	25'	-	25'	-	-							
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-	-		
		LOS	A	-	-	*	-	B	-					
		Delay	8.4	-	-	*	-	13.8	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	B	-					
		Delay	8.1	-	-	*	-	12.3	-					
Queue	25'	-	-	*	-	25'	-							
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C	-					
		Delay	8.6	-	-	*	-	24.8	-					
	PM	Queue	25'	-	-	*	-	60'	-					
		LOS	A	-	-	*	-	C	-					
		Delay	8.2	-	-	*	-	15.3	-					
Queue	25'	-	-	*	-	25'	-							
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C	-					
		Delay	9.3	-	-	*	-	18.4	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	C	-					
		Delay	8.2	-	-	*	-	15.2	-					
Queue	25'	-	-	*	-	25'	-							
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1	1	1	1	1	1	1	1	1			
		LOS	C	C	C	C	C	B	B	C				
		Delay	16.8	17.6	21.5	20.0	18.1	14.6	14.9	16.5				
	PM	Queue	55'	65'	95'	90'	55'	35'	35'	50'				
		LOS	C	C	C	C	C	C	C	C				
		Delay	18.6	18.7	18.1	17.4	16.2	16.9	18.5	16.2				
Queue	65'	70'	60'	60'	45'	55'	65'	50'						
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	B	-	-	A	*	-	*					
		Delay	13.6	-	-	8.2	*	-	*					
	PM	Queue	25'	-	-	25'	*	-	*					
		LOS	B	-	-	A	*	-	*					
		Delay	13.3	-	-	8.2	*	-	*					
Queue	25'	-	-	25'	*	-	*							
Node 700: 88th Avenue/CTH H & South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	B	-	-	A	*	-	*					
		Delay	11.7	-	-	8.0	*	-	*					
	PM	Queue	25'	-	-	25'	*	-	*					
		LOS	B	-	-	A	*	-	*					
		Delay	12.8	-	-	8.2	*	-	*					
Queue	25'	-	-	25'	*	-	*							

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane



EXHIBIT 5-14C
FULL BUILD TRAFFIC OPERATIONS
WITH MODIFICATIONS - SENSITIVITY ANALYSIS
NORTH AND SOUTH DRIVEWAYS ON CTH H

SOMERS, WISCONSIN

Exhibit 5-14D
Year 2035 Full Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions
With Modified Geometrics and Traffic Control - Sensitivity Analysis

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-	-	-	-		
		Delay	-	*	8.0	-	12.5	-	-	-	-	-		
	PM	Queue	-	*	25'	-	25'	-	-	-	-	-		
		LOS	-	*	A	-	B	-	-	-	-	-		
		Delay	-	*	8.0	-	12.0	-	-	-	-	-		
Queue	-	*	25'	-	25'	-	-	-	-	-				
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	B			
		Delay	8.4	-	-	*	-	-	-	-	13.8			
	PM	Queue	25'	-	-	*	-	-	-	-	25'			
		LOS	A	-	-	*	-	-	-	-	B			
		Delay	8.1	-	-	*	-	-	-	-	12.3			
Queue	25'	-	-	*	-	-	-	-	25'					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	C			
		Delay	8.6	-	-	*	-	-	-	-	24.8			
	PM	Queue	25'	-	-	*	-	-	-	-	60'			
		LOS	A	-	-	*	-	-	-	-	C			
		Delay	8.2	-	-	*	-	-	-	-	15.3			
Queue	25'	-	-	*	-	-	-	-	25'					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	C			
		Delay	9.3	-	-	*	-	-	-	-	18.4			
	PM	Queue	25'	-	-	*	-	-	-	-	25'			
		LOS	A	-	-	*	-	-	-	-	C			
		Delay	8.2	-	-	*	-	-	-	-	15.2			
Queue	25'	-	-	*	-	-	-	-	25'					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H ¹ <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1		1	1		
		LOS	C		C	C		C	C		B	B	C	
		Delay	16.8		17.6	21.5		20.0	18.1		14.6	14.9	16.5	
	PM	Queue	55'		65'	95'		90'	55'		35'	35'	50'	
		LOS	C		C	C		C	C		C	C	C	
		Delay	18.6		18.7	18.1		17.4	16.2		16.9	18.5	16.2	
Queue	65'		70'	60'		60'	45'		55'	65'	50'			
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	-	1	-	-	-	-	1		
		LOS	B	-	-	-	A	*	-	-	-	*		
		Delay	13.9	-	-	-	8.3	*	-	-	-	*		
	PM	Queue	25'	-	-	-	25'	*	-	-	-	*		
		LOS	B	-	-	-	A	*	-	-	-	*		
		Delay	13.6	-	-	-	8.2	*	-	-	-	*		
Queue	25'	-	-	-	25'	*	-	-	-	*				



(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement.
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
¹ EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane

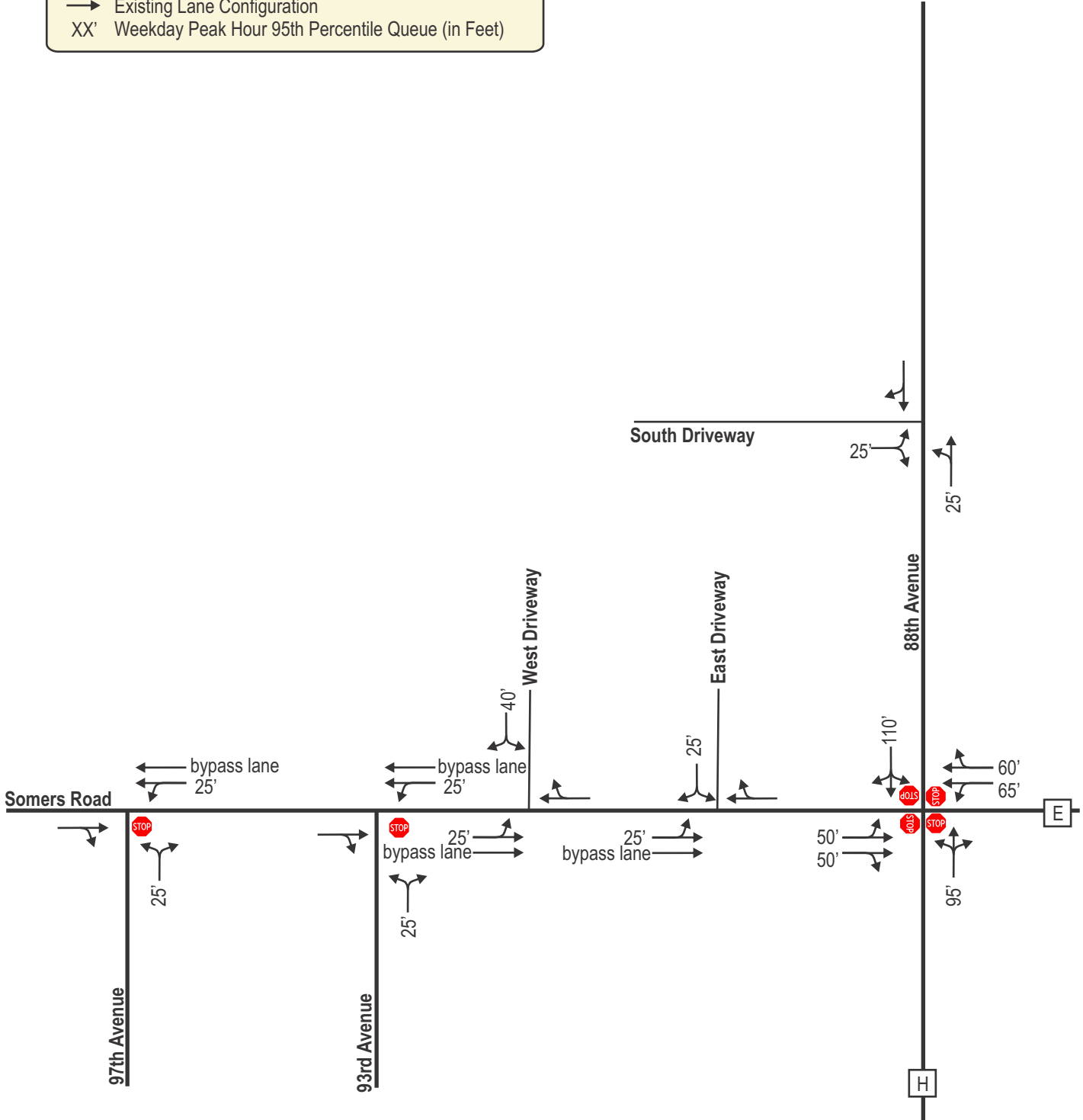


EXHIBIT 5-14D
FULL BUILD TRAFFIC OPERATIONS
WITH MODIFICATIONS - SENSITIVITY ANALYSIS
NORTH DRIVEWAY ONLY ON CTH H

SOMERS, WISCONSIN

LEGEND



-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)

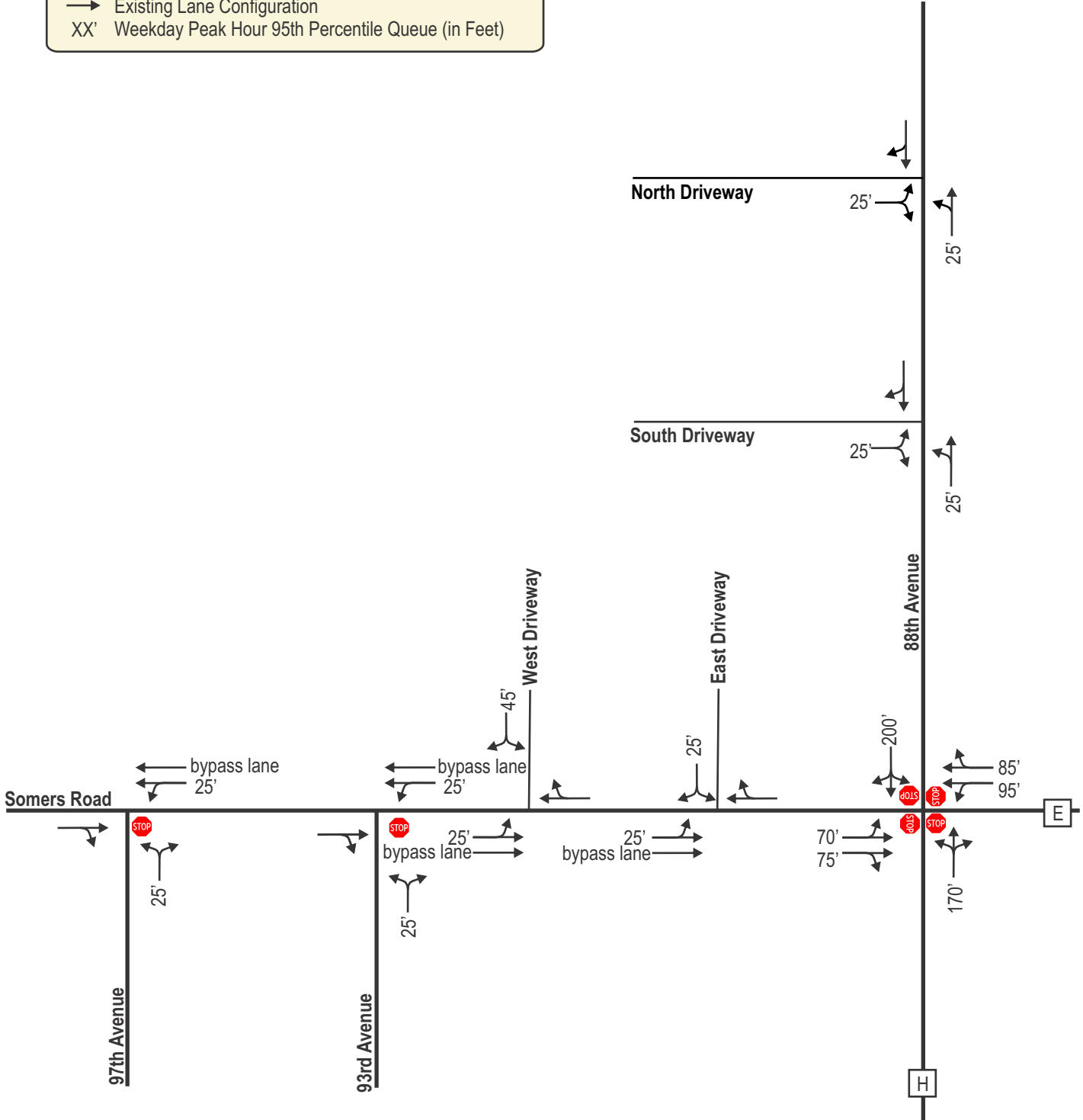


**EXHIBIT 5-18
YEAR 2025 BACKGROUND TRAFFIC
MAXIMUM QUEUE LENGTHS**



SOMERS, WISCONSIN

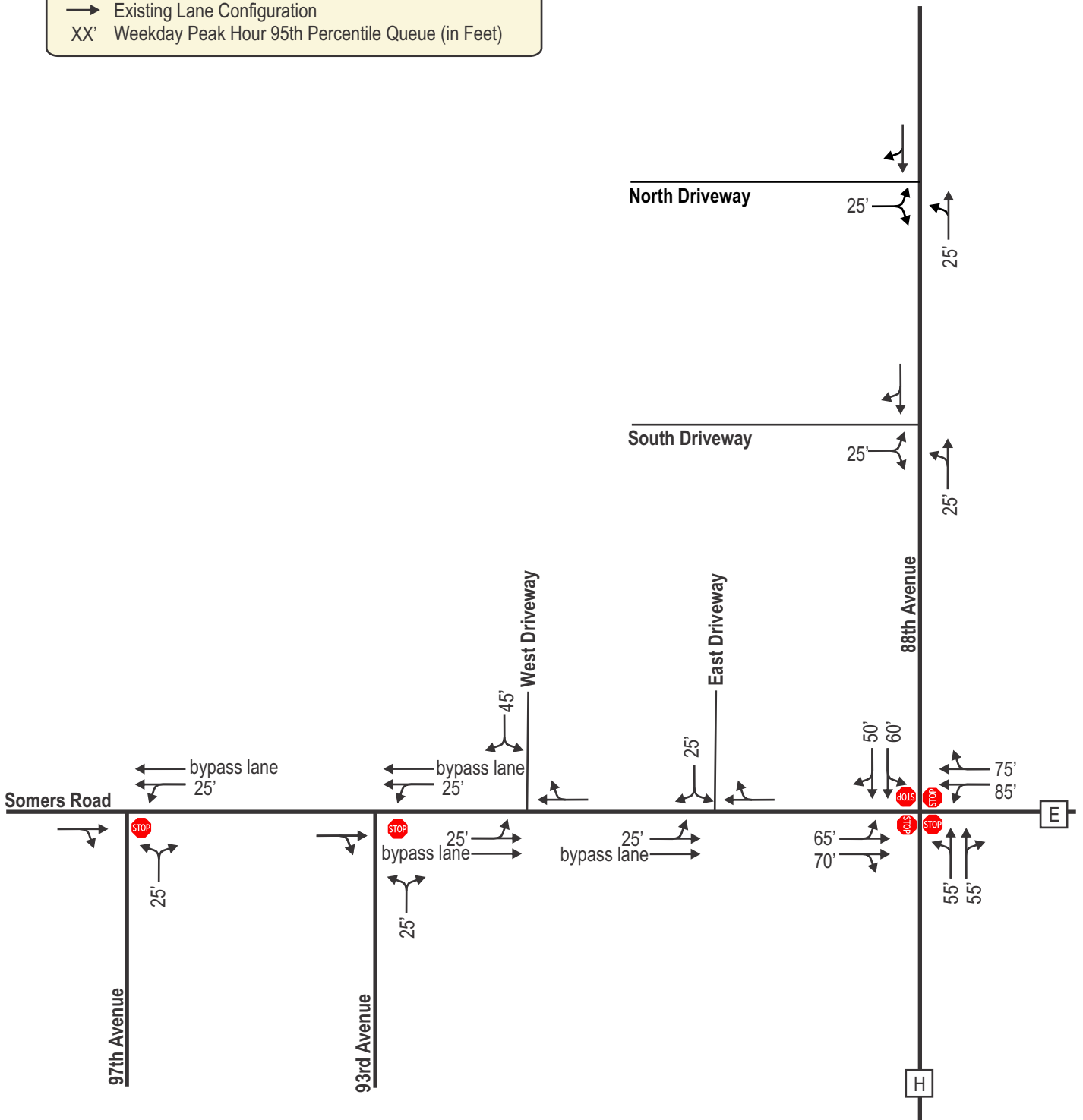
LEGEND

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)





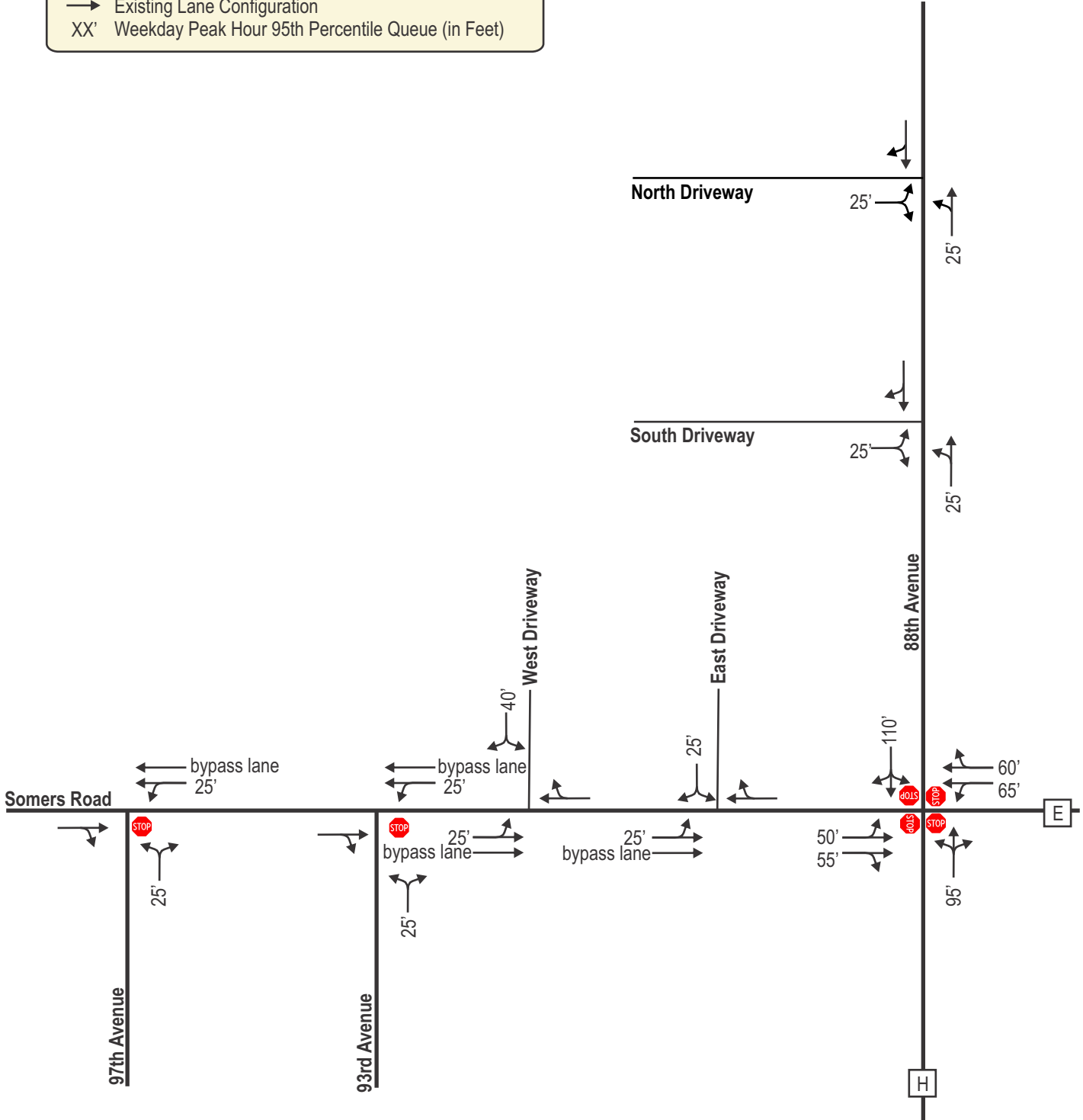
LEGEND

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)



LEGEND



-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)

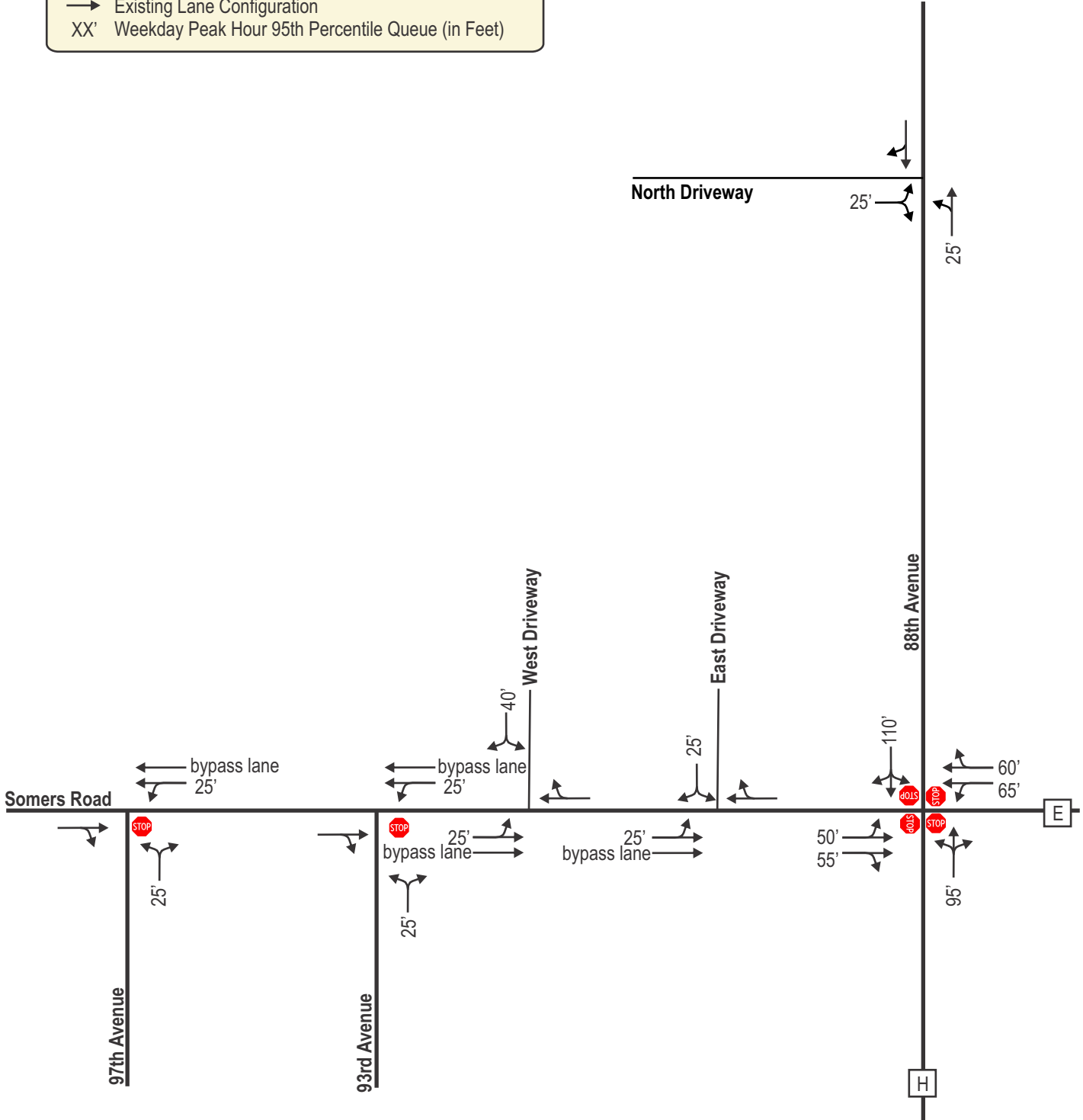


**EXHIBIT 5-21A
YEAR 2025 INITIAL BUILD TRAFFIC
MAXIMUM QUEUE LENGTHS
NORTH AND SOUTH DRIVEWAYS ON CTH H**

SOMERS, WISCONSIN

LEGEND



-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)

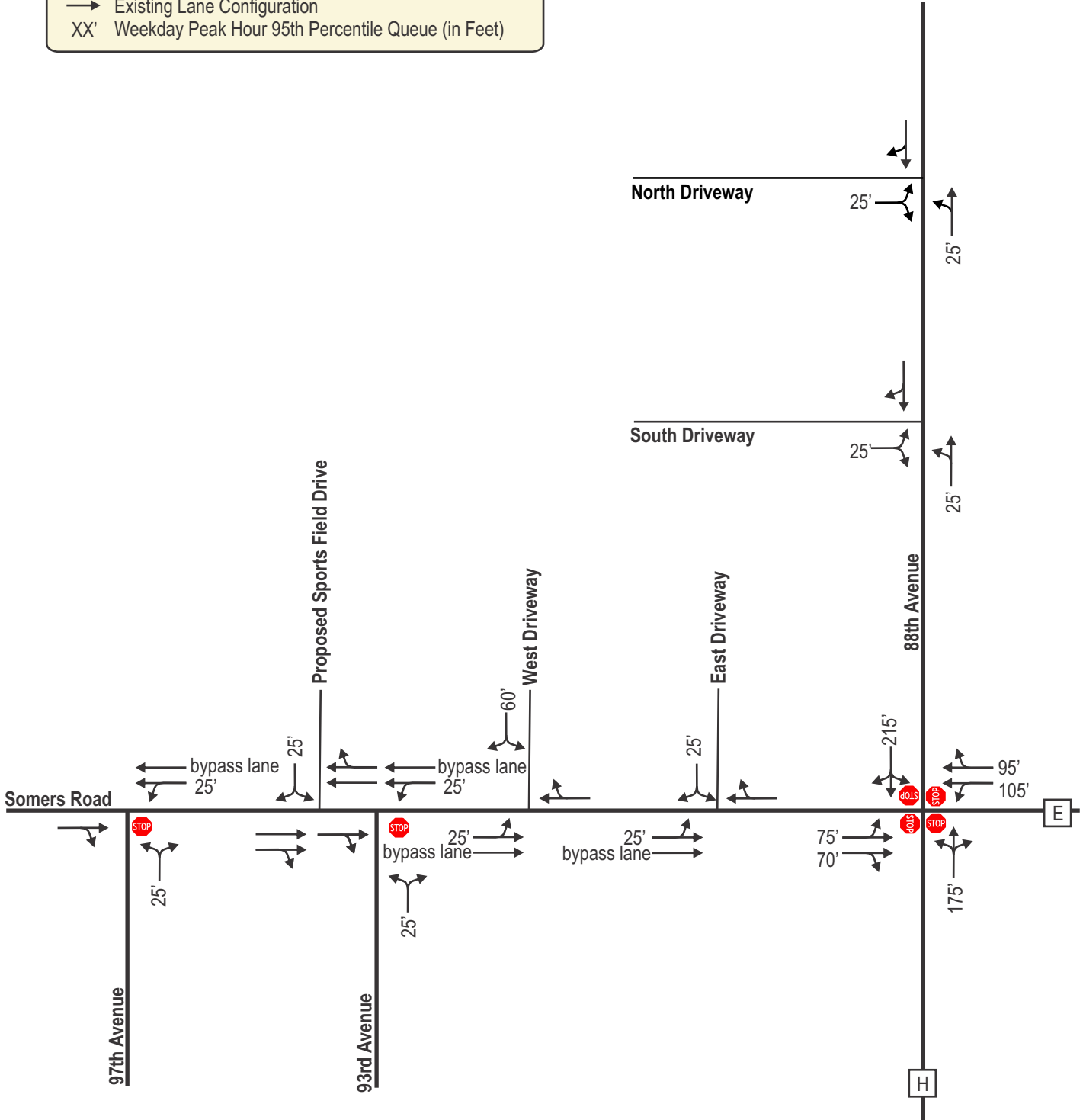


**EXHIBIT 5-21B
YEAR 2025 INITIAL BUILD TRAFFIC
MAXIMUM QUEUE LENGTHS
NORTH DRIVEWAY ONLY ON CTH H**

SOMERS, WISCONSIN

LEGEND



-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)

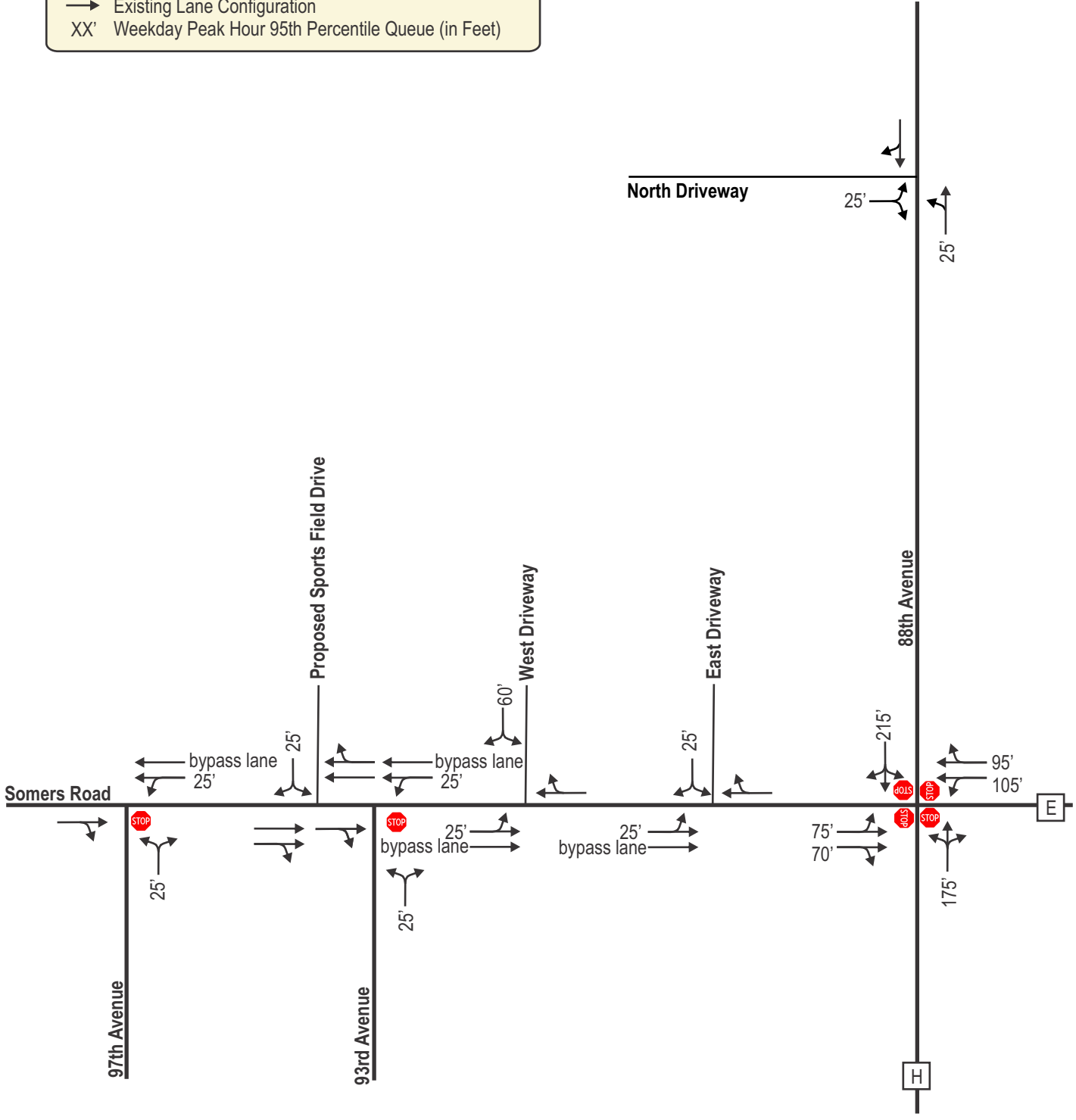


**EXHIBIT 5-23A
YEAR 2035 FULL BUILD TRAFFIC
MAXIMUM QUEUE LENGTHS
NORTH AND SOUTH DRIVEWAYS ON CTH H**

SOMERS, WISCONSIN

LEGEND



-  Stop Control
-  Existing Lane Configuration
- XX'** Weekday Peak Hour 95th Percentile Queue (in Feet)

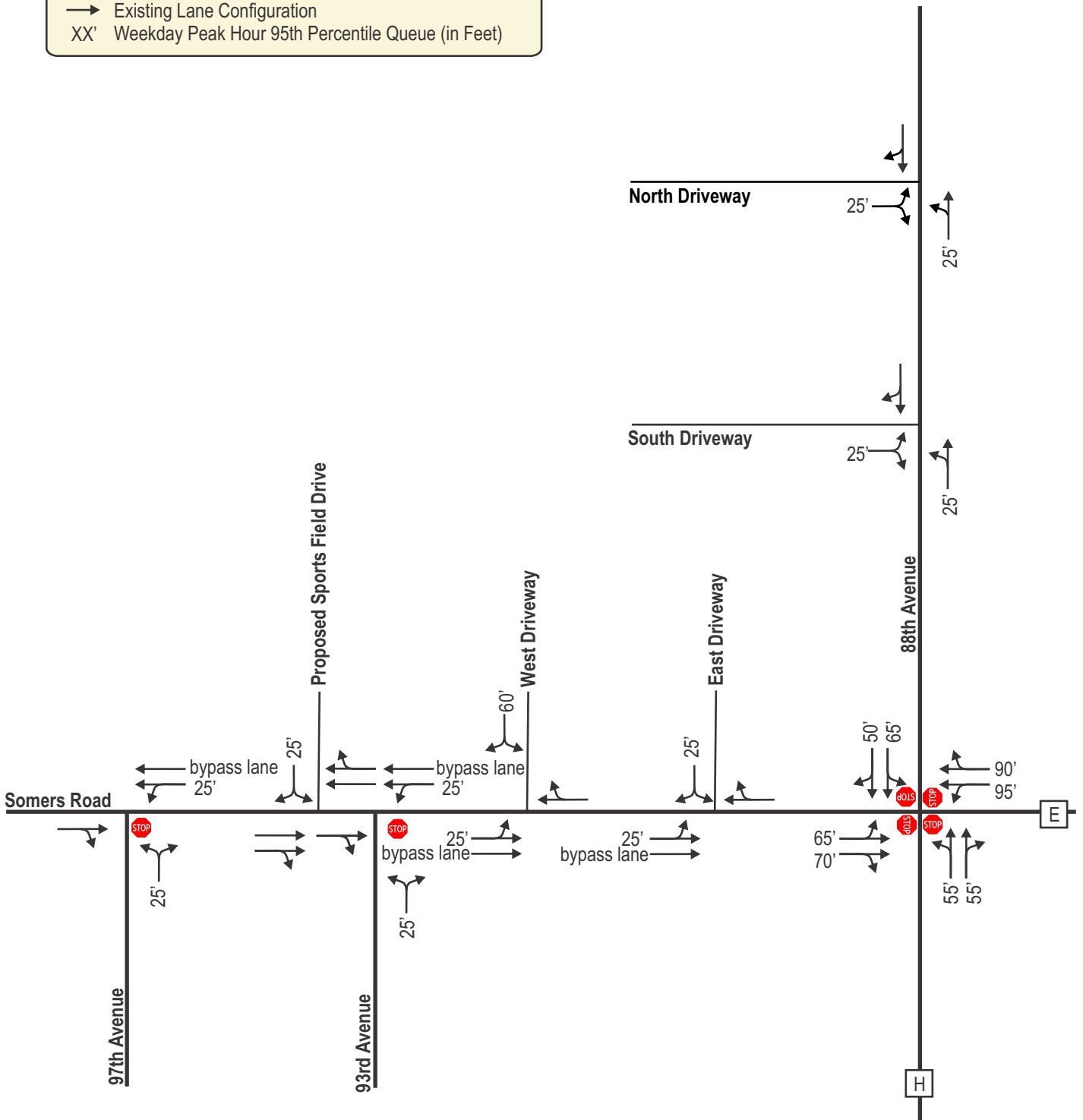


**EXHIBIT 5-23B
YEAR 2035 FULL BUILD TRAFFIC
MAXIMUM QUEUE LENGTHS
NORTH DRIVEWAY ONLY ON CTH H**



SOMERS, WISCONSIN

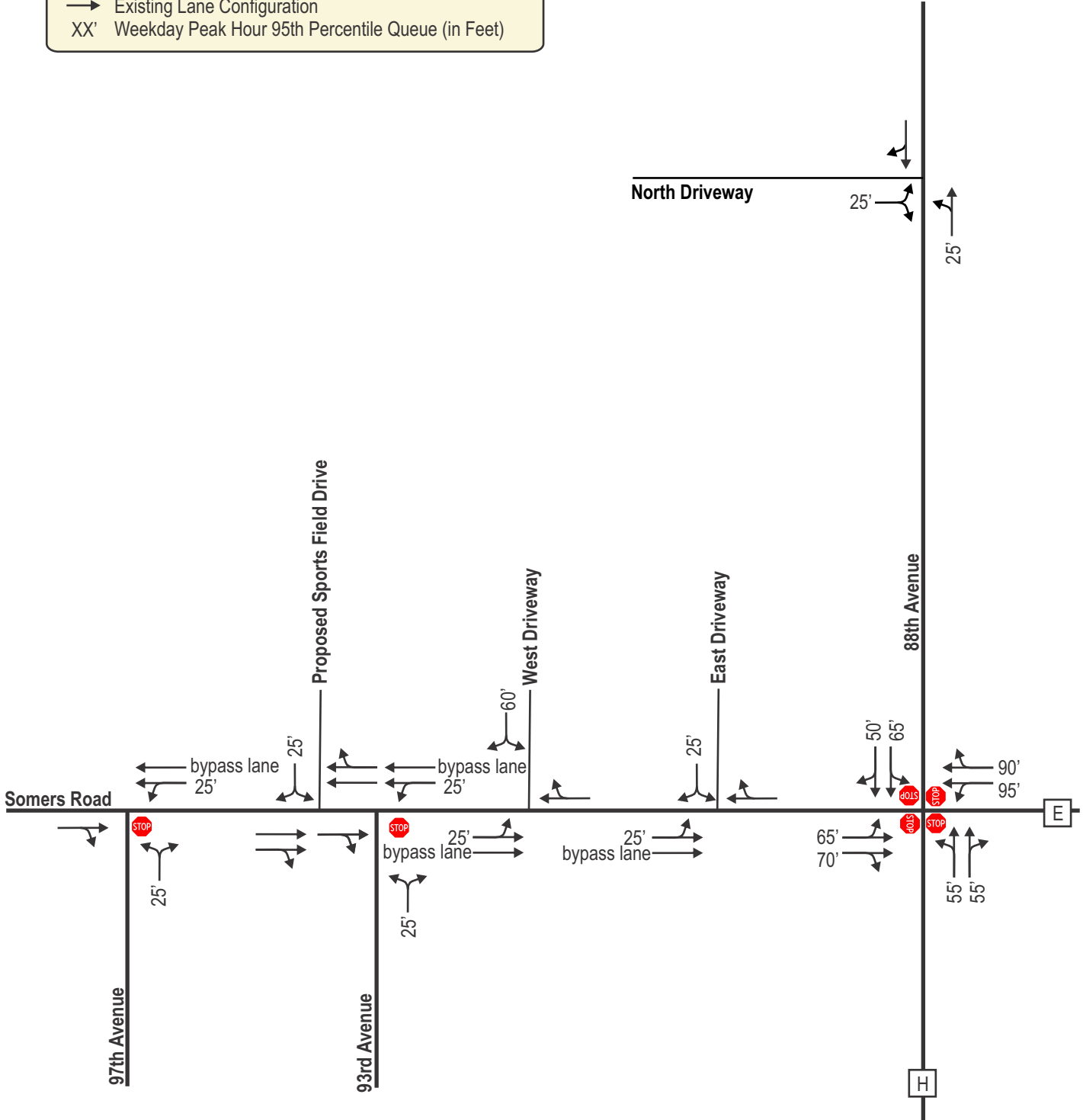
LEGEND

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)



LEGEND

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)



**EXHIBIT 5-23D
YEAR 2035 FULL BUILD TRAFFIC - SENSITIVITY ANALYSIS
MAXIMUM QUEUE LENGTHS
NORTH DRIVEWAY ONLY ON CTH H**

SOMERS, WISCONSIN

CHAPTER VI – RECOMMENDATIONS AND CONCLUSION

PART A – RECOMMENDATIONS

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual (HCM) 6th Edition*. Intersection operation is defined by “level of service.” Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A,’ to very poor, represented by LOS ‘F.’ For the purpose of this study, LOS D or better was used to define acceptable peak hour operating conditions.

Modifications to address traffic impacts are included for the Existing, Background, and Build traffic conditions and have been shown for the following five scenarios:

- “Existing Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions, without the proposed school.
- “Year 2025 Background Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions plus full build out of the identified off-site development, without the proposed school.
- “Year 2025 Initial Build Traffic” – These modifications are expected to be necessary to accommodate the Initial Build traffic volumes, which includes the initial build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development.
- “Year 2035 Background Traffic” – These modifications are expected to be necessary to accommodate the future background traffic volume conditions which include typical background growth along the highways plus full build out of the identified off-site development, without the proposed school.
- “Year 2035 Full Build Traffic” – These modifications are expected to be necessary to accommodate the Full Build traffic volumes, which includes the full build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development, including typical background growth along the highways.

The analysis was conducted using existing intersection geometrics and traffic control. The following modifications, as shown in [Exhibits 1-3 & 1-4](#), are recommended to accommodate the respective Background and Build traffic volume scenarios for the years 2025 and 2035, respectively. *Modifications are for jurisdictional consideration and are not legally binding. Kenosha County and the Village of Somers reserve the right to determine alternative solutions.*

Node 100: Somers Road/CTH E intersection with 97th Avenue

- *Existing Traffic*: No modifications.
- *Year 2025 Background Traffic*: No modifications.
- *Year 2025 Initial Build Traffic*: No modifications.
- *Year 2035 Background Traffic*: No modifications.
- *Year 2035 Full Build Traffic*: No modifications.

Node 200: Somers Road/CTH E intersection with :Proposed Sports Field Drive

- *Existing Traffic*: No modifications.

- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:*
 - Provide a new full access drive on the north approach as shown on the site plan.
 - Provide stop sign control on the north approach.
 - Provide an eastbound bypass lane on the south side of CTH E at the new intersection.
 - Connect the existing westbound bypass lanes on the north side of CTH E between 93rd Avenue and 97th Avenue.

Node 300: Somers Road/CTH E intersection with West School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 400: Somers Road/CTH E intersection with East School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 500: Somers Road/CTH E intersection with 88th Avenue/CTH H

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:*
 - Consider providing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2025 Initial Build Traffic:* No additional modifications.
- *Year 2035 Background Traffic:*
 - If not previously constructed, consider constructing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.

- Consider providing an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection (see sensitivity analysis discussion below). If implemented, the additional lane would taper down to a single lane again once vehicles traverse the intersection.

- *Year 2035 Full Build Traffic:* No additional modifications.

Node 600: 88th Avenue/CTH H intersection with North Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:*
 - Provide a new full access drive on the west approach as shown on the site plan.
 - Provide stop sign control on the west approach.
- *Year 2035 Background Traffic:* No additional modifications.
- *Year 2035 Full Build Traffic:* No additional modifications.

Node 700: Somers Road intersection with South School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

With a new north access driveway onto CTH H proposed, two access scenarios were evaluated as part of this study to analyze the operation of the two driveways along CTH H with and without the existing south school driveway operational. Regardless of the access scenario, both the north and south driveways are expected to operate acceptably at LOS B or better under both weekday peak periods; therefore, since providing the south driveway is not expected to cause an unsafe condition, allowing the south access driveway is recommended to allow for additional access options to/from the site. However, if it is decided to remove the driveway, the north driveway is expected to operate acceptably as the sole access onto CTH H.

All intersections are expected to operate acceptably with the proposed initial build out of the Shoreland Lutheran High School campus with the recommended modifications. Without the recommended modifications under the current year 2025 background traffic conditions, longer delays and queues are expected for the eastbound and westbound movements at the all-way stop-controlled Somers Road intersection with 88th Avenue during the weekday morning arrival and weekday afternoon discharge peak periods, with school in session. The higher delays and queues increase under the initial build conditions. However, the increases in delay under the background and initial build conditions are expected to be slightly higher (15 seconds for westbound vehicles and 7 seconds for eastbound vehicle) during the typical weekday morning (for westbound) and afternoon (for eastbound) discharge peak period under the initial build traffic conditions with the existing lane configurations on all approaches, that is, with no additional eastbound/westbound lanes along Somers Road. The increases in queues under the existing lane configurations are

expected to be 2 to 3 vehicles. It is noted that these delays and increases in queue lengths would only be expected for less than 30 minutes during the typical weekday morning and afternoon school “surge” peak periods with lower delays and queues all other hours of the day.

In addition, with the recommended modifications under future year 2035 background traffic conditions; that is, with additional lanes on the eastbound and westbound approaches but without additional lanes on the northbound and southbound approaches at the Somers Road intersection with 88th Avenue, slightly higher delays (2 seconds for northbound vehicles and 8 seconds for southbound vehicle) would be expected on the north and south approaches during the typical weekday afternoon discharge peak period under the full build traffic conditions with single lanes on the north and south approaches. Queue lengths on the north and south approaches would be expected to be 7 to 9 vehicles; however, these queue lengths would only be expected for less than 30 minutes. To alleviate the expected longer delays and queue lengths, additional northbound and southbound lanes would be required; therefore, a sensitivity analysis was completed to document this geometric condition, as described below.

Because modifications are recommended at the all-way stop-controlled Somers Road intersection with 88th Avenue under current and background conditions and because the modifications are further driven by projected future year volumes during peak “surge” traffic periods during the typical weekday morning school arrival and afternoon school discharge peak periods, a sensitivity analysis was completed for this project to document the operations if dual lanes were not only constructed along Somers Road (eastbound/westbound lanes) but also along 88th Avenue (northbound/southbound lanes) at the all-way stop controlled intersection. With the modifications, all movements are expected to operate safely with moderate queue lengths. Therefore, the following modifications could be considered as optional under the design year background and full build conditions.

Node 500: Somers Road/CTH E intersection with 88th Avenue/CTH H

- *Year 2035 Background Traffic:*
 - Provide an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2035 Full Build Traffic:* No additional modifications.

PART B – CONCLUSION

All movements at the study area intersections are expected to operate safely and efficiently with the development assumptions outlined in this TIA and with the identified recommended modifications if properly designed and implemented through the design year of the development.

Opinion of Probable Cost
 Shoreland Lutheran - Offsite Water Main Extension
 Village of Somers

ITEM	APPROX. QUANTITY	UNIT PRICE	AMOUNT
MOBILIZATION	1 LS	\$20,000	\$20,000
CLEARING AND GRUBBING	1 LS	\$10,000	\$10,000
WATER MAIN (OPEN CUT)			
8-inch diameter	5 LF	\$150	\$750
12-inch diameter	3385 LF	\$150	\$507,750
WATER MAIN (DIRECTIONALLY DRILLED)			
8-inch diameter	300 LF	\$250	\$75,000
12-inch diameter	600 LF	\$500	\$300,000
16-inch diameter	100 LF	\$800	\$80,000
FIRE HYDRANT ASSEMBLY	10 EACH	\$7,800	\$78,000
FIRE HYDRANT BARREL EXTENSION	6 VF	\$900	\$5,400
GATE VALVE AND BOX			
8-inch	4 EACH	\$3,800	\$15,200
12-inch	9 EACH	\$4,500	\$40,500
BUTTERFLY VALVE AND BOX			
16-inch	1 EACH	\$8,400	\$8,400
CONNECTION TO WATER MAIN (NON-PRESSURE)			
16-inch	1 EACH	\$6,000	\$6,000
REMOVE AND REPLACE UNSUITABLE MATERIAL	100 CY	\$60	\$6,000
GRANULAR BACKFILL	390 LF	\$80	\$31,200
DRIVEWAY REMOVAL AND REPLACEMENT	1 LS	\$9,600	\$9,600
RESTORATION OF LAWNS	1 LS	\$10,000	\$10,000
EROSION AND SEDIMENTATION CONTROL	1 LS	\$15,000	\$15,000
TRAFFIC CONTROL AND PROTECTION	1 LS	\$20,000	\$20,000
		CONSTRUCTION SUBTOTAL	\$1,238,800
		CONTINGENCIES (15%)	\$185,200
		CONSTRUCTION TOTAL	\$1,424,000
		ENGINEERING (DESIGN & CONSTRUCTION)	\$80,000
		PROJECT GRAND TOTAL	\$1,504,000



**VILLAGE OF SOMERS
VILLAGE BOARD
MEETING ITEM MEMORANDUM**

MEETING DATE: November 12th, 2024

TO: Village President Stoner and Village Trustees

PREPARED BY: Wendy Burnette-Clerk/Treasurer

AGENDA ITEM: #10 Action on Operator’s Licenses: Sandra Parise, Denise Maegaard, Dormilee Craig, Alexis Tennyson, and Megan Hornbaker

BACKGROUND:

Sandra Parise, Denise Maegaard, Dormilee Craig, Alexis Tennyson, and Megan Hornbaker have applied for operator’s licenses for the Village of Somers.

No discrepancies with these applications.

SUGGESTED ACTION/ACTION REQUESTED/COMMENTS:

Staff recommends approval. In the event that the Village Board agrees with the recommendation to approve: Operators applications, a suggested motion would be as follows:

“Motion to approve Operator’s Licenses for Sandra Parise, Denise Maegaard, Dormilee Craig, Alexis Tennyson, and Megan Hornbaker”

ATTACHMENTS:

NONE