

**Village of Somers  
7511 12th Street  
Somers, WI 53171**

**Village Work Session Meeting  
Agenda  
Wednesday, November 6<sup>th</sup>, 2024  
5:30 p.m.**

<b>Village Board Work Session Meeting:</b>	
<b>Item #</b>	
1	Call to Order
2	Administrator's Report
3	Engineer's Report
4	President & Trustee Reports
5	<p>Discuss Plan Commission Recommendation (Spaay &amp; Frederick)</p> <p>a. Request by John &amp; Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a rezoning from R-2 Suburban Single-Family Residential Dist. &amp; A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)</p> <p>b. Request by John &amp; Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)</p>
6	<p>Discuss Plan Commission Recommendation (Shoreland Lutheran High School):</p> <p>a. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N,</p>

	<p>R22E, Village of Somers. <i>(For information use only, the property is located at 9026 12th Street.)</i></p> <p>b. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. <i>(For information use only, the property is located at 9026 12th Street.)</i></p>
7	Continued discussion on 2025 Budget
8	Review tentative agenda for Village Board meeting on November 12 <sup>th</sup> , 2024
9	Adjourn

I hereby certify that as the designee of the chief elected official of the Village of Somers, I posted this notice of the November 6<sup>th</sup>, 2024 Village Work Session & Agenda in 1 public place & on the Village website.

Dated this 1<sup>st</sup> day of November, 2024.

Wendy Burnette, Clerk-Treasurer

Requests from person with disabilities who need assistance to participate in this meeting should be made to the Clerk's Office at 262-859-2822 with as much notice as possible. **Notice is hereby given that members of the Village Board may participate telephonically. Notice is hereby given that members of the Town Board may be in attendance for the sole purpose of gathering information. A quorum may be present. However, no Board action will be taken.**



**VILLAGE OF SOMERS  
VILLAGE BOARD  
WORK SESSION ITEM MEMORANDUM**

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**WORK SESSION:** November 6<sup>th</sup>, 2024

**TO:** Village President Stoner and Village Trustees

**FROM:** Jason J. Peters, Administrator

**AGENDA ITEM:** #2 Administrator's Report

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Below please find a bulleted summary of major issues that Administration has worked since our October 1<sup>st</sup> Work Session:

**REMINDER: DUE TO THE ELECTION ON NOVEMBER 5<sup>th</sup>, OUR WORK SESSION WILL BE HELD ON WEDNESDAY, NOVEMBER 6<sup>th</sup>**

Development

- As of the date of this memo, there are no matters to be heard for the November Plan Commission.
- The Plan Commission met on October 14<sup>th</sup> to review the following:
  - Request by Steven Frederick to rezone part of 5025 12<sup>th</sup> Street from A-2 to R-2 to allow for a single-family home and requested a CSM to split the lot. The Plan Commission recommendation was to approve (6-0). This matter will be reviewed by the Board at our November 6<sup>th</sup> Work Session.
  - Request by Bear Development for a Comp Plan Amendment and rezoning for their Willow Creek Project off CTH E. The public hearings were held for both matters. Bear explained that they are trying to work with another company to bring in fill to the site. If they are successful, then the site plan will be changing to add more single-family homes. The Plan Commission approved the Comp Plan Change (6-0) and tabled the rezone due to the potential change in the site plan.
  - Request by Shoreland Lutheran High School for a conditional use permit and site plan review for their proposed expansion to their buildings at 9026 12th Street. The public hearing was held. The majority of the comments by the public had to deal with how water would be brought to the site and how the cost of this project would be assessed. The Plan Commission's recommendation was to approve the

conditional use permit for the expansion (6-0). This matter will be reviewed by the Board at our November 6<sup>th</sup> Work Session. Administration, President Stoner, and Attorney Davison met with Shoreland Lutherans attorney on October 15<sup>th</sup> to discuss payment options for the water main.

- On October 10<sup>th</sup>, Planning Staff, Administration, and President Stoner held our regularly scheduled developer’s meeting. We met with representatives from Tropical Smoothie Café to review their updated plans and a resident interested in subdividing their lot.
- On October 15<sup>th</sup>, Administration and President held our monthly update call with Tawani regarding the Archive Center and Mission94.
- The installation of the sewer main for the Kenosha Travel Plaza will begin on November 4<sup>th</sup>.
- An early start permit has been issued for Golden Oil’s convenience store and hotel project on CTH E and the frontage road. They will begin grading the site soon. The overall development agreement is still being drafted.

Administration

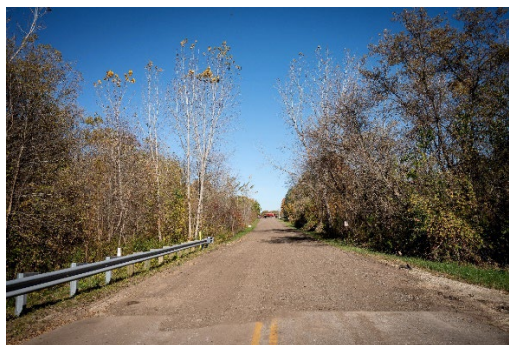
- Staff has continued to work on the final numbers for our budget. Administrator Peters presented the Budget at our October 22<sup>nd</sup> Board Meeting. The public hearing and final action on the Budget will take place for both the Town and the Village on November 26<sup>th</sup>.
- On October 3<sup>rd</sup>, Assistant to the Administrator Poirier met with our Zip Code Review Coalition to review the status of our legislation. This legislation will not be acted upon until 2025. Below is a map of the coalition we have brought together.



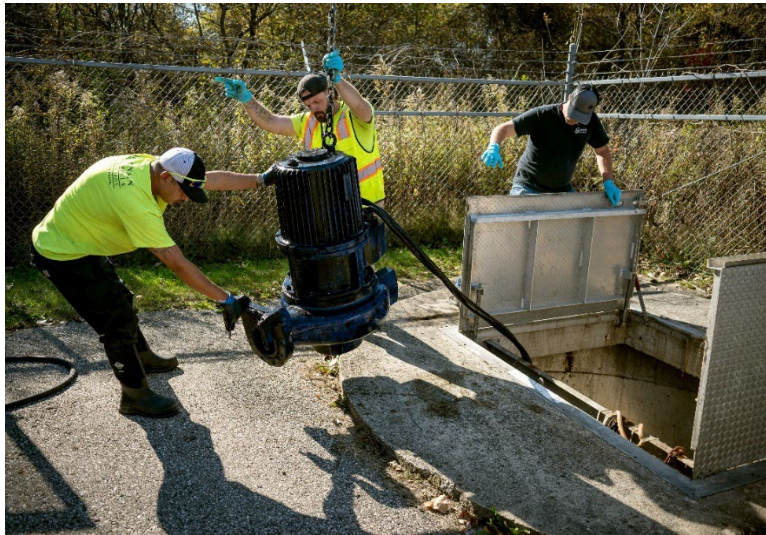
- On October 4<sup>th</sup>, Public Works and Administrator Peters met with the contractor to install our screening fence behind the salt barn. The work was completed on October 23<sup>rd</sup>.



- On October 15<sup>th</sup>, Administration and President Stoner met with the DOT and representatives from various law enforcement agencies to discuss reducing the speed limit on Sheridan Road and how to communicate this change to the public.
- On October 17<sup>th</sup>, Administrator Peters had a meeting with Robert Whitaker with McMahon to discuss the proposed budget 2025 budget for the Fire Department so that they can work these numbers into their personnel study.
- Over the past month, two of our investments have matured. Staff worked with Charles Schwab to reinvest using our ladder strategy.
- Work on our 2024 Paving projects have started. 13<sup>th</sup> Avenue and 16<sup>th</sup> Place are being resurfaced.



- Throughout the month of October, Public Works employee performed maintenance and cleaned the Village Lifts Stations. Pumps were pulled and wet wells were cleaned.



- On October 21<sup>st</sup>, Southport Heating and Cooling came to the Village Hall to prepare for the replacement of the rooftop units. They will need to have a crane on site for the work. The replacement will begin on November 13<sup>th</sup> and be completed by the 15<sup>th</sup>.



- On October 21<sup>st</sup>, the front office staff decorated pumpkins as a team building exercise. The completed works of arts are displayed in the front lobby and residents have been voting on which one is their favorite.



- On October 22<sup>nd</sup>, work began on our new concrete dumpster enclosure. Public Works employees excavated the site for the new dumpster enclosure behind the Village/Town Hall. The contractor began pouring the footing on October 30<sup>th</sup>.



- On October 22<sup>nd</sup>, Administration and President Stoner met with the Kenosha Water Utility. This a semi-annual meeting that is required by our intergovernmental agreement. This meeting is a chance for us to update each other on what is happening in our communicates and how they may affect our water utilities.
- In person voting began on October 22<sup>nd</sup>. There has been a steady stream of voters. The front office and election helpers have done a wonderful job keeping the lines moving!
- On October 23<sup>rd</sup>, Administration met with a software developer to discuss a budgeting tool that helps create a more professional looking “Budget Book”.
- On October 24<sup>th</sup>, Staff met with a software developer that works with our BS&A software to implement a time clock management system.
- On October 26<sup>th</sup>, the Somers Fire & Rescue department was one of the sites of the National DEA Drug Take Back Day. People were able to safely dispose of old prescription drugs. Narcan training was also available as well as Fentanyl and Xylazine test strips.



- On October 30<sup>th</sup>, Administration and Public Works met with Core and Main to discuss the installation of our radio read water meters.
- On October 30<sup>th</sup>, the front office and accounting staff attending an after hours meeting at Kenosha County to learn more about the new tax software that is being implemented by the County.
- Kenosha County IT has completed their work on moving our data to our new servers and installing additional software to help secure our emails and data.

# Development Pipeline

Last updated: 11/01/2024

Name of Developer	Type of Development (Residential, Commercial, Industrial, Mixed Use)	Location	Date Engaged	Pre-Deve. Agree.	Current Zoning	Requested Zoning	Current Comp Plan	Comp Plan Requested	Neighbor. Plan	CSM or Plat	Public Improv. Needed	Est. Improv. Costs	Acres	Current Value	Est. Value	Build Out Year	Year Full Assessed on Tax Roll	TIF Dist.	Requested Assistance	Proposed Assistance	Assistance Committed	Plan Comm. Date	Board Final Approval	Devel. Agree. Date	Comp Plan Ord	NOTES
Akil Ajmeri 3308 Green Bay Road	Multi-Tenant Commercial Building w/ Ayra Liquor Store as anchor	Somers Market north of Jiffy Lube	1/7/2021	No	A-1	B-3	Commercial	Commercial	Southeast	No	No		0.95	\$375,000		2021	2022	2	N/A	N/A	N/A	3/8/2021 Concept				03/21 Plan Commission Meeting heard concept 02/22 Met w/ Developer, moving forward pending easement with RCG 08/23 Received call regarding possible new development
Bear Development Willow Creek	Multi-Family Townhouse Single Family	Hwy E by lift station	8/23/2018	Yes	R-9 A-2 C-1	R-9 R-5 C-1 PUD	High Density Residential Secondary Environmental Other Conservancy Land Non-farmed Wetland Street & Highway ROW	Medium-Density Residential,	Somers Center East	Plat	Road Water Sewer		40.54	\$11,500	\$40,000,000			4	Yes			Concept 7/8/2024 Rezone & Comp 10/14/2024				10/8/2018 Conceptual Presented to Plan Commission 01/2019 Bear updating plan, will come back to Plan Commission 04/2019 Bear has new plan & working with administration to bring to Plan Commission 05/2019 Met with Bear & Root Pike Win. TID 4 amendments to be made so 35% rule is followed 6/2019 TID 4 amends will go to JRB, Plan Commission & Board in July 19 8/13 TID Amendments Finalized, Developer hoping to Bring Plans in Fall 4/20 Spoke to Developer, bringing in new concept plan 11/20 Met w/ Developer, concept with single family & owner occupied duplexes 03/24 Spoke with developer regarding bringing this project back 04/24 New concept submitted 07/24 Concept reviewed by PC 8/24 Met w/ Developer to discuss TID incentives 10/24 Site plan will be modified as they have obtained fill that will allow for basements
Becknell	Industrial/Warehouse	East Frontage	Spring 21	Yes	A-1	BP-1	Business/Industrial Secondary Environmental Other Conservancy	No	Kilborn	Yes	Road Water Sewer		132.1	\$294,600	\$60,000,000	Phase I 2022	Phase I 2023	6 3	Yes, offsite water main	\$460,000	\$460,000	9/13/201	09/28/2021 Comp, Zoning, Site 11/09/2021	10/8/2021	N/A	4/21 Introductory meeting 7/21 Submitted materials 8/21 Weekly meetings regarding possible development agreement 09/13 Plan Commission Approved 9/28 Board Approved 10/05 Board will review DA 11/21 DA Approved 12/21 DA Executed by Village being recorded 06/22 Watermain should be completed in June 9/22 Building is completely vertical 03/23 Occopancy Granted, staff met with developer for Phase II
Flint Development	Industrial 1.8 M sq ft 3 buildings	Somers Expansion Area (West of 94)	6/14/2021	Yes	A-1 Paris	BP-1	Commercial	Business/ Industrial	N/A IGA	Yes	Sewer Water		128.31	\$179,500	\$75,000,000	2022	2023	N/A	N/A	N/A	N/A	IGA 8/11/2021 Concept 11/17/2021 Approve 12/13/2021 Comp & Rezone	02/08/2022 approved subject to DA	7/26/2022	N/A	7/21 1st intro meeting. Submitted letter requesting review by IGA 8/11 IGA Committee heard concept., 9/28 Developer planning on October submission for Dec Plan Commission 10/29 Submitted plans for Plan Commission & IGA 11/21 IGA approved use 3/22 Developer still reviewing offsite utilities 4/22 Developer working on DA 6/22 DA almost complete 6/22 Grading permit granted, DA will be signed once final Exhibits Oked 7/22 Grading began , DA executed & recorded 9/22 Public Improvements went out to Bid 3/23 Offsite utilities substantially complete 5/23 Building III walls are vertical 02/24 Final Punchlist items for final Occupancy2024

Golden Oil	Convenience Store, Fast food, outlot for hotel	East Frontage	Late 2022	No	A-1	B-3	Business/Industrial	N/A	Kilborn Somers West	Yes	Sewer Water	\$40,000,000	62.02	\$17,300		2024	2025	8	Yes Incentives for Hotel	Concept 03.13.2023 CSM to Divide land & Rezone to A4 4/10/2023 07/08/2024	06/25/2024 CSM Approved 07/23/2024 Approved	N/A	4/23 Developer meet w/ staff for revised concept w/ less diesel pumps & status of hotel 6/23 Golden Oil closed on land, updated meeting on hotel 7/23 Meeting regarding utilities, meeting with potential hotel 02/24 Developer still moving forward 04/24 Filed for Plan Commission in June 8/24 Working with Developer on DOT CMS Requirements		
HSA Commercial	Industrial 1 building up to 900K sq ft	Darby Lane Farms CTH S/142	8/18/2021	Yes	A-1	BP-1	Business/Industrial Other Conservancy Secondary Enviro	N/A	Kilbourn South	No	Sewer Water		65.18	\$90,300	\$50,000,000	2022	2023	3	N/A	N/A	N/A	11/8/2021 Rezone 11/29/2021 Site Plan	7/18/2022 5/10/2023 1st amend	N/A	9/24 Submitted for November Plan Commission 12/21 Approved by Board, DA Completed 02/22 DA needs to be recorded 4/22 Developer confirmed that they will be 900K sq ft as opposed to 700k sq ft 6/22 Hope to break ground in fall 7/22 DA recorded 8/22 Grading began 11/22 Foundation Began 2/23 Bringing 1st Amend to DA to Board 4/23 Pre-Con for building to go vertical 5/23 1st Amend signed 06/23 Building has gone vertical 9/23 Meeting regarding Floodplain LOMR 10/23 Meeting for final items before occupancy 02/24 Temp Occupancy will be granted
NE2 Investments	Commerical Tropical Smoothie Café	Somers Market	Fall 2023	Yes	B-4	B-4 w PUD	Commercial	Commercial	Southeast	N/A	N/A	N/A	2.1	\$646,500		2024	2025	2	N/A	None	N/A	4/8/2024 PUD 08/12/2024 PUD	4/23/2024 PUD approved by 08/27/2024	N/A	02/24 Submitted for April PC Requesting CUP 04/24 Board Approved 07/24 PUD request will be heard in August
Ravine Park Partners Hawthorn Apt Phase II	Mixed Use Residential Multi-Family 140 Units 4Commercial Out Lots	Corner of E & 31	3/1/2022	Yes	A-2 C-1	R-11 B-3 C-1 PUD	Commercial Primary Environmental Corridor	High-Density Residential Commercial Primary Environmental Corridor	Pike Creek	CSM	Road Water Sewer		20.05	\$7,900	\$15,000,000	2024	2024					Concept 12/12/2022		Will Need	1/23 Hope to submit for approvals in spring 4/23 Developer waiting on financing options, then will submit
Shoreland Lutheran HS	School Expansion	9026 12th Street	8/24/2024	No	I-1 A-4	I-1	Medium Density Government Institutional	Government Institutional	Northwest	No	Possible Sewer and Water	\$1,000,000	54.45			2024	2024	No	Yes			10/14/2024	Done as apart of stormwater review plan	8/24 Met with Shoreland regarding expansion of school & moving athletic fields, submitted for October plan commission	
Somers Gateway LLC	Hotel	East Frontage	Fall 2023	No	A-4	B-3 PUD	Business/Industrial	N/A	Kilborn Somers West	Yes	Sewer Water		4.16	\$5,000,000		2025	2026	8	Yes Incentives for Hotel	Concept 03.13.2023 CSM to Divide land & Rezone to A4 4/10/2023 07/08/2024	06/25/2024 CSM Approved 07/23/2024 Approved	N/A	4/23 Developer meet w/ staff for revised concept w/ less diesel pumps & status of hotel 6/23 Golden Oil closed on land, updated meeting on hotel 7/23 Meeting regarding utilities, meeting with potential hotel 02/24 Developer still moving forward 04/24 Filed for Plan Commission in June 8/24 Working w/ Developer on DOT requirements for CSM 9/24 CSM Issue cleared up. 10/24 Early grading permit approved		
Somersville EA & E	Multi-Family Duplexes & Apts 521 total units "Somerville"	EA & A	4/1/2020	Yes	R-4, R-5, R-8, R-9, C-1	R-11	Medium Density Residential Conservancy	High Density Residential	Somers Center West	CSM	N/A	N/A	66.8	\$15,200	\$60,000,000	2022	2023	N/A	N/A	N/A	N/A	11/9/2020 6/12/2023	12/8/2020 CSM/Site Plan Conditionally Approved 06/27/2023 updated site plan conditionally approved	Will Need	06/20 held first official meeting w/ developer & his consultants 8/20 Concept Meeting Held 9/25 Plans Submitted for Nov 9th Plan Commission 11/20 Plan Commission, 1st work session 12/20 Conditionally Approved by Plan Commission & Board 03/21 Reaching final engineering for DA 05/21 meeting w/ engineering firm, expected updated exhibits soon 8/21 Exhibits nearing completion 10/21 Additional round of Engineer Comments given to Developer 02/22 Exhibits completed DA coming to Board in March 3/22 Developer reviewing DA 05/22 Meeting with Developer to discuss increase cost of project 9/22 No new activity 12/22 Email received regarding possible new CSM 02/23 Will be resubmitting w/ slightly lower unit count 4/23 possible June plan commission meeting 6/23 PC Meeting, Board, Sie conditionally approved

Stonestreet Partners, LLC	Mixed Use Residential Multi-Family 50 acres Commercial 18 acres Est. 300 units of multi-family	CTH L & STH 31	7/10/2020	Yes	A-1	R-11 B-3 C-1 PUD	Commercial Primary Environmental Corridor Other Conservancy	High-Density Residential Commercial Primary Environmental Corridor Other Conservancy	Somers Center East	CSM	Road Water Sewer	\$8,500,000	89.77	\$22,000	\$60,000,000	2022	2023	5	Yes, Road	\$5,000,000	1/11/2021	02/09/2021 Conditionally 02/23/2021 CSM 10/5/2021 DA Approved	11/18/2021	2021-032	7/20 Presented Concept to Plan Commission Hopes to submit in October 11/20 Submitted Plans, Staff met to discuss changes, resubmitting for Jan. 12/20 Schedule for Jan Plan Commission 01/21 Plan Commission Approved 02/21 Board Conditionally Approved, meetings with Developer for DA 03/21 meeting with developer for DA 4/21 meetings w/ developer for DA 05/21 meeting w/ developer on DA, updated exhibits submitted 07/21 reviewed updated submissions, another round of updated plans 8/21 Exhibits nearing complete & DA language being worked out b/w parties 10/05 Board will review DA 10/21 DA approved. Need updated CSM then DA will be executed 02/22 Expect to break ground in March 3/22 Watermain work started 4/22 Watermain connect completed 7/22 Working on private ROW easement issue 08/22 Grading started 9/22 STH 31 improvements underway & on site utilities 12/22 CTH L improvements done 02/23 Expect building permits to be pulled in spring 3/23 Building permits ready, waiting on developer	
Stream Realty	Industrial 3 Buildings up to 950K sq ft	Groh Farm CTH S/142	11/1/2021	Yes	A-1, A-2, R-2, R-3	BP-1 w/PUD	Business/Industrial Other Secondary Enviro	N/A	Kilbourn South	No	Sewer Water		90	\$775,000	\$50,000,000	2023	2024	3	N/A	N/A	N/A	7/11/2022 CSM 02/13/2023	10/11/2022 CSM 02/14/2023	4/14/2023	N/A	7/22 Developer will be working on access issue with County, will be brought to Board after issue worked out 10/22 Conditionally approved, need to finalize DA 12/22 Developer working w/ County on Memo of Understanding 02/23 CSM to combine parcels goes to PC & Board. DA can then be executed 3/23 DA Sent to Developer for execution 4/23 DA executed & recorded 6/23 temporary grading easements obtained by developer for roadsection 02/24 Hopes to pull permits in Spring/Summer 9/24 1st Amendmen to DA executed 16 month extension

**VILLAGE OF SOMERS**  
**Engineer's Report**  
**November 1, 2024**

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**Somers Trade Center (Groh's Development) - Project No. 220818**

- The developer is Stream Industrial Development Services. Pinnacle is the developer's engineer.
- A public roadway is proposed to extend along the east edge of the development. There is no public bidding planned with this development.
- We are waiting for revised plans, permits, and recorded CSM from the developer. Supposedly they have the temporary grading easements needed to position the roadway within the right-of-way shown on the proposed CSM.

**Golden Oil Sanitary Sewer Connection - Project No. 212153**

- The Developer is Golden Oil; the developer's design engineer is Excel Engineering. The Contractor is Globe Construction.
- Golden Oil has supposedly contracted Globe Construction to complete their onsite utility connections.
- Globe plans to start the public portion of the work on November 11, 2024 and then continue with the private work on Golden Oil property.

**Golden Oil Somers Gateway Development - Project No's. 2325743 and 24000875**

- The Design Engineer is Excel Engineering
- This development includes a convenience store and a Candlewood Suites Hotel
- Updated plans were received on October 29, 2024 and are currently being reviewed.
- The Village has requested that the WDNR review the floodplain calculations.

**Savannah at Pike Creek- Project No. 201664**

- This project is an approximately 65-acre townhouse development at the northwest corner of the intersection of CTH L (18th Street) and STH 31 (Green Bay Road). It also includes two outlots for future commercial development. The development will consist of both public and private infrastructure. The developer is the Stoneleigh Company. Developer's design and construction engineer is Kimley-Horn and their contractor is Horizon/Naperville Construction.
- All public utilities have been tested.
- Stoneleigh is preparing a list of completed items and costs to request a letter of credit reduction.

**Becknell Development Phase II - Project No. 211386**

- This project is a 1-million square feet industrial spec building on a planned unit development with access to the East Frontage Road. The developer's design and construction engineer is Pinnacle Engineering.
- There has been no response from Becknell on the status of the second building.

**VILLAGE OF SOMERS**  
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**Flint 94 Development— Project No. 211728**

- This is the planned 3-building development west of I-94 and south of 12<sup>th</sup> St; The Developer is Flint 94 Commerce Center, LLC; their engineer is Pinnacle Engineering.
- On January 29, 2024, we issued a list of items to be completed before recommending acceptance of utilities; painting the public fire hydrants red is the remaining item.

**45<sup>th</sup> Avenue and 63<sup>rd</sup> Avenue Sewage Pumping Station Replacements—Project No. 201061**

- The project includes the replacement of the suction lift sewage pumping equipment with submersible design. August Winter & Son, Inc is the Contractor.
- Both stations are operating; expect final payment in November.

**Water Tower Authority to Construct - Project No. 161104**

- We formally requested the Authority to Construct from the Public Service Commission of Wisconsin; the water tower is planned on 100<sup>th</sup> Avenue, north of 12<sup>th</sup> Street.
- The WPSC is requesting a joint meeting with other regulators in the next month.

**2024 Roadway Improvements - Project No. 2400374**

- The project includes 13<sup>th</sup> Avenue, 16<sup>th</sup> Place.
- Payne & Dolan, Inc is the contractor. The public informational meeting was July 10, 2024.
- The pre-construction meeting was held on August 28, 2024.
- The asphalt surface was completed the week of October 28, 2024. Shouldering will be done the week of November 4, 2024.

**4<sup>th</sup> Street, 13<sup>th</sup> Avenue Culverts - Project No. 2326136**

- This project will replace an existing culvert crossing under 4<sup>th</sup> Street at 3424 4<sup>th</sup> Street and extend the existing large dual culverts crossing under 13<sup>th</sup> Avenue to provide a flatter slope from the road shoulder.
- We plan to bid the 4<sup>th</sup> Street project this year and possibly 13<sup>th</sup> Avenue depending on WDNR permitting timing.

**GIS Implementation - Project No. 2401442**

- We are collecting GIS data from Kenosha County.
- The goal is to make this data usable to the Village.

**30<sup>th</sup> Avenue Sanitary Sewer - Project No. 2401761**

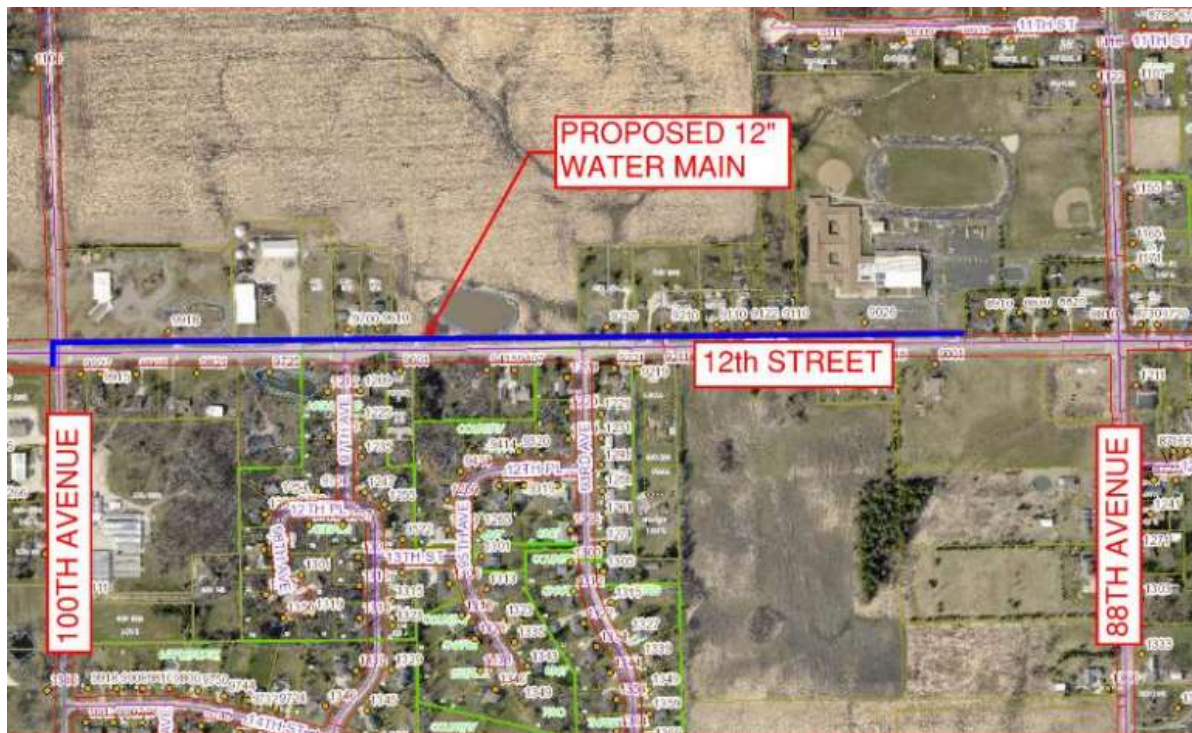
- The Developer is the Wisconsin Apartment Investment Group and the Engineer is Nielsen, Madsen & Barber.
- Orchard Court Apartments wants to disconnect from the Parkside system and connect to the Village sewer system.
- We received plans for the sewer extension; we will wait with the review until there is a reimbursement agreement.

**VILLAGE OF SOMERS**  
**Engineer's Report**  
**November 1, 2024**

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**Shoreland Lutheran High School – Project No. 221315**

- The design engineer is Excel Engineering.
- Our latest review was October 8, 2024; we recently received separate submittals for the school and parking lot expansion. We are waiting for a single submittal.
- A meeting was held with the County and Shoreland on August 20, 2024 to discuss acceptable access and stormwater discharge locations. No decisions were made, except the County is requiring a traffic impact analysis including the future football field to the west.
- Discussions are ongoing regarding a new culvert across CTH H and the need to extend water main (snip below) to the site.



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**VILLAGE OF SOMERS  
VILLAGE BOARD  
WORK SESSION ITEM MEMORANDUM**

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**WORK SESSION:** November 6<sup>th</sup>, 2024

**TO:** Village President Stoner and Board of Trustees

**PREPARED BY:** Jason J. Peters, Administrator

**AGENDA ITEM:** #5 Discuss Plan Commission Recommendation (Spaay & Frederick)

- a. Request by John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52<sup>nd</sup> Avenue.)*
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**BACKGROUND:**

In early May, Staff met with Steven Frederick regarding the possible split of the lot located at 5025 12th Street to build a new house. The lot split would also require a rezoning of a part of the lot from A-2 to R-2. In late August, applications for the rezone and to create two (2) 4.82 acre lots.

UPDATE:

The Plan Commission met on October 14<sup>th</sup> to review the Developer's application for a rezoning and land division. The requisite public hearing for the rezoning was held. There were no comments from the public. The Plan Commission's recommendation was to approve the request for rezoning (6-0) and the request for the Certified Survey Map (6-0).

COMMENTS:

This property has access to sewer and water. Administration is requesting that this matter be placed on our November 12<sup>th</sup> Board meeting for action.

ATTACHMENTS:

Planning and Zoning Memo to Plan Commission dated September 30<sup>th</sup>, 2024

Plan Commission Minutes – Highlighted

Rezoning Application

Land Division Application

DOA Letter



# KENOSHA COUNTY

Shelly Billingsley, Director  
Department of Public Works &  
Development Services

Andy M. Buehler, Director  
Division of Planning & Development

**TO: Village of Somers Plan Commission**  
**FROM: Luke Godshall, Senior Land Use Planner**  
**APPLN DATE: 08-28-24**  
**RPT DATE: 09-30-24**  
**MTG DATE: 10-14-24**  
**RE: Spaay Rezone & CSM**

---

## PROJECT/SITE INFO:

1. Petitioner/Agent: Steven Frederick
2. Property Owner: John & Ruthann Spaay
3. Location/Address: 5025 12<sup>th</sup> St.
4. Tax key Number(s): 82-4-222-142-0070
5. Area: 9.750 acres
6. Existing Zoning: R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist.
7. Proposed Zoning: R-2 Suburban Single-Family Residential Dist.
8. Existing Land Use: Suburban-Density Residential
9. Proposed Land Use: Suburban-Density Residential

---

## PROJECT OVERVIEW:

The Petitioner is proposing to split the subject parcel into two (2) 4.82-acre Lots via Certified Survey Map. The Petitioner has also submitted a corresponding rezoning application to assign R-2 zoning to the entirety of both Lots of the CSM, as the subject parcel is currently split zoned R-2 and A-2.

## PLANNER COMMENTS:

Each of the proposed Lots meet the minimum lot size and width requirements for the R-2 zoning district (minimum lot area of 40,000 square feet (0.92 acres) and a minimum lot width of at least 150 feet). Additionally, the existing single-family dwelling shown on Lot 2 of the CSM meets all minimum setback requirements for the R-2 zoning district.

Kenosha County Highways has commented on the proposed CSM and indicates that each of the proposed Lots of the CSM will be allowed its own driveway access, with the driveway on Lot 1 being located as far east as possible so as to be as far away from the intersection of CTH E & 52<sup>nd</sup> Ave. as possible.

Highways has also indicated that the south half of the CTH E right-of-way shall be a total of 60 feet. The submitted CSM indicates a total of 47 feet of right-of-way dedication for the south half

of CTH and therefore will need to be revised to indicate a total of 60 feet of right-of-way dedicated to the public for the south half of CTH E.

The submitted Certified Survey Map generally complies with the Village of Somers' Chapter 18 Land Division and Platting Control Ordinance. Noted below are several revisions that should be made to the final version of the CSM prior to its recordation in the Kenosha County Register of Deeds office.

**STAFF RECOMMENDATION:**

Should the Plan Commission choose to recommend approval of this Certified Survey Map, staff would recommend the following conditions:

1. Subject to revising the CSM to address any review comments received from the Wisconsin Department of Administration.
2. Subject to making the following revisions to the CSM:
  - a. Label a total of 60' as being dedicated to the public for the south half of CTH (submitted CSM indicates 47').
  - b. Revise the Lot area calculations as needed to account for a total of 60' of right-of-way dedication on the south half of CTH E.
  - c. Add the name and address of the current property owner.
  - d. Add a note indicating the Lots will be served by public sanitary sewer.
  - e. On Sheet 2 of 2, in the Village of Somers Approval area change 'Jason Peters, Village Administrator' to 'George Stoner, Village President'.
3. Subject to sending an electronic copy of the revised CSM showing the aforementioned changes to the Kenosha County Department of Planning and Development office to receive sign-off prior to the surveyor preparing the recordable CSM document for signatures. The county contact is Luke Godshall who can be contacted at the following e-mail address: [luke.godshall@kenoshacounty.org](mailto:luke.godshall@kenoshacounty.org).
4. No land division shall be recorded if any portion or part of the parent parcel has any unpaid taxes or outstanding special assessments without prior approval of the local and Kenosha County Treasurer.
5. Subject to the Certified Survey Map (CSM) being recorded with the Kenosha County Register of Deeds within twelve (12) months of receiving approval from the Somers Village Board.



Village of Somers  
Proceeding from the Village Plan  
Commission Meeting  
Oct. 14, 2024

## 1. Call to Order

Chairman Stoner called the meeting to order at 5:28 p.m.

Present:

- Chairman George **Stoner**
- Commissioner Troy **Steege**
- Commissioner Gregg **Thompson**
- Commissioner Vinnie **Chambers**
- Commissioner Don **Boxx**
- Commissioner Michael **DeLuca**

Absent:

- Commissioner Jerry **Romanowski**
- Alternate Pat **Juliana**

Staff present in person: Administrator Jason Peters, Assistant to the Administrator Kevin Poirier, Somers Fire Chief Ben Andersen, Deputy Chief Jodin Froeber

Village Trustees: Jackie Nelson

Kenosha County Planners: Luke Godshall

Village Engineer: Brett Biber

## 2. Pledge of Allegiance

Chairman **Stoner** led everyone in the Pledge of Allegiance.

## 3. Approve Minutes of the Aug. 12, 2024 Meeting

Commissioner **Boxx** moved to approve to the minutes.

Seconded by Commissioner **Steege**.

Motion carried. 6-0 vote.

## 4. Correspondence

None

## 5. Citizen Comments

Tom **Harper** – 8910 12<sup>th</sup> Street

Mr. **Harper** mentioned that his property is very close to the school and bring up parking concerns. He mentions that cars have parked on the road during events and it is a safety concern, He also mentions noise from indoor and outdoor events and hopes that sound proofing options will be considered.

- 6. Rezone: Public Hearing and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requesting approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52<sup>nd</sup> Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

- 7. Land Division (Certified Survey Map): Discussion and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requests approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52<sup>nd</sup> Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Thompson** seconds the motion.

Motion carried. 6-0.

Passes unanimously

- 8. Comprehensive Plan Amendment: Public Hearing and Action on Request by:** Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St., Kenosha, WI 53142 (Agent); requests an amendment to the land use plan map for the Village of Somers (Map 80) as adopted in the Multi-

Jurisdictional Comprehensive Plan for Kenosha County: 2035 from High-Density Residential to Medium-Density Residential, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Administrator **Peters** mentions that there have been changes to the project since the concept was presented to the board at their July 8<sup>th</sup> meeting.

Bear Development Project Manager Dan **Szczap** mentions that the project presented was a residential mix of 33 single-family homes and 64 two-family buildings that received positive feedback when it was presented in July. They therefore started the process to re-zone and amend the comprehensive land use plan accordingly. They are proposing zero lot-line parcels to accommodate the two-family buildings. It would look like a condominium but be on a fee-simple lot. They have done this before in Mount Pleasant.

**Szczap** suggests three ways to move forward:

1. Move forward with the concept plan and using a PUD to handle deviations
2. Changing the two-family dwellings to condos
3. Changing the zoning code to accommodate zero-lot-line development

Administrator **Peters** mentions that Bear is also looking at the possibility of filling the site and changing it to single-family homes.

**Szczap** mentions that the soil condition and high ground water on the property has been challenging and that the fill would allow for the structures to be built with basements. They are in discussion to secure fill for the site but that has not been finalized.

Commissioner **Boxx** gets confirmation from **Szczap** that if sufficient fill is secured, the duplexes would be changed to single-family homes on the entire site. If the fill is not secured, the developer would plan to move forward with zero-lot-line dwellings to accommodate the challenges of the site and be a financially viable project. The developer states that condominiums is a complicated form of ownership and is not their preference.

Commissioner **Boxx** mentions that the developer is asking for a reduction of the setbacks from what the zoning allows.

Commissioner **Steege** asks whether, in the case the site is converted to single-family homes, the setbacks would be compliant with current ordinances.

**Szczap** states that he is not sure as the site plan is likely to change and they may ask for

minor relief.

Commissioner **Steege** asks for clarification about the changes from the plan resented in July.

**Szczap** states that the building and roads through the site did not change, only the lot lines through the duplexes have changed to create zero-lot-line fee-simple lots.

Commissioner **Boxx** asks details about the maintenance of the property.

**Szczap** explains that declarations, homeowner association and/or covenants would govern maintenance, common areas, architecture and insurance.

Commissioner **DeLuca** asks again whether the developer would still ask for zero-lot-lots if they secured the fill.

**Szczap** states that if the fill is secured, they would eliminate the two-family units in favor of single-family lots.

Chairman **Stoner** is concerned about zero-lot-line and issues with neighbors maintaining their properties to different standards.

**Szczap** states that the management documents would be similar to a condominium.

Chairman **Stoner** mentions concerns about the fill process and that the size of the lots for single family homes would have to change.

Chairman **Thompson** mentions being in favor of the medium density single-family project but is concern about the timing of the fill as it relates to weather.

Commissioner **Boxx** asks whether the management documents from a similar site could be provided to the commission for review.

**Szczap** states that that document is owned by a third party and would need approval before sharing it.

Commissioner **Steege** makes a motion to approve the request.

Commissioner **Chambers** seconds the motion.

Motion carried. 6-0.

Passes unanimously

## 9. **Rezoning w/ PUD:**

**Public Hearing and Action on Request by:** Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St.,

Kenosha, WI 53142 (Agent); requesting approval of a rezoning from R-9 Multiple-Family Residential Dist., C-1 Lowland Resource Conservancy Dist., and A-2 General Agricultural Dist. to R-9 Multiple-Family Residential Dist., R-5 Urban Single-Family Residential Dist., and C-1 Lowland Resource Conservancy Dist., and PUD Planned Unit Development Overlay, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **DeLuca** makes a motion to table the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

#### **10. Conditional Use Permit:**

**Public Hearing and Action on Request by:** Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requesting a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments:

Tom **Terletzky** – 9015 12<sup>th</sup> Street

Mr. **Terletzky** mentions his support for the school's expansion. He raises concerns about the extension of the water to the property from 100<sup>th</sup> Avenue. That resulted in the homeowners paying a large amount of the cost to the benefit of a proposed subdivision that would use a lot more of that water. **Terletzky** objects to the high cost to homeowners along 12<sup>th</sup> Street. He mentions that the Village has the authority to decide how the cost of the pipe would be charged to the benefiting properties.

**Terletzky** proposes a 20/30/50 cost split as follows: 20 percent would be paid by the school, 50 percent would be paid by the proposed subdivision and the remaining would be paid by the residents along Highway E according to frontage. The subdivision would pay back the assessment when the lots are sold. He mentions that this solution would speed up repayment.

Chairman **Stoner** mentions that they have no control over developers and mentions the additional infrastructure cost incurred to develop a subdivision. He states that other communities also assess property owner for utilities going by their properties. He also disputes the benefits

**Terletzky** mentions that homeowners have been subsidizing growth in their community

by paying for the infrastructure and that it is not fair. He mentions that residents are not notified of upcoming assessments until it is too late.

Chairman **Stoner** asks what the public notification process is.

Administrator **Peters** explains the process and what would trigger notification.

**Terletzky** mentions that by the time homeowners are notified it is too late for residents to object. He mentions that by that time the project has been approved and bids are under way. He reiterates that having residents pay for 70 percent of the cost is wrong.

Commissioner **Boxx** makes a motion to approve the request.  
Commissioner **Steege** seconds the motion.

Chairman **Stoner** is concerned that the stormwater part of the project has not started. He also mentions that he will not support the school expansion without connection to municipal water in part due to fire protection. He mentions his support for the overall projects but also mentions that future athletic fields would need to be serviced by municipal water for bathrooms and any concession stands.

Commissioner **Steege** mentions his concerns with the stormwater plan, the traffic analysis and appropriate parking.

Excel Engineer Jeremy **Wagner** mentions that they are coordinating with Kenosha County about the stormwater project. He mentions that they just received the traffic analysis and shows only one additional car stacking at the four-way intersection. As to the parking, he states that the project will add 250 stalls.

Commissioner **Steege** asks whether the parking calculation accounts for gymnasium activities.

**Wagner** states that it does.

Chairman **Stoner** asks whether they are designing the water system for the project.

**Wagner** states that they are doing the plumbing but not the water main coming to the property. He states that the Village Engineer will design that.

Engineer Brett **Biwer** mentions that the design is very preliminary so far.

9100 11<sup>th</sup> Street

Fire Chief **Andersen** whether the State has reviewed the fire suppression system.

Chief **Andersen** states that it would not happen until the project is approved. He states that while the State would not force the property owner to connect to the water but the local ordinance would within six months. He mentions that it would be more cost effective to

plan for municipal water connection at the time of the expansion.

Shoreland Lutheran High School President Paul **Scriver** mentions that they are waiting for a final decision about municipal water connection. He states that preliminary engineering states they do not need to connect. Once the school hears that the water connection is necessary, they will design the project accordingly. He states that the school is not opposed to connection to municipal water.

Commissioner **Thompson** asks about the process to design and bidding for the water line going down 12<sup>th</sup> Street.

Administrator **Peters** explained that the Village engineer would start the design once the project is approved.

Commissioner **Thompson** asks if the design process would start before a decision was made about how the project would be paid for. He also asks whether the subdivision project is moving forward.

Administrator **Peters** answers that there has been no contact with the subdivision developer for the past two years.

Commissioner **Steege** mentions that the commission is not responsible for deciding how the project is financed, only approving the plans. The question of the water connection will be up to the Village Board.

Guy Hawley - 9100 11<sup>th</sup> Street

Mr. **Hawley** mentions that the construction of his house is delayed until the school's stormwater system is in place.

Chairman **Stoner** interjects that this issue has nothing to do with the school's project. He is aware of the situation and offers to speak with Hawley after the meeting.

Commissioner **Boxx** makes a motion to approve the conditional use permit.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

## **11. Site Plan Review:**

**Discussion and Action Request by:** Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requests site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.  
Commissioner **Steege** seconds the motion.

Commissioner **Steege** asks details about the rooftop AC units.

Excel Engineer Alex **Fiebig** answers the questions and states that the number of unit has been reduced and features a two feet parapet.

Motion carried. 6-0.

Passes unanimously

## **12. Adjourn**

Commissioner **Boxx** makes a motion to adjourn.  
Commissioner **Steege** seconds the motion.  
Motion carried 6-0 vote.

Passes unanimously

Drafted Oct. 16, 2024.

These minutes are not official until approved by the Plan Commission. Submitted by Assistant to the Administrator/Communication Specialist Kevin Poirier



# VILLAGE OF SOMERS

## Department of Planning and Development

Sept. 2021

RECEIVED

AUG 28 2024

### REZONING APPLICATION

Kenosha County  
Planning and Development

(a) Property Owner's Name:

Print Name: John Spaay Signature: 

Mailing Address: 6828 GRAND PKWY

City: WAUWATOSA State: WI Zip: 53213

Phone Number: 414-241-6125 E-mail (optional): spaayruthann@gmail.com

Note: Unless the property owner's signature can be obtained in the above space, a letter of agent status **signed** by the legal property owner **must** be submitted if you are a tenant, leaseholder, or authorized agent representing the legal owner, allowing you to act on their behalf.

(b) Agent's Name (if applicable):

Print Name: Steven Frederick Signature: \_\_\_\_\_

Business Name: Tenant

Mailing Address: 5025 12th St

City: Kenosha State: WI Zip: 53144

Phone Number: 262-515-3637 E-mail (optional): steve3637@live.com

(c) Tax key number(s) of property to be rezoned:

82-4-222-142-0070

Property Address of property to be rezoned:

5025 12th Street, Kenosha, WI 53144

(d) Proposed use (a statement of the type, extent, area, etc. of any development project):

Looking to split the 10 acre parcel down the middle from north to South, creating two  
~5 acre parcels. The structures would all be within the newly created ~5 acre East parcel  
and the West ~5 acre parcel would be a wooded lot.

**REZONING APPLICATION**

(e) Check the box next to any and all of the **existing** zoning district classifications present on the subject property:

<input type="checkbox"/> A-1 Agricultural Preservation District	<input type="checkbox"/> B-1 Neighborhood Business District
<input checked="" type="checkbox"/> A-2 General Agricultural District	<input type="checkbox"/> B-2 Community Business District
<input type="checkbox"/> A-3 Agricultural Related Manufacturing, Warehousing and Marketing District	<input type="checkbox"/> B-3 Highway Business District
<input type="checkbox"/> A-4 Agricultural Land Holding District	<input type="checkbox"/> B-4 Planned Business District
<input type="checkbox"/> AE-1 Agricultural Equestrian Cluster Single-Family District	<input type="checkbox"/> B-5 Wholesale Trade and Warehousing District
<input type="checkbox"/> R-1 Rural Residential District	<input type="checkbox"/> BP-1 Business Park District
<input checked="" type="checkbox"/> R-2 Suburban Single-Family Residential District	<input type="checkbox"/> B-94 Interstate Highway 94 Special Use Business District
<input type="checkbox"/> R-3 Urban Single-Family Residential District	<input type="checkbox"/> M-1 Limited Manufacturing District
<input type="checkbox"/> R-4 Urban Single-Family Residential District	<input type="checkbox"/> M-2 Heavy Manufacturing District
<input type="checkbox"/> R-4.5 Urban Single-Family Residential District	<input type="checkbox"/> M-3 Mineral Extraction District
<input type="checkbox"/> R-5 Urban Single-Family Residential District	<input type="checkbox"/> M-4 Sanitary Landfill and Hazardous Waste Disposal District
<input type="checkbox"/> R-6 Urban Single-Family Residential District	<input type="checkbox"/> I-1 Institutional District
<input type="checkbox"/> R-7 Suburban Two-Family and Three-Family Residential District	<input type="checkbox"/> PR-1 Park-Recreational District
<input type="checkbox"/> R-8 Urban Two-Family Residential District	<input type="checkbox"/> C-1 Lowland Resource Conservancy District
<input type="checkbox"/> R-9 Multiple-Family Residential District	<input type="checkbox"/> C-2 Upland Resource Conservancy District
<input type="checkbox"/> R-10 Multiple-Family Residential District	<input type="checkbox"/> FPO Floodplain Overlay District
<input type="checkbox"/> R-11 Multiple-Family Residential District	<input type="checkbox"/> PUD Planned Unit Development Overlay District
<input type="checkbox"/> R-12 Mobile Home/Manufactured Home Park-Subdivision District	<input type="checkbox"/> AO Airport Overlay District
	<input type="checkbox"/> RC Rural Cluster Development Overlay District

(f) Check the box next to any and all of the **proposed** zoning district classifications proposed for the subject property:

<input type="checkbox"/> A-1 Agricultural Preservation District	<input type="checkbox"/> B-1 Neighborhood Business District
<input type="checkbox"/> A-2 General Agricultural District	<input type="checkbox"/> B-2 Community Business District
<input type="checkbox"/> A-3 Agricultural Related Manufacturing, Warehousing and Marketing District	<input type="checkbox"/> B-3 Highway Business District
<input type="checkbox"/> A-4 Agricultural Land Holding District	<input type="checkbox"/> B-4 Planned Business District
<input type="checkbox"/> AE-1 Agricultural Equestrian Cluster Single-Family District	<input type="checkbox"/> B-5 Wholesale Trade and Warehousing District
<input type="checkbox"/> R-1 Rural Residential District	<input type="checkbox"/> BP-1 Business Park District
<input checked="" type="checkbox"/> R-2 Suburban Single-Family Residential District	<input type="checkbox"/> B-94 Interstate Highway 94 Special Use Business District
<input type="checkbox"/> R-3 Urban Single-Family Residential District	<input type="checkbox"/> M-1 Limited Manufacturing District
<input type="checkbox"/> R-4 Urban Single-Family Residential District	<input type="checkbox"/> M-2 Heavy Manufacturing District
<input type="checkbox"/> R-4.5 Urban Single-Family Residential District	<input type="checkbox"/> M-3 Mineral Extraction District
<input type="checkbox"/> R-5 Urban Single-Family Residential District	<input type="checkbox"/> M-4 Sanitary Landfill and Hazardous Waste Disposal District
<input type="checkbox"/> R-6 Urban Single-Family Residential District	<input type="checkbox"/> I-1 Institutional District
<input type="checkbox"/> R-7 Suburban Two-Family and Three-Family Residential District	<input type="checkbox"/> PR-1 Park-Recreational District
<input type="checkbox"/> R-8 Urban Two-Family Residential District	<input type="checkbox"/> C-1 Lowland Resource Conservancy District
<input type="checkbox"/> R-9 Multiple-Family Residential District	<input type="checkbox"/> C-2 Upland Resource Conservancy District
<input type="checkbox"/> R-10 Multiple-Family Residential District	<input type="checkbox"/> FPO Floodplain Overlay District
<input type="checkbox"/> R-11 Multiple-Family Residential District	<input type="checkbox"/> PUD Planned Unit Development Overlay District
<input type="checkbox"/> R-12 Mobile Home/Manufactured Home Park-Subdivision District	<input type="checkbox"/> AO Airport Overlay District
	RC Rural Cluster Development Overlay District

**REZONING APPLICATION**

**(g)** Your request must be consistent with the existing planned land use category as shown on Map 65 of the adopted "Multi-Jurisdictional Comprehensive Plan for Kenosha County: 2035".

The existing planned land use category for the subject property is:

<input type="checkbox"/> Farmland Protection	<input type="checkbox"/> Governmental and Institutional
<input type="checkbox"/> General Agricultural and Open Land	<input type="checkbox"/> Park and Recreational
<input type="checkbox"/> Rural-Density Residential	<input type="checkbox"/> Street and Highway Right-of-Way
<input type="checkbox"/> Agricultural and Rural Density Residential	<input type="checkbox"/> Other Transportation, Communication, and Utility
<input checked="" type="checkbox"/> Suburban-Density Residential	<input type="checkbox"/> Extractive
<input type="checkbox"/> Medium-Density Residential	<input type="checkbox"/> Landfill
<input type="checkbox"/> High-Density Residential	<input type="checkbox"/> Primary Environmental Corridor
<input type="checkbox"/> Mixed Use	<input type="checkbox"/> Secondary Environmental Corridor
<input type="checkbox"/> Commercial	<input type="checkbox"/> Isolated Natural Resource Area
<input type="checkbox"/> Office/Professional Services	<input type="checkbox"/> Other Conservancy Land to be Preserved
<input type="checkbox"/> Industrial	<input type="checkbox"/> Nonfarmed Wetland
<input type="checkbox"/> Business/Industrial Park	<input type="checkbox"/> Surface Water

**(h)** Attach a plot plan or survey plat of property to be rezoned (showing location, dimensions, zoning of adjacent properties, existing uses and buildings of adjacent properties, floodways and floodplains)—drawn to scale.

**(i)** The Village of Somers Department of Planning and Development may ask for additional information.

**(j)** The name of the County Supervisor of the district wherein the property is located (District Map):

Supervisory District Number: 15 County Board Supervisor: Dave Geertsen

**(k)** The fee specified in Section 12.05-8 of this ordinance.

Request for Rezoning Petition (payable to "Kenosha County").....\$1,450.00

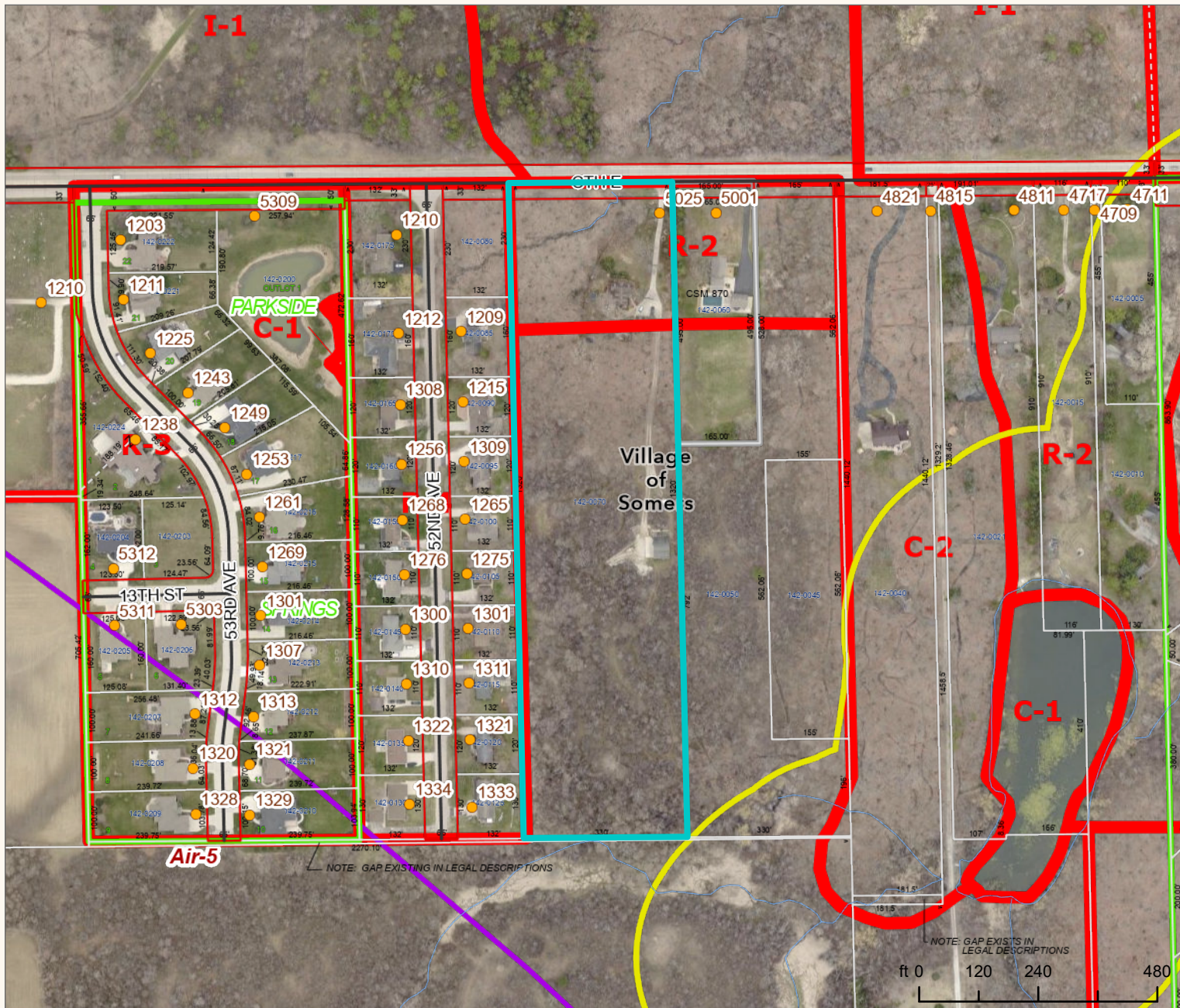
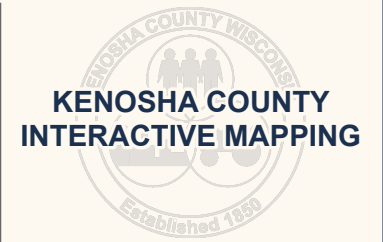
(For other fees see the Fee Schedule)

**Note: Agricultural Use Conversion Charge**

The use value assessment system values agricultural land based on the income that would be generated from its rental for agricultural use rather than its fair market value. When a person converts agricultural land to a non-agricultural use (e.g. residential or commercial development), that person may owe a conversion charge. To obtain more information about the use value law or conversion charge, contact the Wisconsin Department of Revenue's Equalization Section at 608-266-2149 or visit <http://www.revenue.wi.gov/faqs/slf/useassmt.html>.

Note that the act of rezoning property from an agricultural zoning district to a non-agricultural zoning district does not necessarily trigger the agricultural use conversion charge. It is when the use of the property changes from agricultural that the conversion charge is assessed.

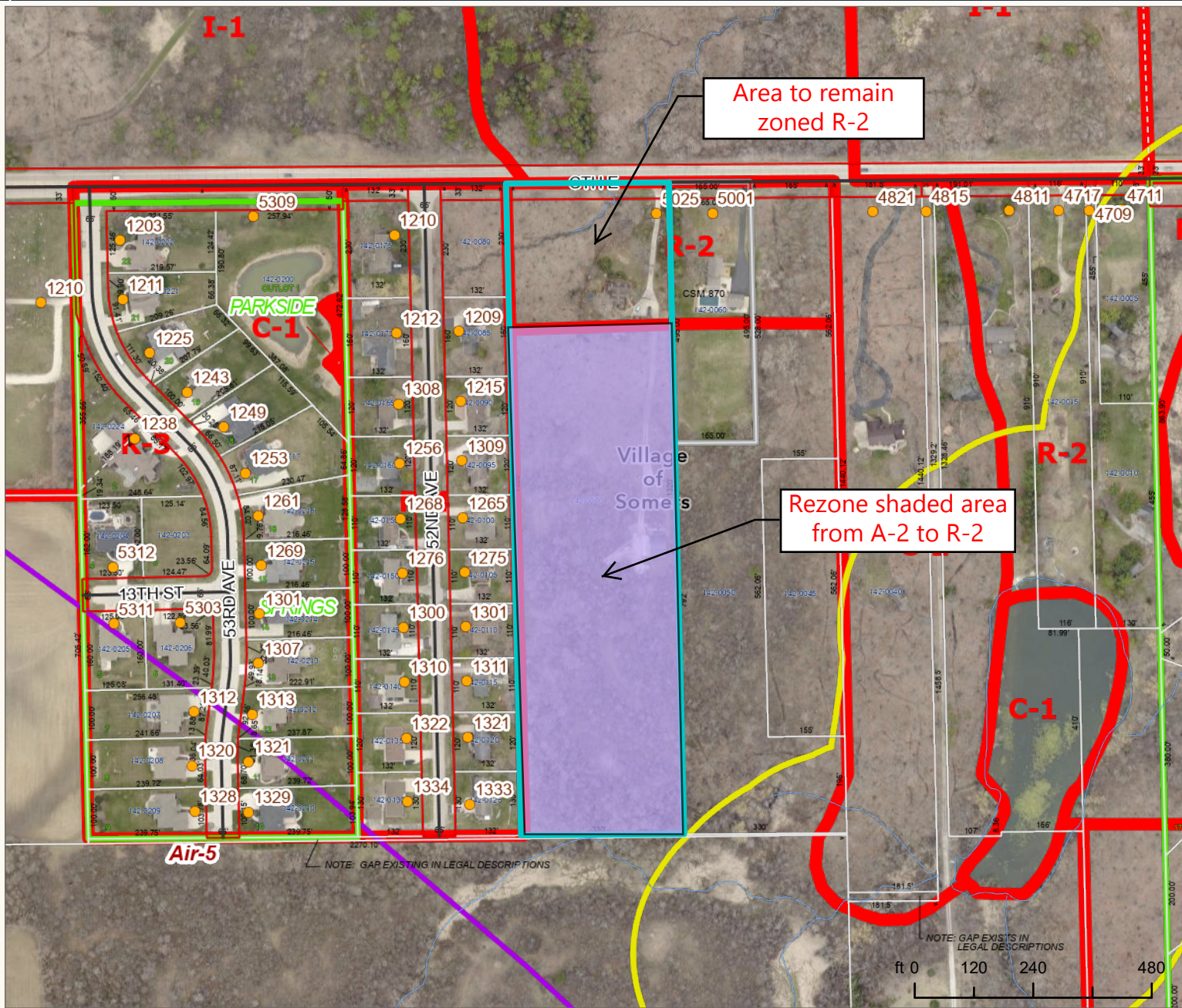
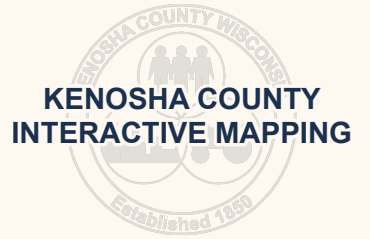
# 82-4-222-142-0070 - EXISTING ZONING



**DISCLAIMER** This map is neither a legally recorded map nor a survey and is not intended to be used as one. This drawing is a compilation of records, data and information located in various state, county and municipal offices and other sources affecting the area shown and is to be used for reference purposes only. Kenosha County is not responsible for any inaccuracies herein contained. If discrepancies are found, please contact Kenosha County.

Date Printed: 9/5/2024

# 82-4-222-142-0070 - PROPOSED ZONING



**DISCLAIMER** This map is neither a legally recorded map nor a survey and is not intended to be used as one. This drawing is a compilation of records, data and information located in various state, county and municipal offices and other sources affecting the area shown and is to be used for reference purposes only. Kenosha County is not responsible for any inaccuracies herein contained. If discrepancies are found, please contact Kenosha County.

Date Printed: 9/5/2024



# VILLAGE OF SOMERS

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## Department of Planning & Development

### VILLAGE OF SOMERS REZONING PROCEDURES

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- 1. Contact the Kenosha County Department of Planning & Development and check with staff to determine if your proposed zoning change meets the requirements of the Multi-Jurisdictional Comprehensive Plan for Kenosha County: 2035, Village of Somers General Zoning and Shoreland/Floodplain Zoning Ordinance and the Village of Somers Land Division and Platting Control Ordinance. Note: If the proposed rezoning is part of a proposed land division see the Certified Survey Map Information and Procedures.
  
- 2. Contact the Kenosha County Department of Planning & Development and schedule a pre-conference meeting, which is required for all rezoning requests.  
  
Meeting Date: 05/02/24
  
- 3. Contact the Village of Somers to determine if your rezoning petition requires concept review by the Village Plan Commission. If so, contact the Village of Somers clerk to schedule a concept meeting with the Village Plan Commission.  
  
Meeting Date: 05/02/24
  
- 4. Complete and submit to the Kenosha County Department of Planning & Development the Village of Somers Rezoning Application by the filing deadline.  
  
Filing Deadline: 08/30/2024
  
- 5. Upon submission you will be given two copies of the date-stamped application. Submit 10 copies of the date-stamped application to the Village of Somers clerk for placement on the agendas of the Village of Somers Plan Commission and the Village of Somers Board and keep one for your records.
  
- 7. Attend the Village Plan Commission and the Village Board meetings. **NOTE:** You must attend or the Village will not be able to act on your request. At these meetings you will be asked to brief the committee on your request.  
  
Village Plan Commission meeting date (tentative): 10/14/2024  
  
Village Board meeting date (tentative): 10/22/2024
  
- 8. Village clerk will provide written notice of final action to property owner/applicant.

**IMPORTANT TELEPHONE NUMBERS**

Kenosha County Center  
 Department of Public Works & Development Services  
 19600 - 75<sup>th</sup> Street, Suite 185-3  
 Bristol, Wisconsin 53104-9772

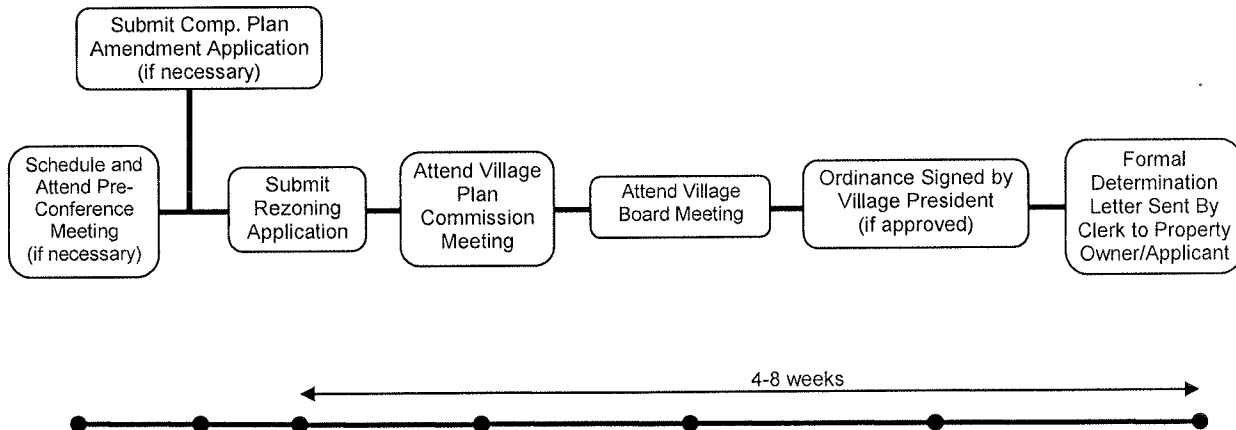
Division of Planning & Development (including Sanitation & Land Conservation) ..... **857-1895**  
 Facsimile #..... 857-1920

Public Works Division of Highways ..... 857-1870

Administration Building  
 Division of Land Information..... 653-2622

Village of Somers ..... 859-2822  
 Wisconsin Department of Natural Resources - Sturtevant Office ..... 884-2300  
 Wisconsin Department of Transportation - Waukesha Office ..... 548-8722

**Rezoning Procedure Timeline**



For Reference Purposes



Village Land Use Plan District Designation(s) (if applicable):

Present Suburban-Density Residential

Proposed Suburban-Density Residential

Present Use(s) of Property: Suburban-Density Residential

Proposed Use(s) of Property: Suburban-Density Residential

The subdivision abuts or adjoins a state trunk highway ..... Yes ( ) No (✓)

The subdivision will be served by public sewer ..... Yes (✓) No ( )

The subdivision abuts a county trunk highway ..... Yes (✓) No ( )

The subdivision contains shoreland/floodplain areas ..... Yes ( ) No (✓)

The subdivision lies within the extra-territorial plat (ETP) authority  
area of a nearby Village or City ..... Yes ( ) No (✓)

\*Applicant is responsible for submitting to the ETP authority any fees and documentation  
needed to obtain a recommendation.

REQUIRED SIGNATURE(S) FOR ALL APPLICATIONS:

Property Owner's Signature

Date

Property Owner's Signature

Date

REQUIRED APPLICABLE SIGNATURES:

Applicant's Signature

Date

Developer's Signature

Date

# PRELIMINARY CERTIFIED SURVEY MAP

## KENOSHA COUNTY CERTIFIED SURVEY MAP No. \_\_\_\_\_

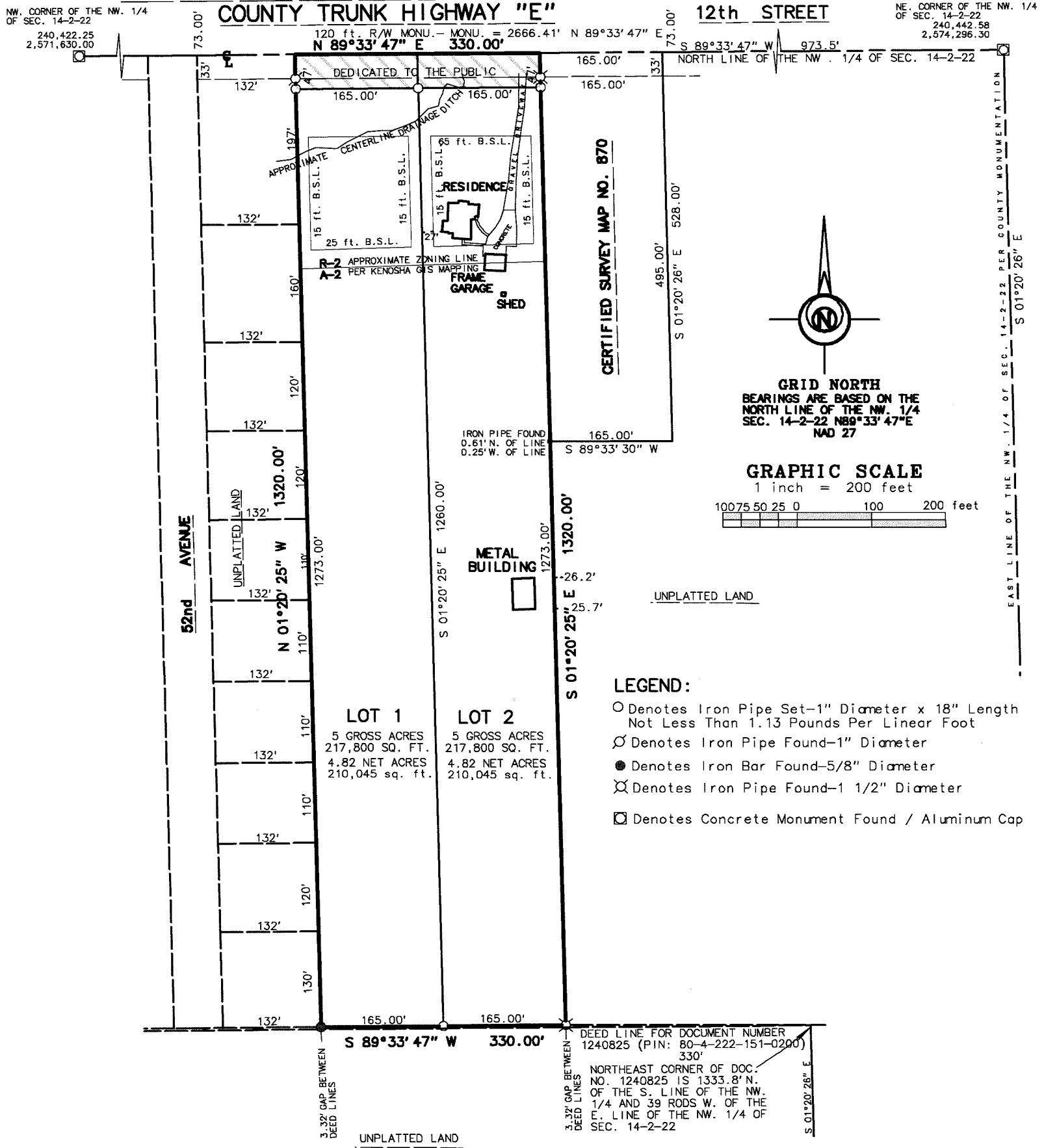
PART OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 14,  
TOWNSHIP 2 NORTH, RANGE 22 EAST OF THE 4TH PRINCIPAL MERIDIAN, VILLAGE  
OF SOMERS, COUNTY OF KENOSHA AND STATE OF WISCONSIN

ORDERED BY:

Steve Fredrick, Agent For The Owner  
5025-12th St.  
Kenosha, WI. 53144

SURVEYED & MAPPED BY:

AMBIT LAND SURVEYING  
8120-12th Ave.  
Wheatland, WI. 53105  
262-537-4874  
arbitpls@gmail.com



PRELIMINARY CERTIFIED SURVEY MAP

KENOSHA COUNTY CERTIFIED SURVEY MAP No. : \_\_\_\_\_

PART OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 14, TOWNSHIP 2 NORTH, RANGE 22 EAST OF THE 4TH PRINCIPAL MERIDIAN, VILLAGE OF SOMERS, COUNTY OF KENOSHA AND STATE OF WISCONSIN

SURVEYOR'S CERTIFICATE

I, Mark A. Bolender, Professional Land Surveyor, hereby certify:

That I have surveyed and mapped a part of the Northeast Quarter of the Northwest Quarter of Section 14, Township 2 North, Range 22 East of the 4th Principal Meridian, Village of Somers, Kenosha County, Wisconsin, described as follows:

Part of the Northeast Quarter of the Northwest Quarter of Section 14, Town 2 North, Range 22 East of the 4th Principal Meridian and more particularly described as follows: COMMENCING at the northeast corner of the Northwest Quarter of said section; THENCE South 89 degrees 33 minutes 47 seconds West, grid bearing from North, Wisconsin Coordinate System, South Zone, NAD 27, along the north line of said quarter section 973.50 feet to the POINT OF BEGINNING; THENCE South 01 degree 20 minutes 25 seconds East and parallel with the east line of said quarter section 1320.00 feet; THENCE South 89 degrees 33 minutes 47 seconds West and parallel with the north line of said quarter section 330.00 feet; THENCE North 01 degree 20 minutes 25 seconds West and parallel with the east line of said quarter section 1320.00 feet and to the north line of said quarter section; THENCE North 89 degrees 33 minutes 47 seconds East along the north line of said quarter section 330.00 feet to the POINT OF BEGINNING, containing 10.0 acres of land, more or less, Village of Somers, County of Kenosha and State of Wisconsin.

That I have made such survey, land division, and map by the direction of Steve Fredrick, agent to the owner of said land.

That such map is a correct representation of the exterior boundaries of the land surveyed and the division thereof.

That I have fully complied with the provisions of Chapter 236.34 of the Wisconsin Statutes and the subdivision regulations of the Village of Somers, in surveying, dividing, and mapping the same.

\_\_\_\_\_ Dated this 26th day of August, 2024.  
Mark A. Bolender, P.L.S. No. S - 1784

VILLAGE OF SOMERS APPROVAL

Approved as a Certified Survey Map this \_\_\_\_\_ day of \_\_\_\_\_, 2024.

\_\_\_\_\_, Date \_\_\_\_\_  
Jason Peters, Village Administrator

\_\_\_\_\_, Date \_\_\_\_\_  
Wendy Burnette, Village Clerk/Treasurer

OWNERS CERTIFICATE

STATE OF WISCONSIN }  
COUNTY OF KENOSHA } S.S.

As owner(s), I(we) hereby certify that I(we) caused the land described on this plat to be surveyed, divided, mapped and dedicated as represented on this Certified Survey Map. I(we) further certify that this Certified Survey Map to be submitted to the following for approval or objection:  
Village of Somers

Witness the hand and seal of said Owner this \_\_\_\_\_ day of \_\_\_\_\_, 2024  
in the presence of

\_\_\_\_\_  
John Spay Ruthann Spay

Witness

Personally came before me the above named Owner(s), to me known to be the person who executed the foregoing instrument and acknowledged the same.

Notary Public \_\_\_\_\_

Kenosha County, Wisconsin My Commission Expires \_\_\_\_\_



**TONY EVERS**  
GOVERNOR  
**KATHY BLUMENFELD**  
SECRETARY  
**Plat Review**  
PO Box 1645, Madison WI 53701  
E-mail: [plat.review@wi.gov](mailto:plat.review@wi.gov)  
<https://doa.wi.gov/platreview>

October 09, 2024

Mark A. Bolender  
AMBIT LAND SURVEYING  
ambit@tds.net

FILE NO. 122027  
Certified Survey Map

Village of Somers, Kenosha County

Dear Mark A. Bolender:

Per request, we have reviewed this certified survey map (CSM) for compliance with s. 236.34 Wis. Stats. Our comments are as follows:

**REVIEW COMMENTS:**

s. 236.20 (2) (c) It appears that the 1260.00' length shown for the line common to Lot 1 & Lot 2 should be changed to 1273.00' so that the lots will close to the net areas as noted.

s. 236.20 (2) (j) The gross areas shown for the lots are not appropriate and should be removed as the dedicated to the public areas are not part of the lots.

s. 236.34 (1m) (c) The CSM must be plotted for recording on 8.5" x 14" durable white media with a 1 1/2" binding margin along the top and 1/2" margins on all other sides (the version submitted to the department for review is on 11" x 17" paper size).

s. 236.34 (1m) (d) The Surveyor certificate must state compliance with Section (not Chapter) 236.34, Wis. Stats.

Note: The north part of the line common to lots 1 & 2 does not extend to the section line and should be removed.

If you have any questions regarding this review, please contact me as listed above.

Regards,

Don Sime, PLS  
Plat Review

Enc: Certified Survey Map

cc: Clerk, Village of Somers



# PRELIMINARY CERTIFIED SURVEY MAP

## KENOSHA COUNTY CERTIFIED SURVEY MAP No. : \_\_\_\_\_

PART OF THE NORTHEAST QUARTER OF THE NORTHWEST QUARTER OF SECTION 14,  
TOWNSHIP 2 NORTH, RANGE 22 EAST OF THE 4TH PRINCIPAL MERIDIAN, VILLAGE  
OF SOMERS, COUNTY OF KENOSHA AND STATE OF WISCONSIN

### SURVEYOR'S CERTIFICATE

I, Mark A. Bolender, Professional Land Surveyor, hereby certify:

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That I have made such survey, land division, and map by the direction of Steve Fredrick, agent to the owner of said land.

That such map is a correct representation of the exterior boundaries of the land surveyed and the division thereof.

That I have fully complied with the provisions of <sup>section</sup> Chapter 236.34 of the Wisconsin Statutes and the subdivision regulations of the Village of Somers, in surveying, dividing, and mapping the same.

\_\_\_\_\_ Dated this 26th day of August, 2024.  
Mark A. Bolender, P.L.S. No. S - 1784

### VILLAGE OF SOMERS APPROVAL

Approved as a Certified Survey Map this \_\_\_\_\_ day of \_\_\_\_\_, 2024.

\_\_\_\_\_, Date \_\_\_\_\_  
Jason Peters, Village Administrator

\_\_\_\_\_, Date \_\_\_\_\_  
Wendy Burnette, Village Clerk/Treasurer

### OWNERS CERTIFICATE

STATE OF WISCONSIN }  
COUNTY OF KENOSHA } S.S.

As owner(s), I(we) hereby certify that I(we) caused the land described on this <sup>map</sup> plat to be surveyed, divided, mapped and dedicated as represented on this Certified Survey Map. I(we) further certify that this Certified Survey Map <sup>is</sup> to be submitted to the following for approval or objection:  
Village of Somers

Witness the hand and seal of said Owner this \_\_\_\_\_ day of \_\_\_\_\_, 2024  
in the presence of

\_\_\_\_\_  
John Spay Ruthann Spay

\_\_\_\_\_  
Witness

Personally came before me the above named Owner(s), to me known to be the person who executed the foregoing instrument and acknowledged the same.

Notary Public \_\_\_\_\_

Kenosha County, Wisconsin My Commission Expires \_\_\_\_\_

**VILLAGE OF SOMERS  
VILLAGE BOARD  
WORK SESSION ITEM MEMORANDUM**

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**WORK SESSION:** November 6<sup>th</sup>, 2024

**TO:** Village President Stoner and Board of Trustees

**PREPARED BY:** Jason J. Peters, Administrator

**AGENDA ITEM:** #6 Discuss Plan Commission Recommendation (Shoreland Lutheran High School):

- a. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. (For information use only, the property is located at 9026 12th Street.)
- b. Request by Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), for approval of site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. (For information use only, the property is located at 9026 12th Street.)

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**BACKGROUND:**

In April of 2024, the Board approved a Comp Plan Change, Rezoning and CSM so that Shoreland Lutheran High School could install a stormwater management system. The proposed system is what is known as a Regenerative Stormwater Conveyance. This system was also in preparation for any expansions to their facilities. In August of 2024, Staff met with Shoreland Lutheran to review their proposed additions to their existing buildings. The proposed additions will include the following: an expanded music room, 8 new classrooms, a new main entry/vestibule area, a new fitness center, and a new gymnasium. The existing gymnasium will also be remolded as a part of the plan. The project requires a conditional use permit for the new gymnasium and site plan/exterior fenestration. Shoreland Lutheran submitted their applications at the end of August.

## UPDATE:

The Plan Commission met on October 14th to review Shoreland Lutheran's conditional use permit application and the requisite public hearing was held. Public comments were given. One individual was concerned over possible parking on the road and noise from the gymnasium during events. The main concern during public comments stems from if water main is brought to the site how would this be paid for and how would the residents be assessed. Administrator Peters will note that under the Village's current ordinance 13.07 (E) the water main would be assessed by front footage. The section reads as follows:

“The amount assessed against any property shall be computed on a front footage basis using the total cost of the improvement including the construction cost for the size of main required to adequately serve and benefit the property as determined by the Utility, but in no case shall such main be less than eight inches in diameter nor shall such main be larger than 12 inches in diameter. The costs of engineering and legal services and any other component of direct or indirect cost which is attributed to the improvement, including the cost of that portion of the water main in intersections of streets shall be included in computing the costs of the improvement.”

The Board may recall that in 2021, Mastercraft proposed a subdivision across the street from this site. At that time the estimate for the total cost of bring water main from 100<sup>th</sup> Avenue was \$807,000. The Plan Commission's recommendation was to approve the request for the conditional use permit (6-0) and the request for site plan and exterior fenestration. (6-0).

## COMMENTS:

There does not appear to be any major issues with the project as a whole. The main issue, as was the case in the Mastercraft proposal, is whether water main will be extended from 100<sup>th</sup> Avenue. On October 15<sup>th</sup>, Administration, President Stoner, and Attorney Davison met with legal counsel for Shoreland Lutheran to discuss the water main issue. At this time, we reviewed the assessment report from 2021, discussed the Village's current ordinance for assessing water main, and discussed options for funding the project.

For our Work Session, Administration would suggest that if and when the Board determines that they are agreeable to Shoreland Lutheran's planned expansion, that clear direction be given as to whether the water main will be extended from 100<sup>th</sup> Avenue. If this determination is made, Administration requests that direction be given to our Engineers to update their assessment report from 2021. The Board will also need to determine how this project will be funded. Administration also recommends that once a clearer picture on how the main will be extended that a meeting be held with those property owners that have front footage along CTH E.

In closing, Shoreland Lutheran has provided a TIA for the project and has received their Airport Overlay approval from the City of Kenosha.

## ATTACHMENTS

Planning and Zoning Memo to Plan Commission dated September 30<sup>th</sup>, 2024

Engineering Comments

Plan Commission Minutes – Highlighted

Conditional Use Application

Conditional Use Narrative

Civil Plans

Stormwater Memo

Floor Plans

Exterior Elevations

Landscaping Plans

Checklist

Traffic Impact Analysis



# KENOSHA COUNTY

Shelly Billingsley, Director  
Department of Public Works &  
Development Services

Andy M. Buehler, Director  
Division of Planning & Development

**TO: Village of Somers Plan Commission**  
**FROM: Luke Godshall, Senior Land Use Planner**  
**APPLN DATE: 08-30-24**  
**RPT DATE: 09-30-24**  
**MTG DATE: 10-14-24**  
**RE: Shoreland Lutheran H.S. Conditional Use Permit & Site Plan Review**

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## PROJECT/SITE INFO:

1. Petitioner/Agent: Tom Schermerhorn, Excel Engineering, Inc.
2. Property Owner: Shoreland Lutheran High School
3. Location/Address: 9026 12TH ST
4. Tax key Number(s): 82-4-222-084-0272
5. Area: 55.74 acres
6. Existing Zoning: I-1 Institutional Dist.
7. Proposed Zoning: I-1 Institutional Dist.
8. Existing Land Use: Governmental and Institutional
9. Proposed Land Use: Governmental and Institutional

---

## PROJECT OVERVIEW:

The Petitioner requests conditional use and site plan/building fenestration review and approval for several proposed building additions onto the existing Shoreland Lutheran High School building, along with additional parking areas. The site is zoned I-1 Institutional District, which requires conditional use approval for school auditoriums, gymnasiums and stadiums (the submitted plans indicate the addition of a new practice gymnasium, as well as the remodeling of the existing gymnasium).

## PLANNER COMMENTS:

### **Building Additions & Design:**

The submitted plans indicate a total area of 31,569 sq. ft. of building additions, with another 11,047 sq. ft. of existing areas to be remodeled. The new building additions include an expanded music room, 8 new classrooms, a new main entry/vestibule area, a new fitness center, and a new gymnasium (in addition to remodeling the existing gymnasium). The new gymnasium requires conditional use approval under Village ordinance.

Building elevations and renderings submitted indicate the building additions to be constructed with a variety of materials including brick veneer, fiber cement panels, and aluminum composite all in a variety of colors. In addition, the existing school building is shown to have its exterior updated with new brick finish and exterior nichia panels.

All proposed building additions meet all minimum required setback distances from property lines and is less than the maximum allowable building height of 60'.

**Parking:**

The submitted site plans indicate new parking areas to be added along the south and east ends of the site, with the new parking at the east end of the site replacing the area of the baseball field. The number of total parking spaces according to the submitted plans is 533 spaces. Additionally, the plan indicates 8 bus parking stalls.

Village zoning ordinance parking requirements for colleges and high schools calls for 1 space for every 5 students, plus one space for each employee on the largest work shift. Of the 533 total spaces, 70 would be for employees (according to the information provided on the submitted site plan review checklist). This leaves 463 spaces available for students, enough to accommodate 2,315 students.

The conditional use for the proposed gymnasium requires an additional parking requirement of one space for every 3 seats within the gymnasium. The submitted plans do not indicate the seating capacity of the new and remodeled gymnasiums to be able to determine if the total of 533 parking spaces would be enough to accommodate employees, students and gymnasium seating requirements. Staff has requested the Petitioner provide the total seating capacity of both the new gymnasium and remodeled existing gymnasium.

Sixteen (16) of the proposed parking spaces shown at the southwestern portion of the site would be located closer than the normally required 20' setback from a public right-of-way. The width of the CTH E right-of-way dedication was recently widened, causing Shoreland's existing parking spaces along CTH E to be closer than the 20'. Shoreland wishes to locate the new parking spaces to be in line with those existing spaces to the east and has requested the reduced setback as part of the Conditional Use and site plan approval.

All parking spaces on the submitted site plans are shown to be 9'x18' in size and provide for 24' wide aisles between the ends of parking spaces to meet ordinance requirements.

**Landscaping:**

A landscaping plan (dated 8-27-24 prepared by Heller & Associates, LLC) was included with the submitted application materials. The plan provides for a wide variety of deciduous and evergreen trees, shrubs and perennials and also indicates that existing trees and vegetation would be retained.

While the landscaping plan generally meets ordinance landscaping requirements, it is recommended that berms be utilized along the parking areas which abut the CTH E and CTH H rights-of-way. The landscaping section of the zoning ordinance indicates that "the use of berms shall constitute over 75% of the parking areas abutting a right-of-way. The berm shall be designed to be meandering and undulating with a minimum height of four feet with slopes no greater than 4:1."

**Lighting:**

A site photometric plan (dated 8-30-24 prepared by Excel) was included with the submitted application materials. The plan shows that the proposed lighting intensity and proposed lighting fixtures meet ordinance standards.

This project will require approval from the City of Kenosha for an Airport Site Plan review, as the property is located within a City of Kenosha Airport Overlay zoning district.

Planning & Development will issue zoning permits for the project only after the Petitioner has executed a Developer's Agreement with the Village and has met any other of the Village's requirements for the development.

**STAFF RECOMMENDATION:**

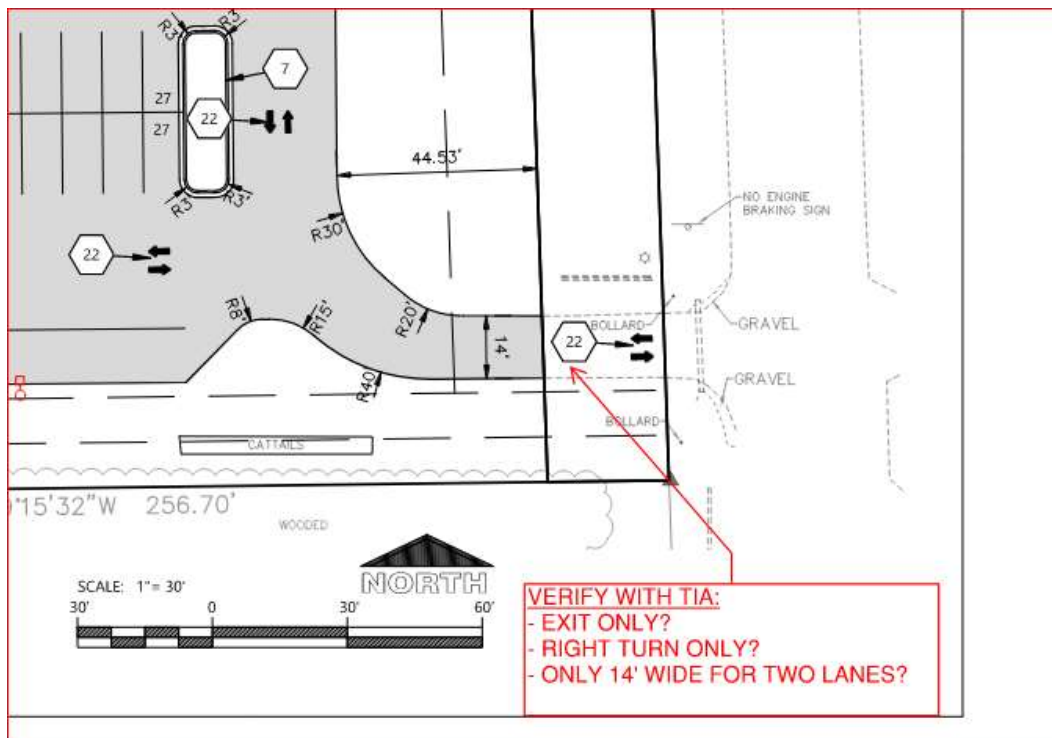
Should the Plan Commission choose to recommend approval of this project, staff would recommend the following conditions:

1. The Conditional Use shall be subject to the conditions present in the Village of Somers General Zoning and Shoreland/Floodplain Zoning Ordinance Section ZN 5.03(8)(b)121; School auditoriums, gymnasiums and stadiums in the I-1 District:
  - a. At least one (1) off-street parking space shall be provided for every three (3) seats located within the auditorium, gymnasium or stadium  
*(Note: Petitioner shall provide seating capacity total of the gymnasiums to determine if this condition is being met)*
  - b. Ingress and egress to the premises shall be determined with due regard to topography and public road and pedestrian traffic taking into consideration hills, curves, speed limits and vision clearance.
  - c. Night lighting shall not shine on adjoining property.
  - d. In the case of outdoor stadiums, the parking area shall be paved or maintained in a dust free condition.
2. The Conditional Use approval to allow for the addition of sixteen (16) parking spaces at the southwest portion of the site to be located closer than 20' from the public right-of-way.
3. Subject to a revised landscaping plan being submitted which includes berms along at least 75% of the parking areas abutting a right-of-way.
4. Subject to submitting plans to be reviewed/approved for any proposed trash enclosures to be located on the site.
5. Subject to receiving approval of any necessary Traffic Impact Analysis (TIA) as may be required by Kenosha County Division of Highways.
6. Subject to receiving approval of an Airport Site Plan Review by the City of Kenosha.
7. Subject to final review and approval by the Village of Somers engineer.
8. Subject to approval by the Somers Fire & Rescue Department.
9. Subject to a Kenosha County zoning permit being issued for any proposed structures including principal buildings, signage, fences, accessory buildings, etc.
10. Subject to implementation of a Developer's Agreement between the Petitioner and the Village of Somers.

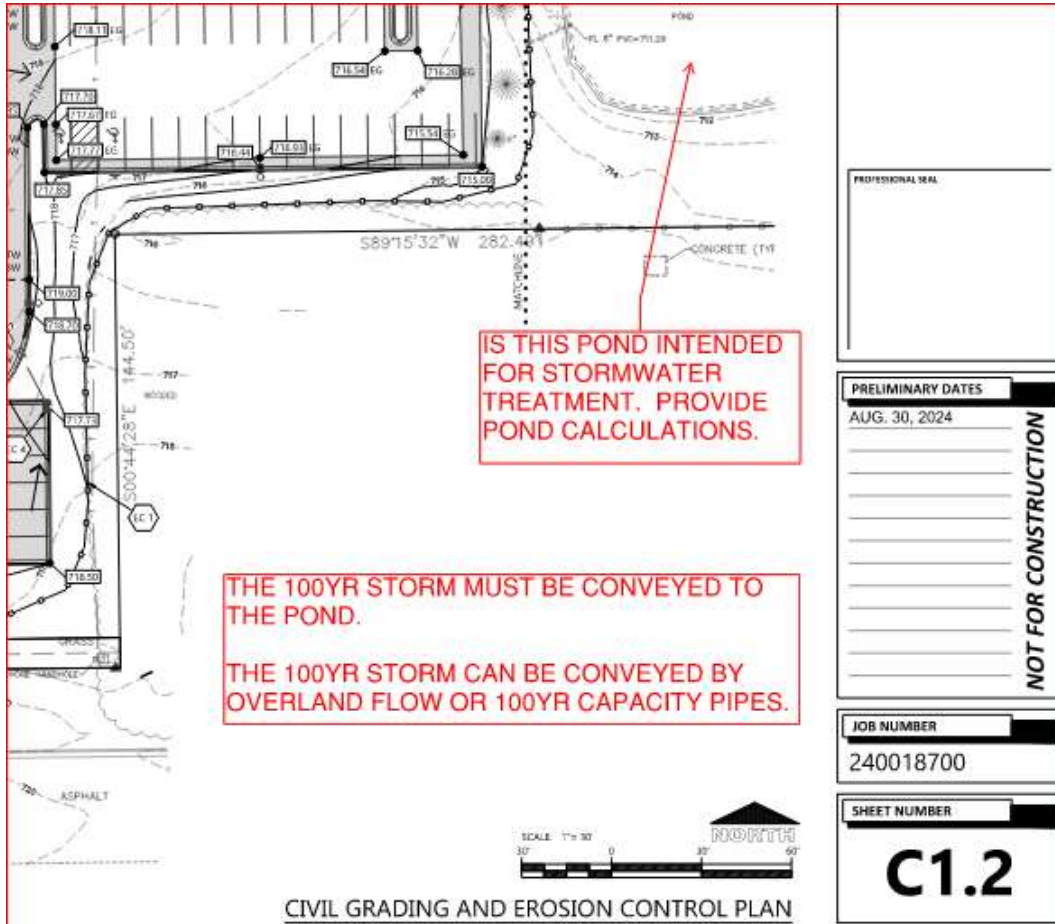
**Shoreland Lutheran High School**  
**October 14, 2024 Plan Commission Meeting**  
Village of Somers

We reviewed the plans and stormwater memo dated August 30, 2024; our comments are listed below.

1. Provide full engineering plans and calculations consolidating the stormwater plan and the site development plan.
2. It is our understanding that the school may be required to extend the public water main to provide water service and fire protection. Provide engineering plans if required.
3. Sheet C1.1: Verify that the existing driveway to CTH H can remain based on the traffic impact analysis (TIA). The driveway is too narrow for two lanes of traffic as shown on the plans.

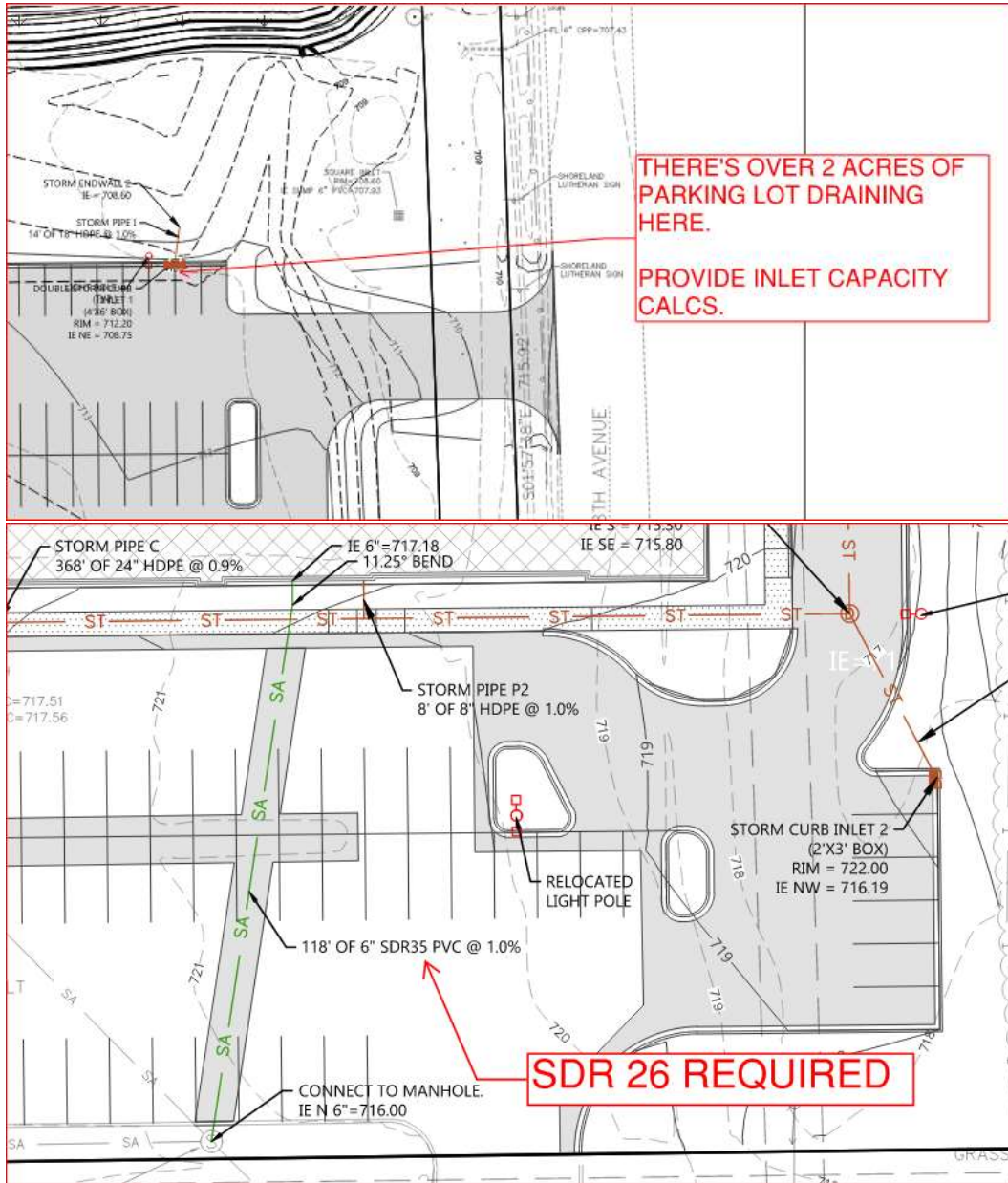


4. Sheet C1.2:
  - a. Provide pond calculations if the existing pond is intended to provide storm water treatment.
  - b. The 100-yr storm must be conveyed to the stormwater ponds, using overland flow or 100-yr capacity pipes.



5. Sheet C1.3:

- a. Provide inlet capacity calculations for the parking lot.
- b. Verify that the stormwater is not overtopping the inlet and bypassing to the CTH H ditch.
- c. SDR 26 PVC pipes are required for sanitary laterals.



Status of Engineer's Approval: Not Approved

*Brett D. Biver*

Brett D. Biver, PE

Cc: Jason J. Peters, JD/MPA – Village Administrator  
George Stoner – Village President  
Doug Snyder, P.E. – Village Engineer





Village of Somers  
Proceeding from the Village Plan  
Commission Meeting  
Oct. 14, 2024

## 1. Call to Order

Chairman Stoner called the meeting to order at 5:28 p.m.

Present:

- Chairman George **Stoner**
- Commissioner Troy **Steege**
- Commissioner Gregg **Thompson**
- Commissioner Vinnie **Chambers**
- Commissioner Don **Boxx**
- Commissioner Michael **DeLuca**

Absent:

- Commissioner Jerry **Romanowski**
- Alternate Pat **Juliana**

Staff present in person: Administrator Jason Peters, Assistant to the Administrator Kevin Poirier, Somers Fire Chief Ben Andersen, Deputy Chief Jodin Froeber

Village Trustees: Jackie Nelson

Kenosha County Planners: Luke Godshall

Village Engineer: Brett Biwer

## 2. Pledge of Allegiance

Chairman **Stoner** led everyone in the Pledge of Allegiance.

## 3. Approve Minutes of the Aug. 12, 2024 Meeting

Commissioner **Boxx** moved to approve to the minutes.

Seconded by Commissioner **Steege**.

Motion carried. 6-0 vote.

## 4. Correspondence

None

## 5. Citizen Comments

Tom **Harper** – 8910 12<sup>th</sup> Street

Mr. **Harper** mentioned that his property is very close to the school and bring up parking concerns. He mentions that cars have parked on the road during events and it is a safety concern, He also mentions noise from indoor and outdoor events and hopes that sound proofing options will be considered.

6. **Rezone: Public Hearing and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requesting approval of a rezoning from R-2 Suburban Single-Family Residential Dist. & A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52<sup>nd</sup> Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

7. **Land Division (Certified Survey Map): Discussion and Action on Request by:** John & Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), requests approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. *(For information only, the property is located on the south side of Highway E, two parcel east of 52<sup>nd</sup> Avenue.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Commissioner **Thompson** seconds the motion.

Motion carried. 6-0.

Passes unanimously

8. **Comprehensive Plan Amendment: Public Hearing and Action on Request by:** Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St., Kenosha, WI 53142 (Agent); requests an amendment to the land use plan map for the Village of Somers (Map 80) as adopted in the Multi-

Jurisdictional Comprehensive Plan for Kenosha County: 2035 from High-Density Residential to Medium-Density Residential, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.

Administrator **Peters** mentions that there have been changes to the project since the concept was presented to the board at their July 8<sup>th</sup> meeting.

Bear Development Project Manager Dan **Szczap** mentions that the project presented was a residential mix of 33 single-family homes and 64 two-family buildings that received positive feedback when it was presented in July. They therefore started the process to re-zone and amend the comprehensive land use plan accordingly. They are proposing zero lot-line parcels to accommodate the two-family buildings. It would look like a condominium but be on a fee-simple lot. They have done this before in Mount Pleasant.

**Szczap** suggests three ways to move forward:

1. Move forward with the concept plan and using a PUD to handle deviations
2. Changing the two-family dwellings to condos
3. Changing the zoning code to accommodate zero-lot-line development

Administrator **Peters** mentions that Bear is also looking at the possibility of filling the site and changing it to single-family homes.

**Szczap** mentions that the soil condition and high ground water on the property has been challenging and that the fill would allow for the structures to be built with basements. They are in discussion to secure fill for the site but that has not been finalized.

Commissioner **Boxx** gets confirmation from **Szczap** that if sufficient fill is secured, the duplexes would be changed to single-family homes on the entire site. If the fill is not secured, the developer would plan to move forward with zero-lot-line dwellings to accommodate the challenges of the site and be a financially viable project. The developer states that condominiums is a complicated form of ownership and is not their preference.

Commissioner **Boxx** mentions that the developer is asking for a reduction of the setbacks from what the zoning allows.

Commissioner **Steege** asks whether, in the case the site is converted to single-family homes, the setbacks would be compliant with current ordinances.

**Szczap** states that he is not sure as the site plan is likely to change and they may ask for

minor relief.

Commissioner **Steege** asks for clarification about the changes from the plan resented in July.

**Szczap** states that the building and roads through the site did not change, only the lot lines through the duplexes have changed to create zero-lot-line fee-simple lots.

Commissioner **Boxx** asks details about the maintenance of the property.

**Szczap** explains that declarations, homeowner association and/or covenants would govern maintenance, common areas, architecture and insurance.

Commissioner **DeLuca** asks again whether the developer would still ask for zero-lot-lots if they secured the fill.

**Szczap** states that if the fill is secured, they would eliminate the two-family units in favor of single-family lots.

Chairman **Stoner** is concerned about zero-lot-line and issues with neighbors maintaining their properties to different standards.

**Szczap** states that the management documents would be similar to a condominium.

Chairman **Stoner** mentions concerns about the fill process and that the size of the lots for single family homes would have to change.

Chairman **Thompson** mentions being in favor of the medium density single-family project but is concern about the timing of the fill as it relates to weather.

Commissioner **Boxx** asks whether the management documents from a similar site could be provided to the commission for review.

**Szczap** states that that document is owned by a third party and would need approval before sharing it.

Commissioner **Steege** makes a motion to approve the request.

Commissioner **Chambers** seconds the motion.

Motion carried. 6-0.

Passes unanimously

## 9. **Rezoning w/ PUD:**

**Public Hearing and Action on Request by:** Berwick Properties Inc, 4011 80th St, Kenosha, WI 53142 (Owner), Daniel Szczap, Bear Development, LLC, 4011 80th St.,

Kenosha, WI 53142 (Agent); requesting approval of a rezoning from R-9 Multiple-Family Residential Dist., C-1 Lowland Resource Conservancy Dist., and A-2 General Agricultural Dist. to R-9 Multiple-Family Residential Dist., R-5 Urban Single-Family Residential Dist., and C-1 Lowland Resource Conservancy Dist., and PUD Planned Unit Development Overlay, on Tax Parcel #s 82-4-222-151-0560, 82-4-222-151-0561, 82-4-222-151-0563, located in the NE 1/4 of Section 15, T2N, R22E, Village of Somers. *(For information use only, the property is East and South of the Pike Creek Lift Station.)*

Public comments: none

Commissioner **DeLuca** makes a motion to table the request.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

#### **10. Conditional Use Permit:**

**Public Hearing and Action on Request by:** Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requesting a Conditional Use Permit to allow a school gymnasium in the I-1 Institutional Dist., on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments:

**Tom Terletzky** – 9015 12<sup>th</sup> Street

Mr. **Terletzky** mentions his support for the school's expansion. He raises concerns about the extension of the water to the property from 100<sup>th</sup> Avenue. That resulted in the homeowners paying a large amount of the cost to the benefit of a proposed subdivision that would use a lot more of that water. **Terletzky** objects to the high cost to homeowners along 12<sup>th</sup> Street. He mentions that the Village has the authority to decide how the cost of the pipe would be charged to the benefiting properties.

**Terletzky** proposes a 20/30/50 cost split as follows: 20 percent would be paid by the school, 50 percent would be paid by the proposed subdivision and the remaining would be paid by the residents along Highway E according to frontage. The subdivision would pay back the assessment when the lots are sold. He mentions that this solution would speed up repayment.

Chairman **Stoner** mentions that they have no control over developers and mentions the additional infrastructure cost incurred to develop a subdivision. He states that other communities also assess property owner for utilities going by their properties. He also disputes the benefits

**Terletzky** mentions that homeowners have been subsidizing growth in their community

by paying for the infrastructure and that it is not fair. He mentions that residents are not notified of upcoming assessments until it is too late.

Chairman **Stoner** asks what the public notification process is.

Administrator **Peters** explains the process and what would trigger notification.

**Terletzky** mentions that by the time homeowners are notified it is too late for residents to object. He mentions that by that time the project has been approved and bids are under way. He reiterates that having residents pay for 70 percent of the cost is wrong.

Commissioner **Boxx** makes a motion to approve the request.  
Commissioner **Steege** seconds the motion.

Chairman **Stoner** is concerned that the stormwater part of the project has not started. He also mentions that he will not support the school expansion without connection to municipal water in part due to fire protection. He mentions his support for the overall projects but also mentions that future athletic fields would need to be serviced by municipal water for bathrooms and any concession stands.

Commissioner **Steege** mentions his concerns with the stormwater plan, the traffic analysis and appropriate parking.

Excel Engineer Jeremy **Wagner** mentions that they are coordinating with Kenosha County about the stormwater project. He mentions that they just received the traffic analysis and shows only one additional car stacking at the four-way intersection. As to the parking, he states that the project will add 250 stalls.

Commissioner **Steege** asks whether the parking calculation accounts for gymnasium activities.

**Wagner** states that it does.

Chairman **Stoner** asks whether they are designing the water system for the project.

**Wagner** states that they are doing the plumbing but not the water main coming to the property. He states that the Village Engineer will design that.

Engineer Brett **Biwer** mentions that the design is very preliminary so far.

9100 11<sup>th</sup> Street

Fire Chief **Andersen** whether the State has reviewed the fire suppression system.

Chief **Andersen** states that it would not happen until the project is approved. He states that while the State would not force the property owner to connect to the water but the local ordinance would within six months. He mentions that it would be more cost effective to

plan for municipal water connection at the time of the expansion.

Shoreland Lutheran High School President Paul **Scriver** mentions that they are waiting for a final decision about municipal water connection. He states that preliminary engineering states they do not need to connect. Once the school hears that the water connection is necessary, they will design the project accordingly. He states that the school is not opposed to connection to municipal water.

Commissioner **Thompson** asks about the process to design and bidding for the water line going down 12<sup>th</sup> Street.

Administrator **Peters** explained that the Village engineer would start the design once the project is approved.

Commissioner **Thompson** asks if the design process would start before a decision was made about how the project would be paid for. He also asks whether the subdivision project is moving forward.

Administrator **Peters** answers that there has been no contact with the subdivision developer for the past two years.

Commissioner **Steege** mentions that the commission is not responsible for deciding how the project is financed, only approving the plans. The question of the water connection will be up to the Village Board.

Guy Hawley - 9100 11<sup>th</sup> Street

Mr. **Hawley** mentions that the construction of his house is delayed until the school's stormwater system is in place.

Chairman **Stoner** interjects that this issue has nothing to do with the school's project. He is aware of the situation and offers to speak with Hawley after the meeting.

Commissioner **Boxx** makes a motion to approve the conditional use permit.

Commissioner **Steege** seconds the motion.

Motion carried. 6-0.

Passes unanimously

## **11. Site Plan Review:**

**Discussion and Action Request by:** Shoreland Lutheran High School Federation, Inc., 9026 12th St., Kenosha, WI 53144, (Owner), requests site plan review and exterior fenestration review on Tax Parcel # 82-4-222-084-0272, located in the SE 1/4 of Section 8, T2N, R22E, Village of Somers. *(For information use only, the property is located at 9026 12th Street.)*

Public comments: none

Commissioner **Boxx** makes a motion to approve the request.  
Commissioner **Steege** seconds the motion.

Commissioner **Steege** asks details about the rooftop AC units.

Excel Engineer Alex **Fiebig** answers the questions and states that the number of unit has been reduced and features a two feet parapet.

Motion carried. 6-0.

Passes unanimously

## 12. Adjourn

Commissioner **Boxx** makes a motion to adjourn.  
Commissioner **Steege** seconds the motion.  
Motion carried 6-0 vote.

Passes unanimously

Drafted Oct. 16, 2024.

These minutes are not official until approved by the Plan Commission. Submitted by Assistant to the Administrator/Communication Specialist Kevin Poirier



# VILLAGE OF SOMERS

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## Department of Planning & Development

### VILLAGE OF SOMERS CONDITIONAL USE PERMIT PROCEDURES

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- 1. Contact the Kenosha County Department of Planning & Development and check with staff to determine if your proposed use is a permitted use, an accessory use or a conditional use. If it is a conditional use, then a conditional use permit must be applied for and received prior to occupying or using the site for that use.
  
- 2. Contact the Kenosha County Department of Planning & Development and schedule a pre-conference meeting, which is required for all conditional use permit requests.  
  
Meeting Date: 7/18/24
  
- 3. Contact the Village of Somers to determine if your conditional use permit application requires requires concept review by the Village Plan Commission. If so, contact the Village of Somers clerk to schedule a concept meeting with the Village Plan Commission.  
  
Meeting Date: 7/18/24
  
- 4. Complete and submit to the Kenosha County Department of Planning & Development the Village of Somers Conditional Use Permit Application by the filing deadline.  
  
Filing Deadline: 8/30/24
  
- 5. Upon submission your will be given two copies of the date-stamped application. Submit a copy of the date-stamped application to the Village of Somers clerk for placement on the agendas of the Village of Somers Plan Commission and the Village of Somers Board. Keep the other copy for your records.
  
- 6. Attend the Village Plan Commission and the Village Board meetings. **NOTE:** You must attend or the Village will not be able to act on your request. At these meeting you will be asked to brief the committee on your request and ultimately agree to a list of conditions of approval relating to your proposed use.  
  
Village Plan Commission meeting date (tentative): 10/14/24  
  
Village Board meeting date (tentative): 10/15, 11/6 & 11/12
  
- 7. Village clerk will provide written notice of final action to property owner/applicant.
  
- 8. If approved, you may commence with applying for and obtaining any necessary zoning permit(s) for construction (i.e. new buildings, building additions, signage, fencing, etc...) with the Kenosha County Department of Planning and Development.

**IMPORTANT TELEPHONE NUMBERS**

Kenosha County Center  
 Department of Public Works & Development Services  
 19600 - 75<sup>th</sup> Street, Suite 185-3  
 Bristol, Wisconsin 53104-9772

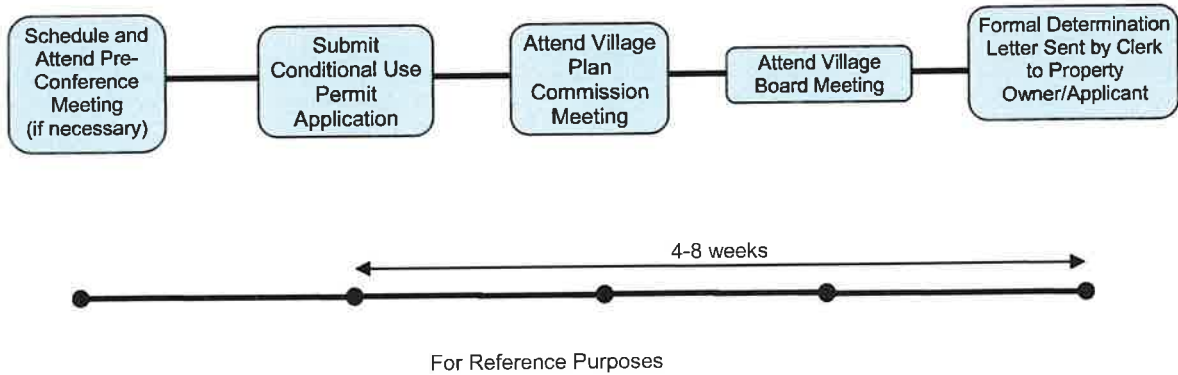
Division of Planning & Development (including Sanitation & Land Conservation)..... **857-1895**  
 Facsimile #..... 857-1920

Public Works Division of Highways ..... 857-1870

Administration Building  
 Division of Land Information..... 653-2622

Village of Somers ..... 859-2822  
 Wisconsin Department of Natural Resources - Sturtevant Office ..... 884-2300  
 Wisconsin Department of Transportation - Waukesha Office ..... 548-8722

**Conditional Use Permit Timeline**





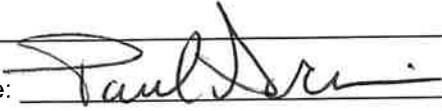
# VILLAGE OF SOMERS

## Department of Planning & Development

### CONDITIONAL USE PERMIT APPLICATION

(a) Property Owner's Name:

Shoreland Lutheran High School Federation, Inc

Print Name: Paul Scriver Signature: 


Mailing Address: 9206 21st Street

City: Kenosha State: WI Zip: 53144

Phone Number: 262-515-4527 E-mail (optional): scriver@slhs.com

Note: Unless the property owner's signature can be obtained in the above space, a letter of agent status signed by the legal property owner must be submitted if you are a tenant, leaseholder, or authorized agent representing the legal owner, allowing you to act on their behalf.

(b) Agent's Name (if applicable):

Print Name: Tom Schermerhorn Signature: 


Business Name: Excel Engineering Inc

Mailing Address: 100 Camelot Dr

City: Fond du Lac State: WI Zip: 54935

Phone Number: 920-322-1751 E-mail (optional): tom.s@ExcelEngineer.com

(c) Architect's Name (if applicable):

Print Name: Tom Schermerhorn Signature: 


Business Name: Excel Engineering Inc

Mailing Address: 100 Camelot Dr

City: Fond du Lac State: WI Zip: 54935

Phone Number: 920-322-1751 E-mail (optional): tom.s@ExcelEngineer.com

(d) Engineer's Name (if applicable):

Print Name: Ben Warntjes Signature: 

Business Name: Excel Engineering Inc

Mailing Address: 100 Camelot Dr

City: Fond du Lac State: WI Zip: 54935

Phone Number: 920-322-1575 E-mail (optional): ben.w@excelengineer.com

(e) Tax key number(s) of subject site:

82-4-222-084-0272

Address of the subject site:

9026 12th St

(f) Plan of Operation (or attach separate plan of operation)

Type of structure:

School building

Proposed operation or use of the structure or site:

Gym and classroom addition on the east side of the building, choir room addition to the southwest corner, parking lot modifications and parking lot expansion to the east. CUP is required for the gym.

Number of employees (by shift): 50 existing, 10 proposed, 1 shift

Hours of Operation: 7:45 AM - 3:00 PM

Any outdoor entertainment? If so, please explain: Athletic Fields

Any outdoor storage? If so, please explain: None

Zoning district of the property: I-1 Institutional

(g) Attach a plat of survey prepared by a professional land surveyor or site plan drawn to scale and approved by the Kenosha County Department of Planning & Development showing all of the information required under section 12.05-1(h)3 for a zoning permit. In addition, the plat of survey or site plan layout shall show the location, elevation and use of any abutting lands and the location and foundation elevations of structures within 50 feet of the subject site; soil mapping unit lines; ordinary high water mark, historic high water marks and floodlands on or within 50 feet of the subject premises, existing and proposed gravel, asphalt and concrete surfaces, existing and proposed signage, existing and proposed buildings, and existing and proposed landscaping.

CONDITIONAL USE PERMIT APPLICATION

For conditional use permit applications that are made within shoreland and floodland areas, such description shall also include information that is necessary for the Village Plan Commission to determine whether the proposed development will hamper flood flows, impair floodplain storage capacity, or cause danger to human, animal or aquatic life. This additional information may include plans, certified by a registered professional engineer or professional surveyor, showing existing and proposed elevations or contours of the ground; fill or storage elevation; basement and first floor elevations of structures; size, location, and spatial arrangement of all existing and proposed structures on the site; location and elevation of streets water supply and sanitary facilities; aerial photographs, and photographs showing existing surrounding land uses and vegetation upstream and downstream; soil types and any other pertinent information required by either the Village of Somers Planning & Development office, Village Plan Commission or Village Board:

(h) The Kenosha County Department of Planning & Development may ask for additional information.

(i) The fee specified in Section 12.05-8 of this ordinance.

Request for Conditional Use Permit (payable to "Kenosha County") ..... \$1,350.00

(For other fees see the [Fee Schedule](#))

ATTACH BUSINESS  
SUMMARY HERE

ATTACH TO-SCALE MAP OF  
PROPERTY SHOWING SITE  
PLAN OF OPERATION HERE



August 30, 2024

## Site Plan Review & CUP Application - Project Narrative

Project: Shoreland Lutheran Addition – Phase 1  
9026 12<sup>th</sup> Street  
Kenosha, WI 53144

Shoreland Lutheran High School Federation, Inc. is requesting a site plan review and conditional use permit approval for building and parking lot additions to the existing school campus located at 9026 12<sup>th</sup> Street in the Village of Somers. The property is zoned I-1 (Institutional) and the underlying private school use is considered permitted under the I-1 zoning district. However, the building additions in this proposed phase will include new classrooms in conjunction with a new gymnasium/fitness center and a remodeling of the existing gymnasium. In the I-1 zoning district gymnasiums are considered conditional uses. Therefore, a conditional use permit is required.

As a part of the parking lot addition scope of work, due to the anticipated additional traffic on the eastern side of the site, a traffic impact analysis (TIA) is being completed. Based on the TIA recommendations the existing drive onto 88<sup>th</sup> Avenue could be demolished, and a new drive entrance would be constructed onto 88<sup>th</sup> Avenue.

Stormwater for the overall school campus has been designed and is currently in the review process with the Village. The stormwater will be constructed concurrently with the building addition. The existing well onsite will be used to supply water to the proposed addition. A new sanitary lateral will be installed out of the addition. The sanitary lateral will connect into the existing private interceptor main.

As a part of the conditional use permit for the proposed gymnasium, the development is requesting to proceed with the following exceptions from the Village of Somers Zoning Ordinance:

### Section ZN3.06(3)(j) – Parking Requirements

- Non-Residential Use Pavement Front Yard Setback
  - The Traffic, Parking, and Access section of the City Zoning Ordinance requires that all non-residential parking spaces and driveways to be a minimum of 20 feet from the established right-of-way. However, due to the size constraints of the existing institutional facility, an attempt to match the existing parking lot setback is proposed for the additional parking area in the front of the site at approximately 7 feet from the right-of-way line (encroaching approximately 13 feet into the required pavement setback area; see attached site plan indicating this setback area proposed). This exception is required to provide adequate and safe vehicle and pedestrian circulation areas in front of the existing building at the main entrance of the facility. When the existing parking lot was constructed the parking lot met setback requirements. Due to changes in setback requirements and a 10' strip of the shoreland property being given to the county to expand the ROW of

12<sup>th</sup> Street the existing parking does not comply with current setbacks. The proposed parking expansion at the front of the existing facility cannot be shifted to meet this pavement setback as it would impact the number of parking stall spaces for the proposed building expansion as well as impact the onsite maneuver area for traffic circulation through the front of the existing site.

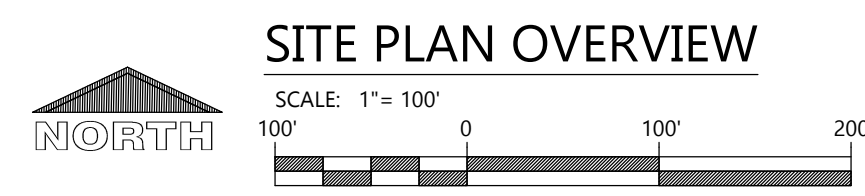
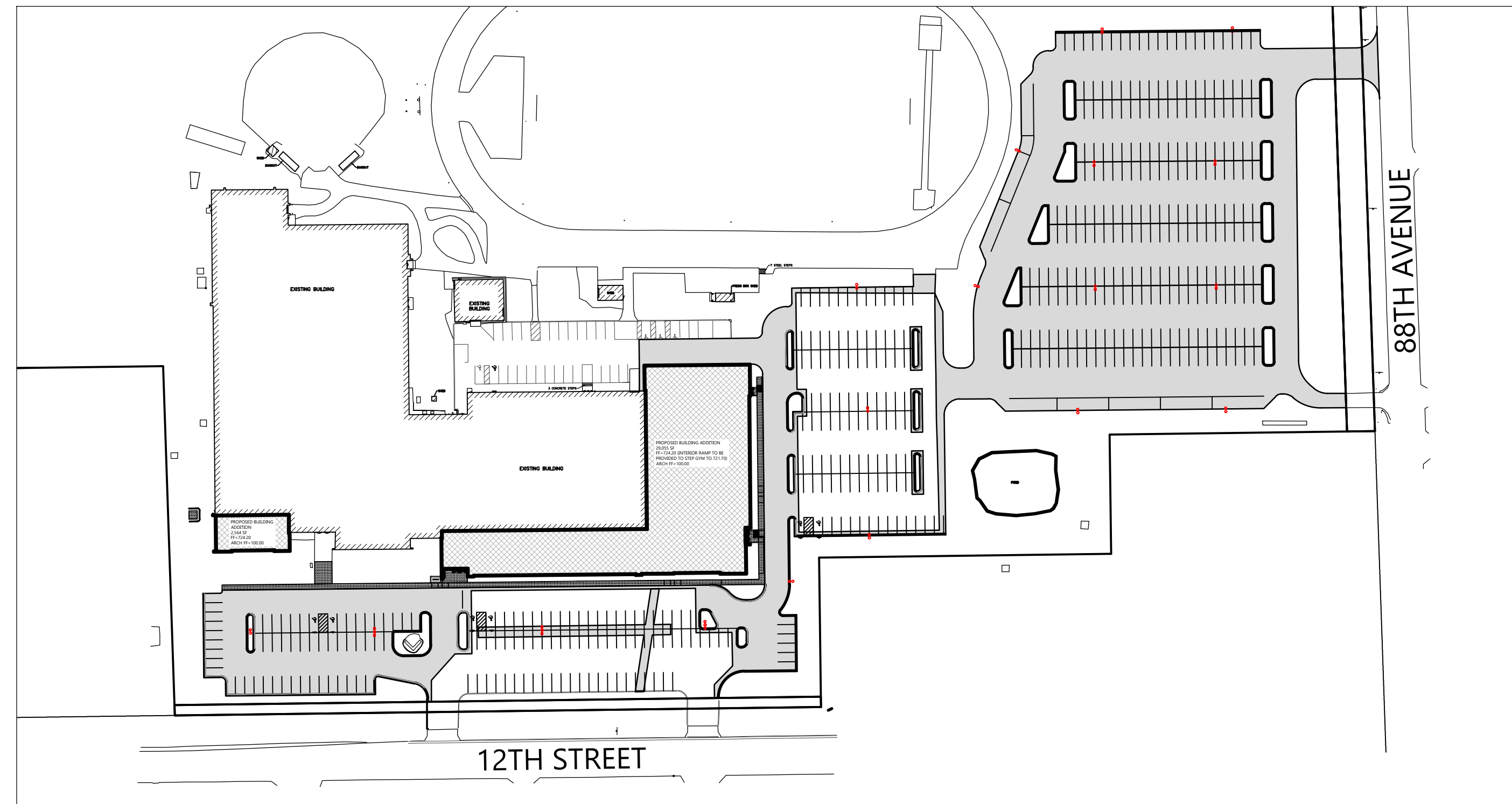
# PROPOSED BUILDING ADDITION PHASE ONE FOR: SHORELAND LUTHERAN HIGH SCHOOL

## KENOSHA, WI

### PROJECT INFORMATION

SITE INFORMATION:			
PROPERTY AREA: LOT 1 OF PROPOSED CSM	2,427,904 S.F. (55.74 ACRES)		
EXISTING ZONING: I-1 (INSTITUTIONAL)			
PROPOSED ZONING: I-1 (INSTITUTIONAL)			
PROPOSED USE: PHASE 1 - EXISTING SCHOOL BLDG. ADDITION & PARKING LOT EXPANSION			
AREA OF SITE DISTURBANCE: 5.40 ACRES			
EXISTING SITE DATA			
	AREA (AC)	AREA (SF)	RATIO
BUILDING FLOOR AREA	2.14	93,153	3.8%
PAVEMENT (ASP., CONC. & GRAVEL)	4.77	207,664	8.6%
TOTAL IMPERVIOUS	6.91	300,817	12.4%
LANDSCAPE/ OPEN SPACE	48.83	2,127,087	87.6%
PROJECT SITE	55.74	2,427,904	100.0%
PROPOSED SITE DATA			
	AREA (AC)	AREA (SF)	RATIO
BUILDING FLOOR AREA	2.80	122,121	5.0%
PAVEMENT (ASP., CONC. & GRAVEL)	6.70	291,777	12.0%
TOTAL IMPERVIOUS	9.50	413,898	17.0%
LANDSCAPE/ OPEN SPACE	46.24	2,014,006	83.0%
PROJECT SITE	55.74	2,427,904	100.0%

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN  
CALL DIGGERS HOTLINE  
1-800-242-8511  
TOLL FREE TELEFAX (414) 259-0947  
TDD (FOR THE HEARING IMPAIRED)  
1-800-542-2289  
WISCONSIN STATUTE 182.0175 (1974)  
REQUIRES MINIMUM OF 3 WORK DAYS  
NOTICE BEFORE YOU EXCAVATE



### LEGEND

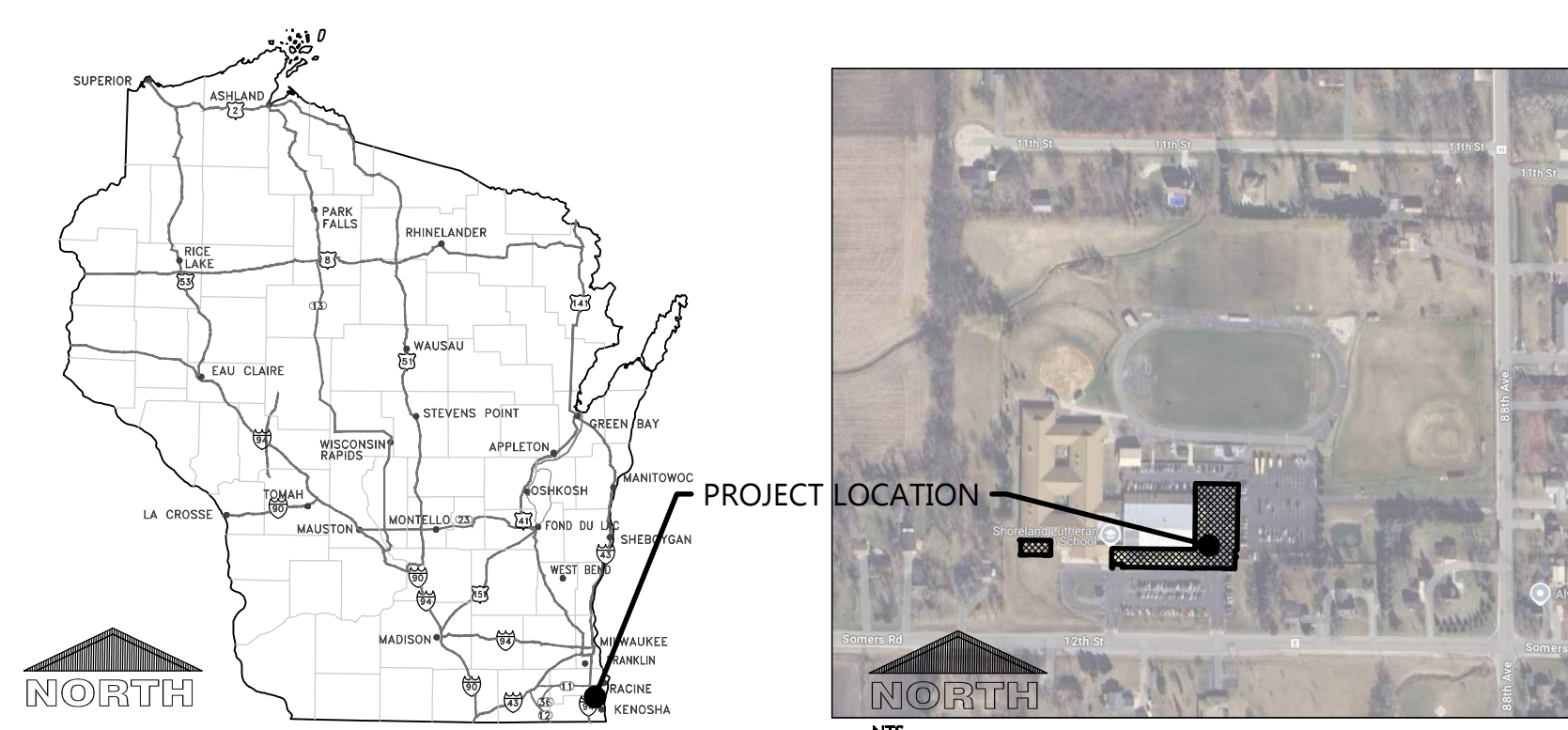
NOTE: ALL SYMBOLS SHOWN MAY NOT APPEAR ON DRAWINGS.

SYM.	IDENTIFICATION	SYM.	IDENTIFICATION
0000.00	PROPOSED SPOT ELEVATIONS (FLOW LINE OF CURB UNLESS OTHERWISE SPECIFIED)	0000.00C	PROPOSED SPOT ELEVATIONS (TOP OF CURB, FLOWLINE OF CURB)
0000.00G	EXISTING GRADE SPOT ELEVATIONS	0000.00FL	PROPOSED SPOT ELEVATIONS (TOP OF WALK, BOTTOM OF WALK @ FLOWLINE)
0000.00BG	PROPOSED SPOT ELEVATIONS (REFERENCE R-WALL DETAIL) BG-FINISHED SURFACE GRADE AT BACK OF WALL	0000.00TW	PROPOSED SPOT ELEVATIONS (TOP OF WALK, BOTTOM OF WALK @ FLOWLINE)
0000.00FG	PROPOSED SPOT ELEVATIONS (REFERENCE R-WALL DETAIL) FG-FINISHED SURFACE GRADE AT FRONT OF WALL	0000.00BW	PROPOSED SPOT ELEVATIONS (TOP OF WALK, BOTTOM OF WALK @ FLOWLINE)
EXISTING SITE SYMBOLS			
EXISTING SIGN	EXISTING UTILITY POLE	EXISTING HANDICAP PARKING STALL	EXISTING UTILITY POLE WITH GUY WIRE
EXISTING WATER VALVE IN BOX	EXISTING STREET LIGHT	EXISTING WATER VALVE IN MANHOLE	EXISTING TELEPHONE PEDESTAL
EXISTING WATER SERVICE VALVE	EXISTING ELECTRIC PEDESTAL	EXISTING WELL	EXISTING ELECTRIC BOX
EXISTING STORM CATCH BASIN	EXISTING FLOOD LIGHT	EXISTING STORM CURB INLET	EXISTING TELEPHONE MANHOLE
EXISTING SQUARE CATCH BASIN	EXISTING CABLE TV PEDESTAL	EXISTING LIGHT POLE	EXISTING GAS VALVE
1-1/4" REBAR SET WEIGHING 4.30 LB/FT.	EXISTING HEDGE	3/4" REBAR SET WEIGHING 1.50 LB/FT.	EXISTING WOODED AREA
1-1/4" REBAR FOUND	EXISTING MARSH AREA	3/4" REBAR FOUND	EXISTING DECIDUOUS TREE WITH TRUNK DIAMETER
2" IRON PIPE FOUND	EXISTING CONIFEROUS TREE	1" IRON PIPE FOUND	EXISTING SHRUB
SECTION CORNER	EXISTING STUMP	PROPOSED SIGN	PROPOSED STORM FIELD INLET - ST FI
PROPOSED SIGN	PROPOSED LIGHT POLE	PROPOSED HANDICAP PARKING STALL	PROPOSED DRAINAGE FLOW
PROPOSED WATER VALVE IN BOX	PROPOSED APRON END SECTION	PROPOSED WATER VALVE IN MANHOLE	SOIL BORING
PROPOSED WATER SERVICE VALVE	CENTER LINE	PROPOSED WELL	PROPOSED CLEANOUT
PROPOSED STORM CATCH BASIN - ST CB	PROPOSED DOWNSPOUT TO GRADE	PROPOSED STORM CURB INLET - ST CI	PROPOSED DOWNSPOUT TO RISER
EXISTING LINETYPES			
EXISTING CHAINLINK FENCE	POL - EXISTING POLISH SEWER AND MANHOLE	EXISTING WOOD FENCE	P - EXISTING PROCESS SEWER AND MANHOLE
EXISTING BARBED WIRE FENCE	CLW - EXISTING CLEAR WATER LINE	EXISTING CURB AND GUTTER	FO - EXISTING UNDERGROUND FIBER OPTIC LINE
EXISTING GUARD RAIL	E - EXISTING UNDERGROUND ELECTRIC CABLE	EXISTING GROUND CONTOUR	T - EXISTING UNDERGROUND TELEPHONE CABLE
EXISTING STORM SEWER AND MANHOLE	G - EXISTING UNDERGROUND GAS LINE	EXISTING SANITARY SEWER AND MANHOLE	OU - EXISTING OVERHEAD UTILITY LINE
EXISTING WATER LINE AND HYDRANT	RAILROAD TRACKS	INTERIOR PROPERTY LINE	RIGHT-OF-WAY LINE
PROPOSED LINETYPES			
PROPOSED CHAINLINK FENCE	POL - PROPOSED POLISH SEWER AND MANHOLE	PROPOSED WOOD FENCE	P - PROPOSED PROCESS SEWER AND MANHOLE
PROPOSED BARBED WIRE FENCE	CLW - PROPOSED CLEAR WATER LINE	PROPOSED CURB AND GUTTER	FO - PROPOSED UNDERGROUND FIBER OPTIC LINE
PROPOSED GUARD RAIL	E - PROPOSED UNDERGROUND ELECTRIC CABLE	PROPOSED GROUND CONTOUR	T - PROPOSED UNDERGROUND TELEPHONE CABLE
PROPOSED STORM SEWER AND MANHOLE - ST MH	G - PROPOSED UNDERGROUND GAS LINE	PROPOSED SANITARY SEWER AND MANHOLE - SAN MH	OU - PROPOSED OVERHEAD UTILITY LINE
PROPOSED WATER LINE AND HYDRANT	MATCHLINE	PROPOSED PROPERTY LINE	GRADING/SEEDING LIMITS

### PROJECT CONTACTS

<b>OWNER INFORMATION</b> SHORELAND LUTHERAN HIGH SCHOOL ATTN: PAUL SCRIVER 9026 12TH STREET KENOSHA, WI 53144 Phone: (262) 515-4527 Email: scriver@slhs.us	<b>CIVIL</b> BEN WARNTJES Phone: (920) 322-1575 E-mail: ben.warntjes@excelengineer.com	<b>VILLAGE ADMINISTRATOR</b> JASON PETERS Phone: (262) 859-2822 E-mail: jpeters@somers.org	<b>CITY ENGINEER</b> BAXTER & WOODMAN CONSULTING ENGINEERS DOUG SNYDER Phone: (815) 444-3349 E-mail: dsnyder@baxterwoodman.com	<b>CITY FIRE CHIEF</b> BEN ANDERSEN Phone: (262) 859-2277 E-mail: bandersen@somers.org	<b>CITY BUILDING INSPECTOR</b> SCOTT SEYMOUR Phone: (262) 859-2822 E-mail: sseymour@somers.org	<b>CITY DIRECTOR OF PUBLIC WORKS</b> ANDREW KREYE Phone: (262) 859-2822 E-mail: akreye@somers.org
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### LOCATION MAP



### PROJECT NOTES

#### GENERAL PROJECT NOTES

- ALL DRIVEWAYS AND CURB CUTS TO BE CONSTRUCTED ACCORDING TO LOCAL ORDINANCES. CONTRACTOR TO OBTAIN ALL NECESSARY PERMITS.
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL WORK IN ROW PERMITS.

#### CONSTRUCTION STAKING SERVICES

CONSTRUCTION STAKING SHALL BE COMPLETED BY EXCEL ENGINEERING AS REQUESTED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. CONTRACTOR TO CONTACT RYAN WILGREEN AT 920-926-9800 OR RYAN.WILGREEN@EXCELENGINEER.COM TO GET STAKING PRICES TO INCLUDE IN BID TO OWNER. PAYMENT OF STAKING COSTS ABOVE AND BEYOND THE BASE PRICE DUE TO RESTAKING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR, NOT THE OWNER. CAD DRAWING FILES AND SURVEY CONTROL WILL NOT BE PROVIDED FOR STAKING PURPOSES.

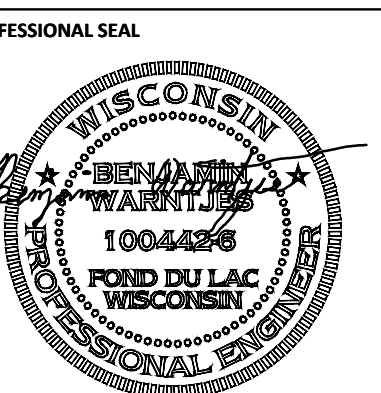
### SHEET INDEX

SHEETS BELOW INTENDED TO BE PRINTED IN COLOR. REFER TO DIGITAL FORMAT DRAWINGS IF PRINTED GRAYSACLE TO ENSURE SCOPE CLARITY.

NUMBER	SHEET NAME / DESCRIPTION
C0.1	CIVIL COVER SHEET
C0.2	CIVIL SPECIFICATIONS
C1.0	EXISTING SITE AND DEMOLITION PLAN
C1.1	SITE PLAN
C1.2	GRADING AND EROSION CONTROL PLAN
C1.3	UTILITY PLAN
C2.0	DETAILS
C3.1	SITE PHOTOMETRIC PLAN & DETAILS
L1.0	OVERALL LANDSCAPE PLAN
L1.1	ENLARGED LANDSCAPE PLAN
L1.2	ENLARGED LANDSCAPE PLAN
L1.3	LANDSCAPE DETAILS, NOTES & SCHEDULES

#### PROJECT INFORMATION

PROPOSED BUILDING ADDITION PHASE ONE FOR:  
**SHORELAND LUTHERAN HIGH SCHOOL**  
9026 12TH STREET • KENOSHA, WI 53144



#### PRELIMINARY DATES

AUG. 30, 2024

NOT FOR CONSTRUCTION

#### JOB NUMBER

240018700

#### SHEET NUMBER

**C0.1**

# CIVIL SPECIFICATIONS

## DIVISION 31 EARTH WORK

### 31 10 00 SITE CLEARING (DEMOLITION)

- CONTRACTOR SHALL CALL DIGGERS HOT LINE AND CONDUCT A PRIVATE UTILITY LOCATE AS REQUIRED TO ENSURE THAT ALL UTILITIES HAVE BEEN LOCATED BEFORE STARTING SITE DEMOLITION. DESIGN ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCIES BETWEEN PLANS AND FIELD CONDITIONS PRIOR TO CONSTRUCTION.
- CONTRACTOR TO FIELD TELEVISION EXISTING SANITARY AND STORM LATERALS THAT ARE SCHEDULED TO BE RE-USED AND/OR CONNECTED TO ON SITE AT THE TIME OF DEMOLITION. THE TELEVISION SHALL BE COMPLETED TO ENSURE THE EXISTING LATERALS ARE FREE OF OBSTRUCTIONS AND IN SOUND STRUCTURAL CONDITION. TELEVISION OF THESE LATERALS WILL BE COMPLETED AT BEGINNING OF CONSTRUCTION AND DESIGN ENGINEER SHALL BE NOTIFIED OF ANY PIPE OBSTRUCTIONS AND/OR STRUCTURAL DEFICIENCIES IMMEDIATELY AFTER COMPLETION OF FIELD TELEVISION.
- DEMOLITION PLAN IS AN OVERVIEW OF DEMOLITION TO TAKE PLACE ON SITE. CONTRACTOR TO FIELD VERIFY EXISTING SITE CONDITIONS PRIOR TO BEGINNING. CONTRACTOR SHALL REMOVE, REPAIR OR DEMOLISH ALL ITEMS AS NEEDED DURING CONSTRUCTION.
- CONTRACTOR TO PROTECT EXISTING IMPROVEMENTS THAT ARE SCHEDULED TO REMAIN. ANY DAMAGE TO EXISTING FACILITIES SHALL BE REPAIRED AT CONTRACTORS EXPENSE.
- ALL CONCERTE NOTED TO BE REMOVED SHALL BE REMOVED TO THE NEAREST CONTROL POINT.

### 31 20 00 EARTH MOVING

- CONTRACTOR SHALL CALL DIGGERS HOT LINE AND CONDUCT A PRIVATE UTILITY LOCATE AS REQUIRED TO ENSURE THAT ALL UTILITIES HAVE BEEN LOCATED BEFORE STARTING EXCAVATION. DESIGN ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCIES BETWEEN PLANS AND FIELD CONDITIONS PRIOR TO CONSTRUCTION.
- PROVIDE LABOR, MATERIALS AND EQUIPMENT FOR ALL EXCAVATION, GRADING, FILL AND BACKFILL WORK AS REQUIRED TO COMPLETE THE GENERAL CONSTRUCTION WORK. ALL EXCAVATION AND BACKFILL FOR ELECTRICALS AND MECHANICALS ARE THE RESPONSIBILITY OF THE RESPECTIVE CONTRACTOR UNLESS OTHERWISE SPECIFIED IN THE BID DOCUMENTS.
- ALL ORGANIC TOPSOIL INSIDE THE BUILDING AREA, UNDER PAVED AREAS, AND AT SITE FILL AREAS SHALL BE REMOVED. PROOF ROLL SUBGRADES BEFORE PLACING FILL WITH HEAVY PNEUMATIC TIRED EQUIPMENT, SUCH AS A FULLY LOADED TANDEM AXLE DUMP TRUCK, TO IDENTIFY SOFT SPOTS AND AREAS OF EXCESS YIELDING. CONTRACTOR SHALL VERIFY TOPSOIL DEPTHS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REVIEW AND FOLLOW THE RECOMMENDATIONS OF THE GEOTECHNICAL REPORT AND ACCORDING TO EXISTING CONDITIONS PRIOR TO SUBMITTING BID FOR THE PROJECT. EXCESS MATERIALS SHALL BE REMOVED FROM THE SITE UNLESS OTHERWISE DIRECTED IN THE PLANS OR BY LOCAL ZONING REQUIREMENTS.
- PLACE AND COMPACT FILL MATERIAL IN LAYERS TO REQUIRED ELEVATIONS. UNIFORMLY MOISTEN OR REWET SUBGRADE AND EACH SUBSEQUENT FILL OR BACKFILL LAYER BEFORE COMPACTATION AS RECOMMENDED TO ACHIEVE SPECIFIED DRY DENSITY. REMOVE AND REPLACE, OR SCARIFY AND AIR DRY, OTHERWISE SATISFACTORY SOIL MATERIAL THAT IS TOO WET TO COMPACT TO SPECIFIED DRY DENSITY.
- PLACE BACKFILL AND FILL MATERIALS IN LAYERS NOT MORE THAN 8" IN LOOSE DEPTH FOR MATERIAL COMPACTED BY HEAVY COMPACTION EQUIPMENT, AND NOT MORE THAN 4" IN LOOSE DEPTH FOR MATERIAL COMPACTED BY HAND-OPERATED TAMPIERS.
- COMPACT THE SOIL TO NOT LESS THAN THE FOLLOWING PERCENTAGES OF MAXIMUM DRY DENSITY ACCORDING TO ASTM D 998, STANDARD PROCTOR TEST. FILL MAY NOT BE PLACED ON FROZEN GROUND AND NO FROZEN MATERIALS MAY BE USED FOR BACK FILL. APPLY THE MORE STRINGENT REQUIREMENTS WHEN COMPARING BETWEEN THE FOLLOWING AND THE GEOTECHNICAL REPORT.
  - UNDER FOUNDATIONS, SUBGRADE, AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 98 PERCENT.
  - UNDER INTERIOR SLAB-ON-GRADE WHERE GROUNDWATER IS MORE THAN 3 FEET BELOW THE SLAB, PLACE A DRAINAGE COURSE LAYER OF 3/4" CRUSHED STONE WITH 5% TO 12% FINES PER THICKNESS INDICATED ON FOUNDATION PLANS ON PREPARED SUBGRADE. COMPACT SUBGRADE AND DRAINAGE COURSE TO NOT LESS THAN 95 PERCENT.
  - UNDER INTERIOR SLAB-ON-GRADE WHERE GROUNDWATER IS WITHIN 3 FEET OF THE SLAB SURFACE, PLACE A DRAINAGE COURSE LAYER OF CLEAN 3/4" CRUSHED STONE WITH NO MORE THAN 5% FINES PER THICKNESS INDICATED ON FOUNDATION PLANS ON PREPARED SUBGRADE. COMPACT THE SUBGRADE AND DRAINAGE COURSE TO NOT LESS THAN 95 PERCENT.
  - UNDER EXTERIOR CONCRETE AND ASPHALT PAVEMENTS - COMPACT THE SUBGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 95 PERCENT.
  - UNDER WALKWAYS - COMPACT SUBGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 95 PERCENT.
  - UNDER LAWNS OR UNPAVED AREAS - COMPACT SUBGRADE AND EACH LAYER OF BACKFILL OR FILL MATERIAL TO NOT LESS THAN 85 PERCENT.
- CONTRACTOR SHALL ENGAGE A QUALIFIED INDEPENDENT TESTING AND INSPECTING AGENCY TO PERFORM FIELD TESTS AND INSPECTIONS. CONTRACTOR SHALL PROVIDE DOCUMENTATION OF PASSING DENSITY TESTING AND PROOF-ROLLING TO ENGINEER UPON COMPLETION. IT IS SUGGESTED THAT THE GEOTECHNICAL FIRM USED TO PERFORM THE SUBGRADE SOIL INVESTIGATION BE ENGAGED FOR THE FIELD QUALITY CONTROL TESTS. THE GEOTECHNICAL REPORT WAS PERFORMED BY NAME OF GEOTECHNICAL FIRM.
  - ALLOW THE TESTING AGENCY TO TEST AND INSPECT SUBGRADES AND EACH FILL OR BACKFILL LAYER. PROCEED WITH SUBSEQUENT EARTHWORK ONLY AFTER TEST RESULTS AND PREVIOUSLY COMPLETED WORK COMPLY WITH REQUIREMENTS. PROVIDE ONE TEST FOR EVERY 2000 SQUARE FEET OF PAVED AREA OR BUILDING SLAB. ONE TEST FOR EACH SPREAD FOOTING, AND ONE TEST FOR EVERY 50 LINEAR FEET OF WALL STRIP FOOTING.
    - WHEN THE TESTING AGENCY REPORTS THAT SUBGRADES, FILLS, OR BACKFILLS HAVE NOT ACHIEVED DEGREE OF COMPACTION SPECIFIED, SCARIFY AND MOISTEN OR REWET, OR REMOVE AND REPLACE SOIL TO DEPTH REQUIRED; RECOMPACT AND RETEST UNTIL SPECIFIED COMPACTION IS OBTAINED.
    - IF THE BUILDING SITE SHALL BE GRADED TO PROVIDE DRAINAGE AWAY FROM THE BUILDING AS INDICATED ON THE PLANS, SITE EARTHWORK SHALL BE GRADED TO WITHIN 0.10' OF REQUIRED EARTHWORK ELEVATIONS ASSUMING POSITIVE DRAINAGE IS MAINTAINED IN ACCORDANCE WITH THE GRADING PLAN.

### 31 30 00 EROSION CONTROL/STORMWATER MANAGEMENT

- THE EXCEL ENGINEERING DESIGN ENGINEER SHALL PREPARE A SITE SPECIFIC EROSION CONTROL AND A STORMWATER MANAGEMENT PLAN PURSUANT TO NR 216.45 AND NR 216.47. THE DESIGN ENGINEER SHALL ALSO FILE A CONSTRUCTION NOTICE OF INTENT WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES PURSUANT TO NR 216.45 AND NR 216.47. THE DESIGN ENGINEER SHALL ALSO FILE A CONSTRUCTION NOTICE OF INTENT WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES PURSUANT TO NR 216.45 TO OBTAIN COVERAGE UNDER THE GENERAL WIPDES STORM WATER PERMIT.
  - THE CONTRACTOR SHALL KEEP THE NOTICE OF INTENT PERMIT, APPROVED EROSION CONTROL AND STORMWATER MANAGEMENT PLANS, AND PLAN AMENDMENTS ON THE CONSTRUCTION SITE AT ALL TIMES PURSUANT TO NR 216.45 UNTIL PERMIT COVERAGE IS TERMINATED.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL LOCAL EROSION CONTROL PERMITS.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEETING THE MONITORING, MAINTENANCE, AND REPORTING REQUIREMENTS OF NR 216.48. INSPECTIONS OF IMPLEMENTED EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES MUST AT A MINIMUM BE INSPECTED EVERY 7 CALENDAR DAYS OR EVERY 14 DAYS AND WITHIN 24 HOURS OF THE END OF A RAIN EVENT OF 0.5" OR MORE. A RAIN EVENT MAY BE CONSIDERED TO BE THE TOTAL AMOUNT OF RAINFALL RECORDED IN ANY CONTINUOUS 24-HOUR PERIOD. THE CONTRACTOR SHALL REPAIR OR REPLACE EROSION AND SEDIMENT CONTROL AS NECESSARY WITHIN 24 HOURS OF AN INSPECTION OR AFTER A DEPARTMENT NOTIFICATION WHERE REPAIR OR REPLACEMENT IS REQUIRED.
  - THE CONTRACTOR SHALL MAINTAIN, AT THE CONSTRUCTION SITE OR AVAILABLE VIA AN INTERNET WHERE, WEEKLY WRITTEN REPORTS OF ALL INSPECTIONS CONDUCTED. WISCONSIN DNR CONSTRUCTION SITE INSPECTION REPORT FORM 1400-187 SHALL BE USED. WEEKLY INSPECTION REPORTS SHALL INCLUDE ALL OF THE FOLLOWING:
    - THE DATE, TIME, AND LOCATION OF THE CONSTRUCTION SITE INSPECTION.
    - THE NAME OF THE INDIVIDUAL WHO PERFORMED THE INSPECTION.
    - AN ASSESSMENT OF THE CONDITION OF THE EROSION AND SEDIMENT CONTROL.
    - A DESCRIPTION OF ANY EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICE IMPLEMENTATION AND MAINTENANCE PERFORMED.
    - A DESCRIPTION OF THE PRESENT PHASE OF LAND DISTURBING CONSTRUCTION ACTIVITY AT THE CONSTRUCTION SITE.
- EROSION AND SEDIMENT CONTROL, IMPLEMENTED DURING CONSTRUCTION SHALL STRICTLY COMPLY WITH THE GUIDELINES AND REQUIREMENTS SET FORTH IN WISCONSIN ADMINISTRATIVE CODE (WAC) NR 151. THE STATE OF WISCONSIN DEPARTMENT OF NATURAL RESOURCES RUNOFF MANAGEMENT PERFORMANCE STANDARDS. TECHNICAL STANDARDS PUBLISHED BY THE WISCONSIN DNR SHALL ALSO BE UTILIZED TO IMPLEMENT THE REQUIRED PERFORMANCE STANDARDS. THE METHODS AND TYPES OF EROSION CONTROL WILL BE DIFFERENT ON THE LOCATION AND TYPE OF WORK INVOLVED. ALL SEDIMENT CONTROL MEASURES SHALL BE ADJUSTED TO MEET FIELD CONDITIONS AT THE TIME OF CONSTRUCTION, AND INSTALLED PRIOR TO ANY LOCAL EROSION CONTROL COMPACTED TO AN AVERAGE DENSITY PER WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
  - DESIGN AND CONSTRUCTION OF ALL CAST-IN-PLACE EXTERIOR CONCRETE FLAT WORK SHALL CONFORM TO AC 308-0R & AC 318-0R.
  - ALL EXTERIOR CONCRETE FLAT WORK SUBJECT TO PROTECTION TO BE PROVIDED PER MOST STRINGENT REQUIREMENTS OF THE GEOTECHNICAL REPORT OR THIS SPECIFICATION. CONCRETE FLAT WORK CONSTRUCTIONS IS AS FOLLOWS:
    - SEMI-MASS CONCRETE - 4" OF CONCRETE OVER 4" OF 3/4" CRUSHED AGGREGATE BASE.
    - CONCRECTION JOINTS SHALL CONSIST OF 1/8" WIDE BY 1" DEEP TEGGLED JOINT WHERE APPLICABLE.
- DESIGN MIXES SHALL BE IN ACCORDANCE WITH ASTM C94
  - STRENGTH TO BE MINIMUM OF 4000 PSI AT 28 DAYS FOR EXTERIOR CONCRETE.
  - MAXIMUM ALLOWED FINISH SHALL BE 1/2" TO 3/4" TO 1/2" INCH CLEAR OR WASHED STONE AND SHALL BE PLACED IN LAYERS AT LEAST 12 INCHES THICK. THE STONE SHALL BE WASHED AND CLEAN. THE STONE TRACING FABRIC AS NEEDED.
  - TRACING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT (12" MIN WIDTH) AND SHALL BE A MINIMUM OF 50 FEET LONG. SURFACE WATER SHALL BE PREVENTED FROM PASSING THROUGH THE TRACING PAD. OTHER TRACKOUT CONTROL PRACTICES INCLUDING STABILIZED WORK SURFACES, MANUFACTURED TRACKOUT CONTROL DEVICES, TIRE WASHING, AND STREET PAVEMENT CLEANING SHALL BE IMPLEMENTED AS NECESSARY TO MITIGATE THE TRACKOUT OF SEDIMENT OFFSITE. FOLLOW PROCEDURES FOUND IN WISCONSIN DNR TECHNICAL STANDARD 1062 (CURRENT EDITION).
  - STORM DRAIN INLET PROTECTION SHALL BE PROVIDED FOR ALL NEW AND DOWNSTREAM STORM CATCH BASINS AND CURB INLETS. TYPE 8 OR C PROTECTION SHOULD BE PROVIDED AND SHALL BE IN CONFORMANCE WITH WISCONSIN DNR TECHNICAL STANDARD 1068 (CURRENT EDITION).
- ALL CONCRETE FLAT WORK SURFACES AND CONCRETE CURB FINISHES SHALL BE 0.75 INCHES FINISH.
  - CONCRETE FLAT WORK SHALL HAVE CONSTRUCTION JOINTS OR SAW CUT JOINTS PLACED AS INDICATED ON THE PLANS OR PER THIS SPECIFICATION. SAW CUTS SHALL BE DONE AS SOON AS POSSIBLE, BUT NO LATER THAN 24 HOURS AFTER CONCRETE IS PLACED. CONCRETE CURB AND GUTTER JOINTING SHALL BE PLACED EVERY 10' OR CLOSER IF MIN. IF CONCRETE PAVEMENT IS ADJACENT TO CONCRETE CURB, JOINTING IN THE PAVEMENT AND CURB SHALL ALIGN. ALL EXTERIOR CONCRETE SHALL HAVE A BROWN FINISH UNLESS NOTED OTHERWISE. A UNIFORM COAT OF A HIGH SOLIDS CURING COMPOUND MEETING ASTM C693 SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES. ALL CONCRETE IS TO BE CURED FOR 7 DAYS. EXTERIOR CONCRETE SHALL BE SEPARATED FROM BUILDING WITH CONTINUOUS 0.5 INCH FIBER EXPANSION JOINT AND 0.25 INCH FIBER EXPANSION JOINT AT DECORATIVE MASONRY UNITS.
  - ALL REINFORCING BARS SHALL BE ASTM A618 GRADE 60. THICKNESS OF CONCRETE COVER OVER REINFORCEMENT SHALL BE NOT LESS THAN 3" WHERE CONCRETE IS DEPOSITED AGAINST THE GROUND WITHOUT THE USE OF FORMS AND NOT LESS THAN 1.5" FOR UP TO #5 BARS AND 2" FOR #6 TO #10 BARS IN ALL OTHER LOCATIONS. ALL REINFORCING SHALL BE LAPPED 48 DIAMETERS FOR UP TO #6 BARS, 62 DIAMETERS FOR #7 TO #9 BARS, 68 DIAMETERS FOR #10 BARS OR AS NOTED ON THE DRAWINGS AND EXTENDED AROUND CORNERS WITH CORNER BARS. PLACING AND DETAILING OF STEEL REINFORCING AND REINFORCING SUPPORTS SHALL BE IN ACCORDANCE WITH CRSI AND ACT MANUAL AND STANDARD PRACTICES. THE REINFORCEMENT SHALL NOT BE PAINTED AND MUST BE FREE OF GREASE, OIL, DIRT OR DEEP TRIP WHEN PLACED IN THE WORK. ALL WELDED WIRE FABRIC SHALL MEET THE REQUIREMENTS OF ASTM A 1064. WELDED WIRE FABRIC SHALL BE PLACED 2" FROM TOP OF SLAB UNLESS INDICATED OTHERWISE.
  - CONTRACTOR SHALL ENGAGE A QUALIFIED INDEPENDENT TESTING AND INSPECTING AGENCY TO SAMPLE MATERIALS, PERFORM TESTS, AND SUBMIT TEST REPORTS DURING CONCRETE PLACEMENT. TESTS WILL BE PERFORMED ACCORDING TO ACT 301. CAST AND LABORATORY CURS ONE SET OF FOUR STANDARD CYLINDERS FOR EACH COMPOSITE SAMPLE FOR EACH DAY'S POUR OF EACH CONCRETE MIX EXCEEDING 5 CU YD, BUT LESS THAN 25 CU YD, PLUS ONE SET FOR EACH ADDITIONAL 50 CU YD OR FRACTION THEREOF. PERFORM COMPRESSIVE STRENGTH TESTS ACCORDING TO ASTM C 39. TEST TWO SPECIMENS AT 7 DAYS AND TWO SPECIMENS AT 28 DAYS. PERFORM SLUMP TESTING ACCORDING TO ASTM C 143. PROVIDE ONE TEST AT POINT OF PLACEMENT FOR EACH COMPOSITE SAMPLE, BUT NOT LESS THAN ONE TEST FOR EACH DAY'S POUR OF EACH CONCRETE MIX. PERFORM ADDITIONAL TESTS WHEN CONCRETE CONSISTENCY APPEARS TO CHANGE.
    - PROTECT FRESHLY PLACED CONCRETE FROM PREMATURE DRYING AND EXCESSIVE COLD OR HOT TEMPERATURES. IN HOT, DRY, AND WINDY WEATHER, APPLY AN EVAPORATION-CONTROL, COMPOUND ACCORDING TO MANUFACTURER'S INSTRUCTIONS AFTER SCREEDING AND BULL FLOATING, BUT BEFORE POWER FLOATING AND TROWELLING.
    - LIMIT MAXIMUM WATER-CEMENTitious RATIO OF CONCRETE EXPOSED TO FREEZING, THAWING AND DEICING SALTS TO 0.45.
    - TEST RESULTS WILL BE REPORTED IN WRITING TO THE DESIGN ENGINEER, READY MIX PRODUCER, AND CONTRACTOR WITHIN 56 HOURS AFTER TESTS. REPORTS OF COMPRESSIVE STRENGTH TESTS SHALL CONTAIN THE PROJECT IDENTIFICATION NAME AND NUMBER, DATE OF CONCRETE PLACEMENT, NAME OF CONCRETE TESTING SERVICE, CONCRETE TYPE AND CLASS, LOCATION OF CONCRETE BATCH ON SITE, DESIGN COMPRESSIVE BREAKING STRENGTH, AND TYPE OF BREAK FOR BOTH 7-DAY TESTS AND 28-DAY TESTS.
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**PROJECT INFORMATION**

PROPOSED BUILDING ADDITION PHASE ONE FOR:  
**SHORELAND LUTHERAN HIGH SCHOOL**  
 9026 12TH STREET • KENOSHA, WI 53144

PROFESSIONAL SEAL

PRELIMINARY DATES

AUG. 30, 2024

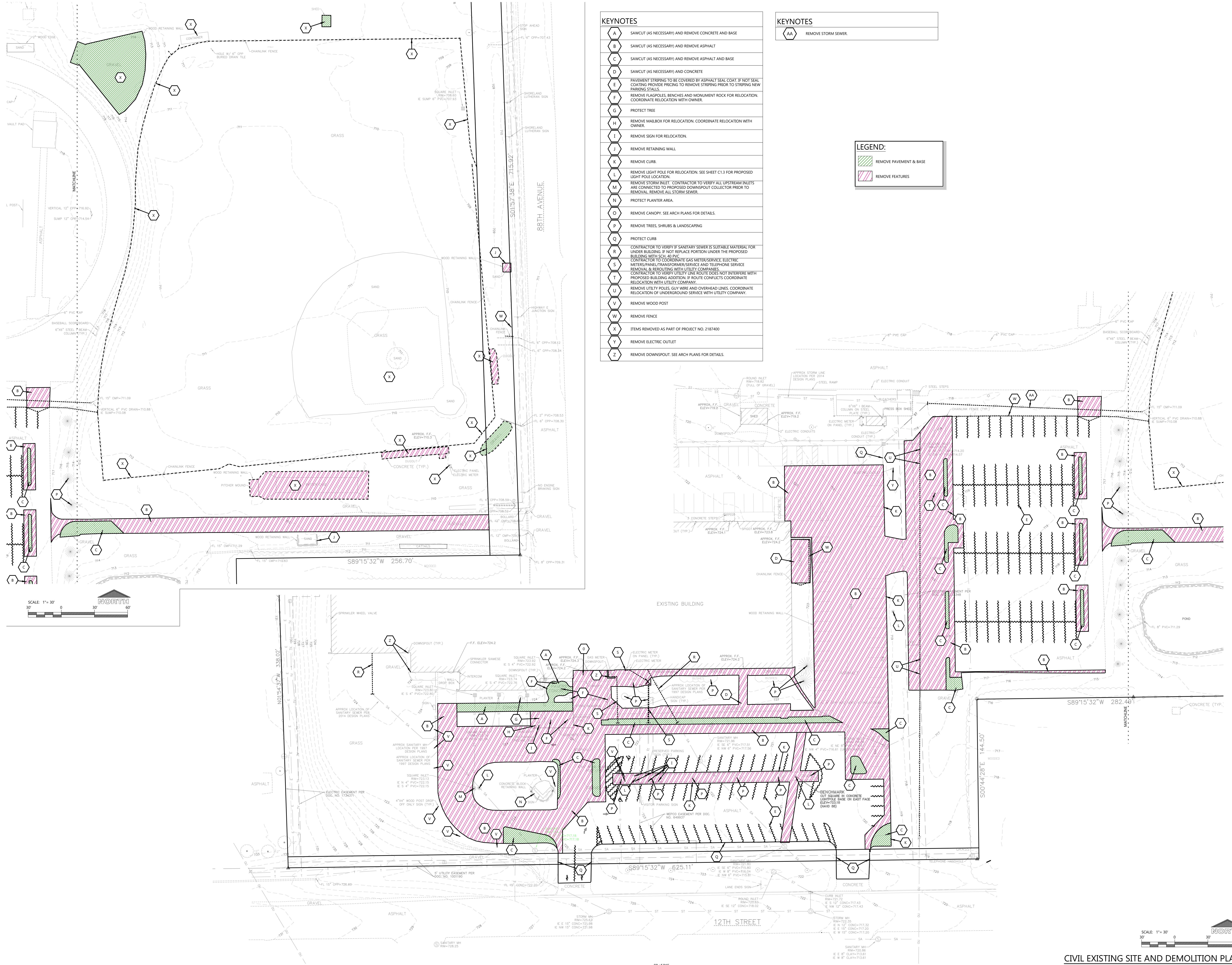
NOT FOR CONSTRUCTION

JOB NUMBER

240018700

SHEET NUMBER

**C1.0**



**KEYNOTES**

A	SAWCUT (AS NECESSARY) AND REMOVE CONCRETE AND BASE
B	SAWCUT (AS NECESSARY) AND REMOVE ASPHALT
C	SAWCUT (AS NECESSARY) AND REMOVE ASPHALT AND BASE
D	SAWCUT (AS NECESSARY) AND CONCRETE
E	PAVEMENT STRIPING TO BE COVERED BY ASPHALT SEAL COAT. IF NOT SEAL COATING PROVIDE PRICING TO REMOVE STRIPING PRIOR TO STRIPING NEW PARKING STALLS.
F	REMOVE FLAGPOLES, BENCHES AND MONUMENT ROCK FOR RELOCATION. COORDINATE RELOCATION WITH OWNER.
G	PROTECT TREE
H	REMOVE MAILBOX FOR RELOCATION. COORDINATE RELOCATION WITH OWNER.
I	REMOVE SIGN FOR RELOCATION.
J	REMOVE RETAINING WALL
K	REMOVE CURB.
L	REMOVE LIGHT POLE FOR RELOCATION. SEE SHEET C1.3 FOR PROPOSED LIGHT POLE LOCATION.
M	REMOVE STORM INLET. CONTRACTOR TO VERIFY ALL UPSTREAM INLETS ARE CONNECTED TO PROPOSED DOWNSPOUT COLLECTOR PRIOR TO REMOVAL. REMOVE ALL STORM SEWER.
N	PROTECT PLANTER AREA.
O	REMOVE CANOPY. SEE ARCH PLANS FOR DETAILS.
P	REMOVE TREES, SHRUBS & LANDSCAPING
Q	PROTECT CURB
R	CONTRACTOR TO VERIFY IF SANITARY SEWER IS SUITABLE MATERIAL FOR UNDER BUILDING. IF NOT REPLACE PORTION UNDER THE PROPOSED BUILDING WITH SOLID PVC.
S	CONTRACTOR TO COORDINATE GAS METER SERVICE, ELECTRIC METERS/PANELS/TRANSFORMER SERVICE AND TELEPHONE SERVICE REMOVAL & REROUTING WITH UTILITY COMPANIES.
T	CONTRACTOR TO VERIFY UTILITY LINE ROUTE DOES NOT INTERFERE WITH PROPOSED BUILDING ADDITION. IF ROUTE CONFLICTS COORDINATE RELOCATION WITH UTILITY COMPANY.
U	REMOVE UTILITY POLES, GUY WIRE AND OVERHEAD LINES. COORDINATE RELOCATION OF UNDERGROUND SERVICE WITH UTILITY COMPANY.
V	REMOVE WOOD POST
W	REMOVE FENCE
X	ITEMS REMOVED AS PART OF PROJECT NO. 2187400
Y	REMOVE ELECTRIC OUTLET
Z	REMOVE DOWNSPOUT. SEE ARCH PLANS FOR DETAILS.

**KEYNOTES**

AA	REMOVE STORM SEWER.
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**LEGEND:**

[Green Hatched Box]	REMOVE PAVEMENT & BASE
[Pink Hatched Box]	REMOVE FEATURES

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PROFESSIONAL SEAL

PRELIMINARY DATES

JULY 9, 2024  
JULY 16, 2024  
AUG. 30, 2024

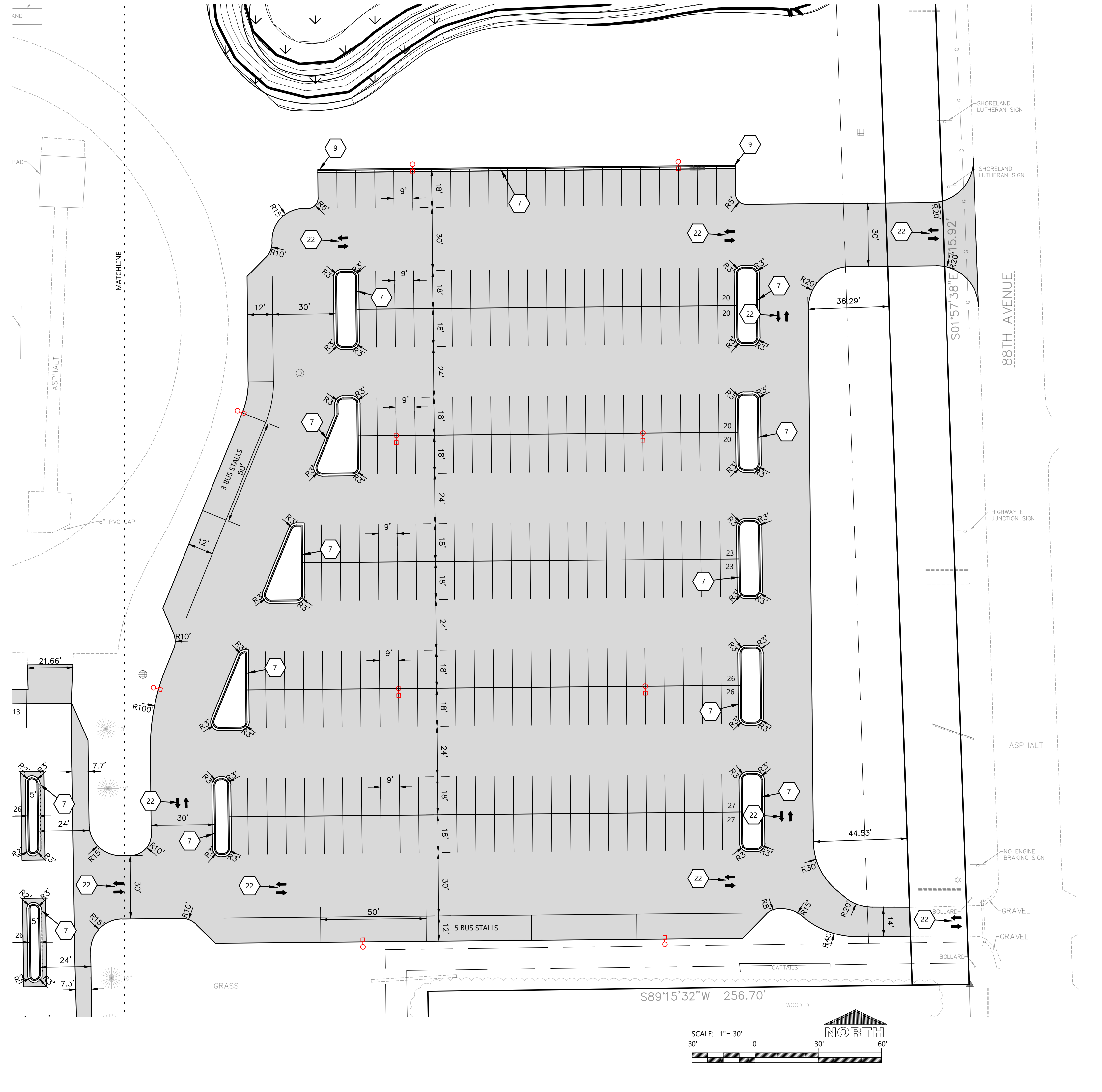
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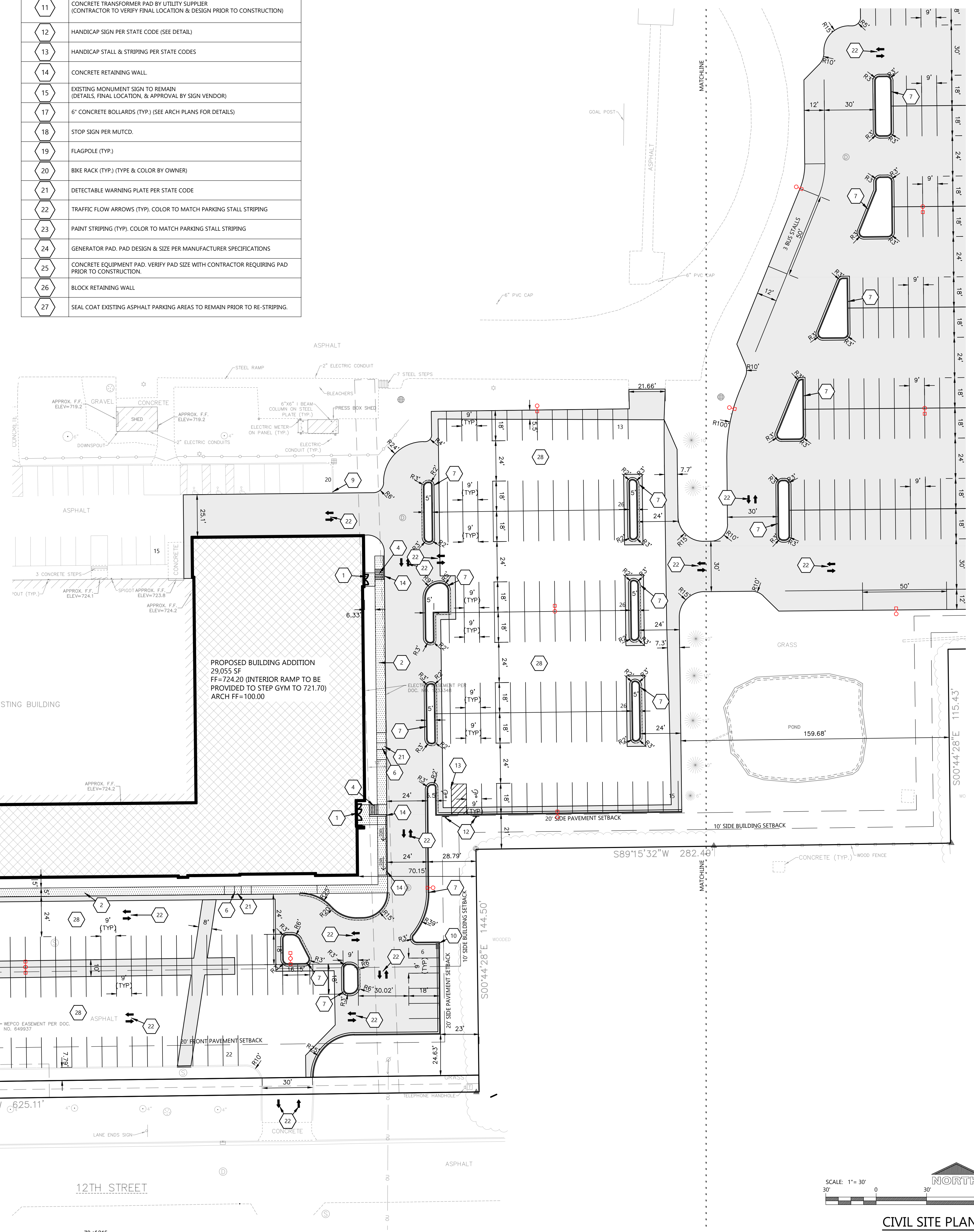


**KEYNOTES**

- 1 CONCRETE STOOP (SEE STRUCTURAL PLANS FOR DETAILS)
- 2 RAISED WALK (SEE DETAIL)
- 3 FLUSH WALK (SEE DETAIL)
- 4 CONCRETE STAIRS (SEE ARCH/STRUCTURAL PLANS FOR INFORMATION)
- 5 CURB RAMP (SEE DETAIL)
- 6 ADA CURB RAMP (SEE DETAIL)
- 7 18" CURB & GUTTER (SEE DETAIL)
- 8 18" MOUNTABLE CURB & GUTTER (SEE DETAIL)
- 9 CURB TAPER (SEE DETAIL)
- 10 CURB CUT (SEE DETAIL)
- 11 CONCRETE TRANSFORMER PAD BY UTILITY SUPPLIER (CONTRACTOR TO VERIFY FINAL LOCATION & DESIGN PRIOR TO CONSTRUCTION)
- 12 HANDICAP SIGN PER STATE CODE (SEE DETAIL)
- 13 HANDICAP STALL & STRIPING PER STATE CODES
- 14 CONCRETE RETAINING WALL
- 15 EXISTING MONUMENT SIGN TO REMAIN (DETAILS, FINAL LOCATION, & APPROVAL BY SIGN VENDOR)
- 17 6" CONCRETE BOLLARDS (TYP.) (SEE ARCH PLANS FOR DETAILS)
- 18 STOP SIGN PER MUTCD
- 19 FLAGPOLE (TYP.)
- 20 BIKE RACK (TYP.) (TYPE & COLOR BY OWNER)
- 21 DETECTABLE WARNING PLATE PER STATE CODE
- 22 TRAFFIC FLOW ARROWS (TYP.) COLOR TO MATCH PARKING STALL STRIPING
- 23 PAINT STRIPING (TYP.) COLOR TO MATCH PARKING STALL STRIPING
- 24 GENERATOR PAD. PAD DESIGN & SIZE PER MANUFACTURER SPECIFICATIONS
- 25 CONCRETE EQUIPMENT PAD. VERIFY PAD SIZE WITH CONTRACTOR REQUIRING PAD PRIOR TO CONSTRUCTION.
- 26 BLOCK RETAINING WALL
- 27 SEAL COAT EXISTING ASPHALT PARKING AREAS TO REMAIN PRIOR TO RE-STRIPING.

**LEGEND:**

HATCH	PAVEMENT SECTION	HATCH	PAVEMENT SECTION
[Hatch]	STANDARD ASPHALT	[Hatch]	HEAVY DUTY CONCRETE
[Hatch]	HEAVY DUTY ASPHALT	[Hatch]	LOADING DOCK CONCRETE
[Hatch]	SIDEWALK CONCRETE	[Hatch]	DUMPSTER PAD / APRON CONCRETE
[Hatch]	LIGHT DUTY CONCRETE	[Hatch]	SHEDDING CURB & GUTTER
[Hatch]	INVERTED CURB & GUTTER	[Hatch]	



PROJECT INFORMATION

PROPOSED BUILDING ADDITION PHASE ONE FOR:  
**SHORELAND LUTHERAN HIGH SCHOOL**  
9026 12TH STREET • KENOSHA, WI 53144

PROFESSIONAL SEAL

PRELIMINARY DATES  
AUG. 30, 2024

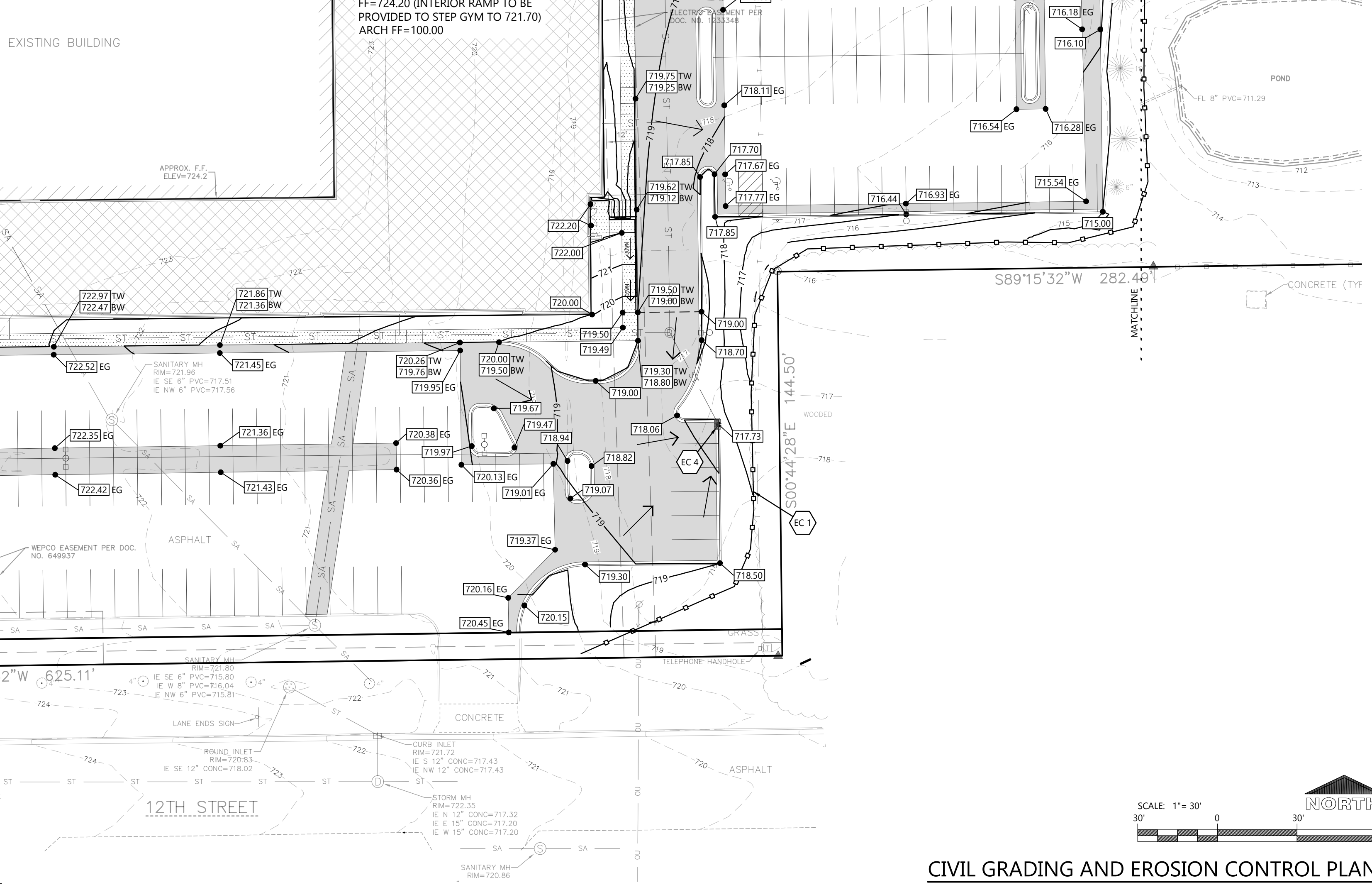
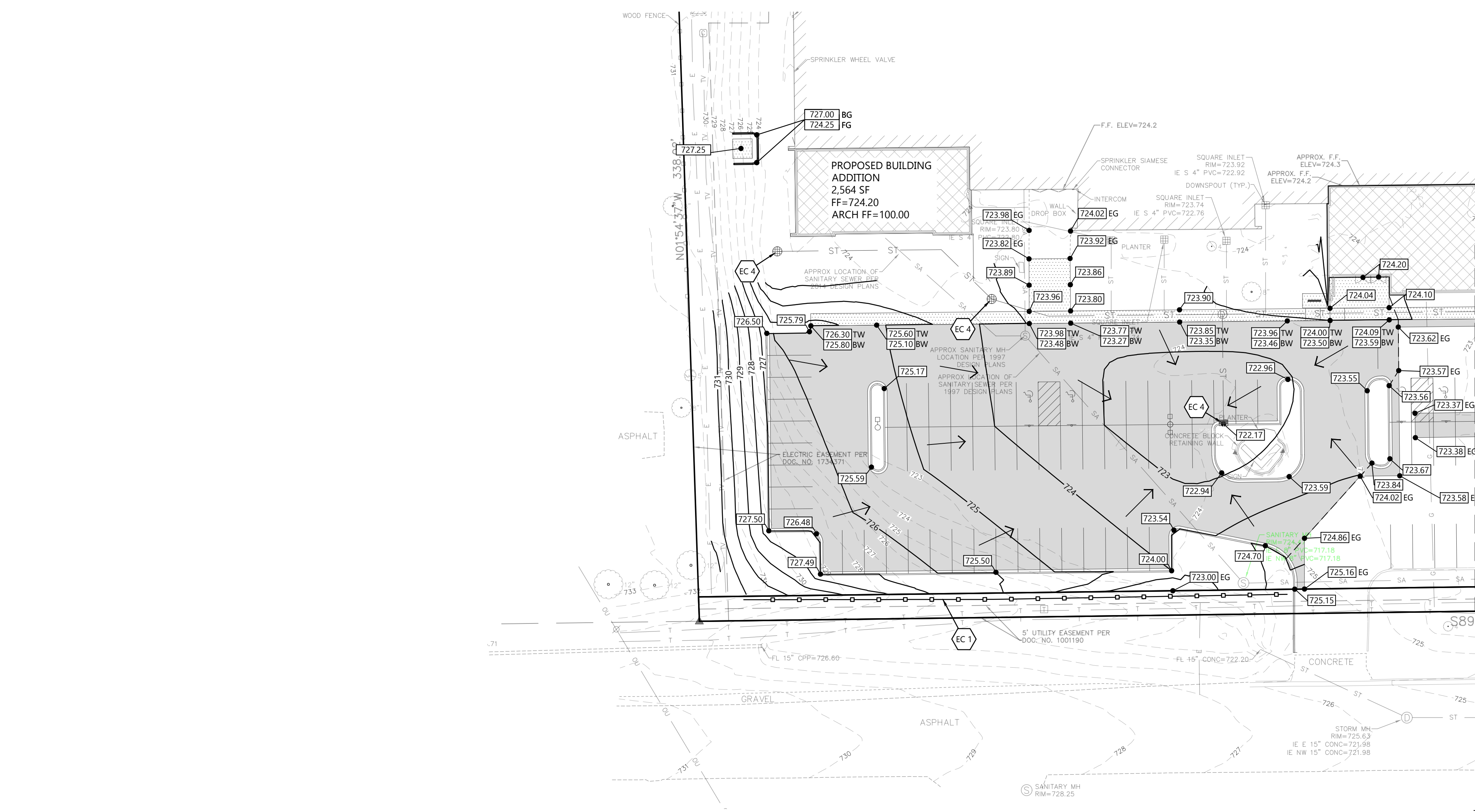
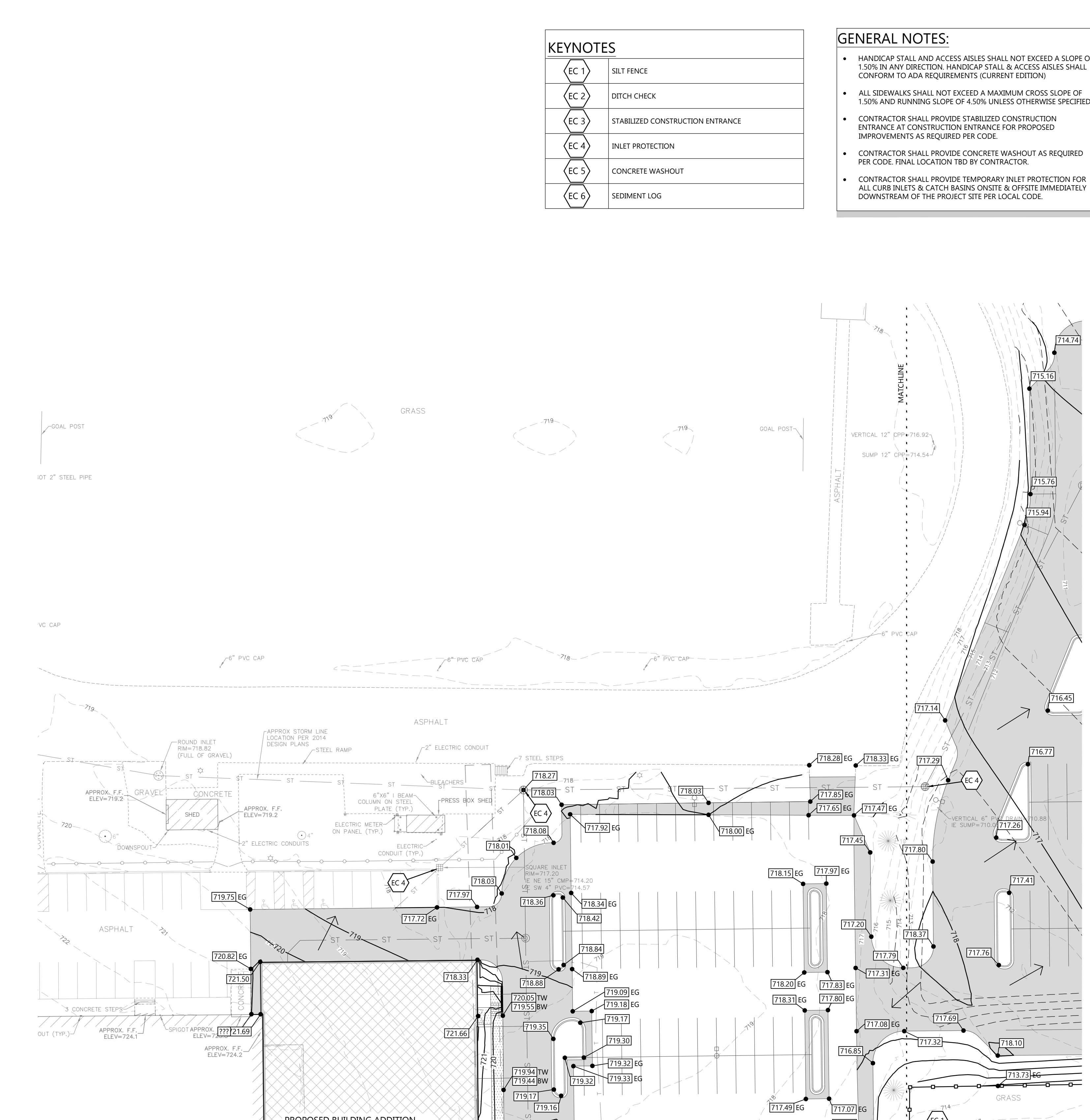
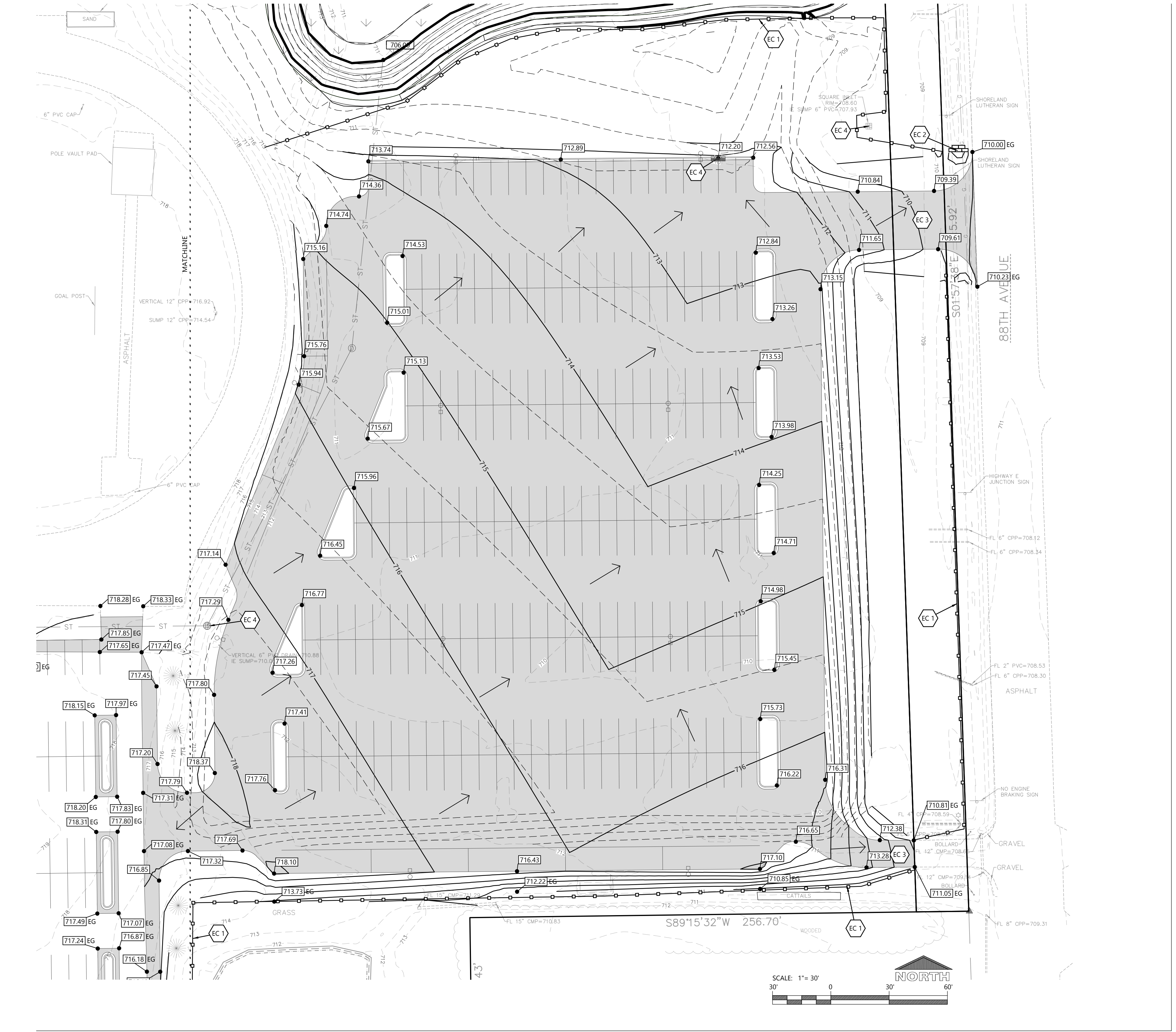
JOB NUMBER  
240018700

SHEET NUMBER  
**C1.2**

NOT FOR CONSTRUCTION

KEYNOTES	
EC 1	SILT FENCE
EC 2	DITCH CHECK
EC 3	STABILIZED CONSTRUCTION ENTRANCE
EC 4	INLET PROTECTION
EC 5	CONCRETE WASHOUT
EC 6	SEDIMENT LOG

- GENERAL NOTES:**
- HANDICAP STALL AND ACCESS ASILES SHALL NOT EXCEED A SLOPE OF 1.50% IN ANY DIRECTION. HANDICAP STALL & ACCESS ASILES SHALL CONFORM TO ADA REQUIREMENTS (CURRENT EDITION)
  - ALL SIDEWALKS SHALL NOT EXCEED A MAXIMUM CROSS SLOPE OF 1.50% AND RUNNING SLOPE OF 4.50% UNLESS OTHERWISE SPECIFIED.
  - CONTRACTOR SHALL PROVIDE STABILIZED CONSTRUCTION ENTRANCE #1 AT CONSTRUCTION ENTRANCE FOR PROPOSED IMPROVEMENTS AS REQUIRED PER CODE.
  - CONTRACTOR SHALL PROVIDE CONCRETE WASHOUT AS REQUIRED PER CODE. FINAL LOCATION TBD BY CONTRACTOR.
  - CONTRACTOR SHALL PROVIDE TEMPORARY INLET PROTECTION FOR ALL CURB INLETS & CATCH BASINS ON-SITE & OFF-SITE IMMEDIATELY DOWNSTREAM OF THE PROJECT SITE PER LOCAL CODE.



PROJECT INFORMATION

PROPOSED BUILDING ADDITION PHASE ONE FOR:  
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PROFESSIONAL SEAL

PRELIMINARY DATES

AUG. 30, 2024

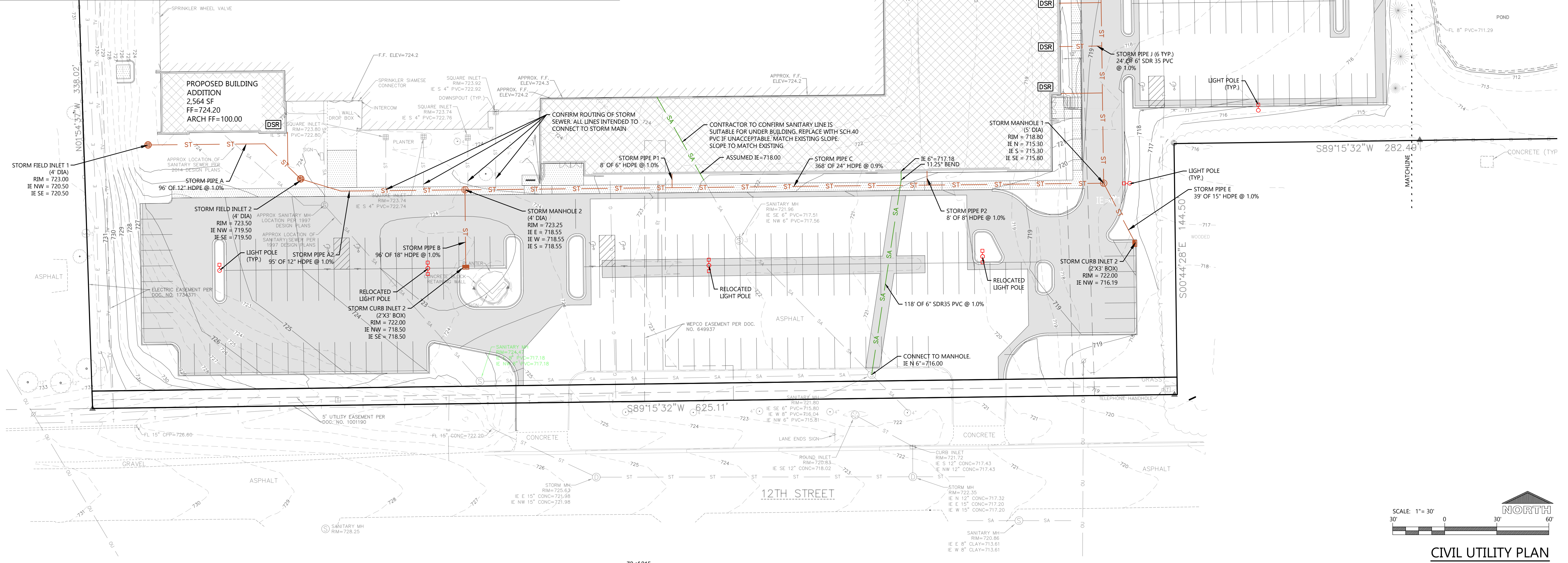
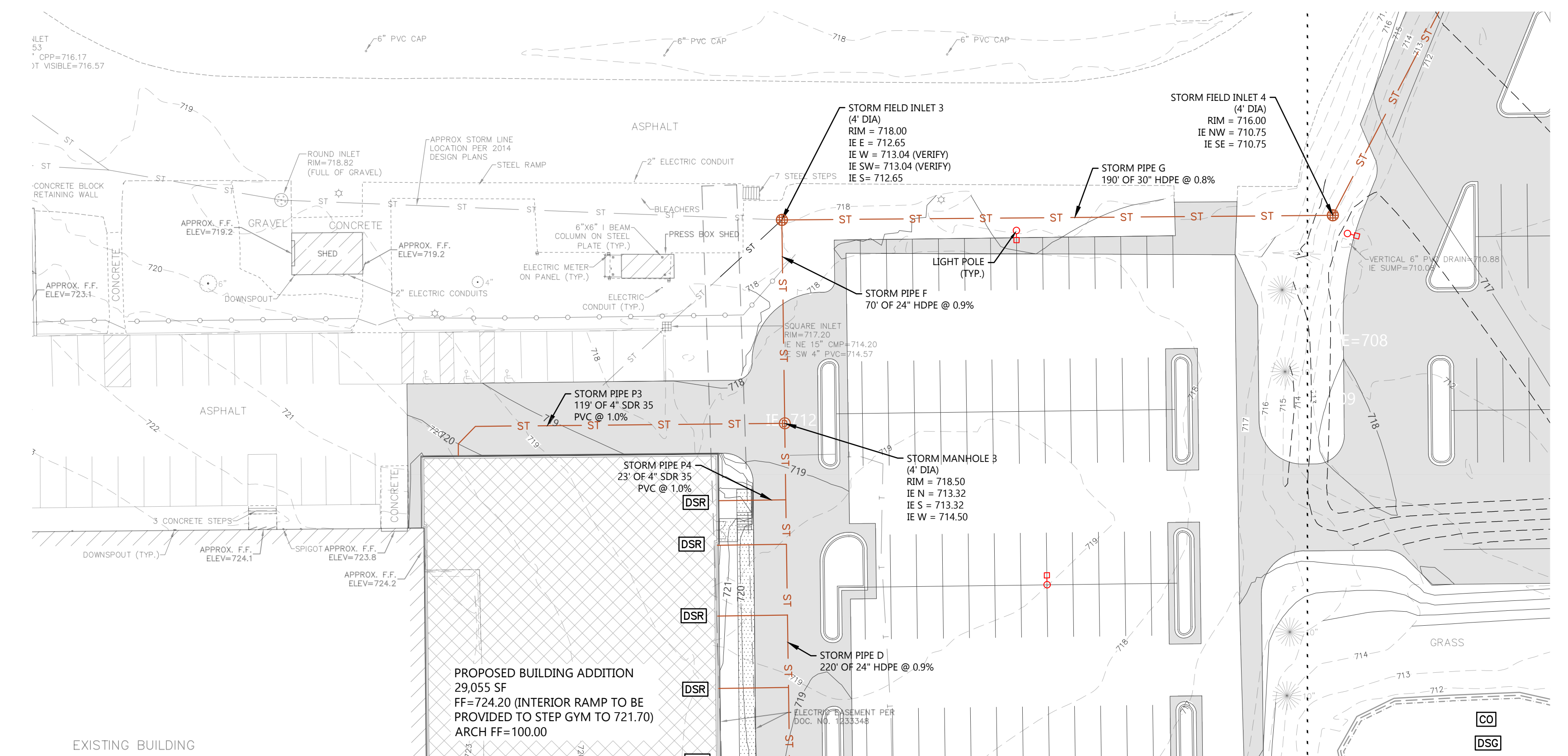
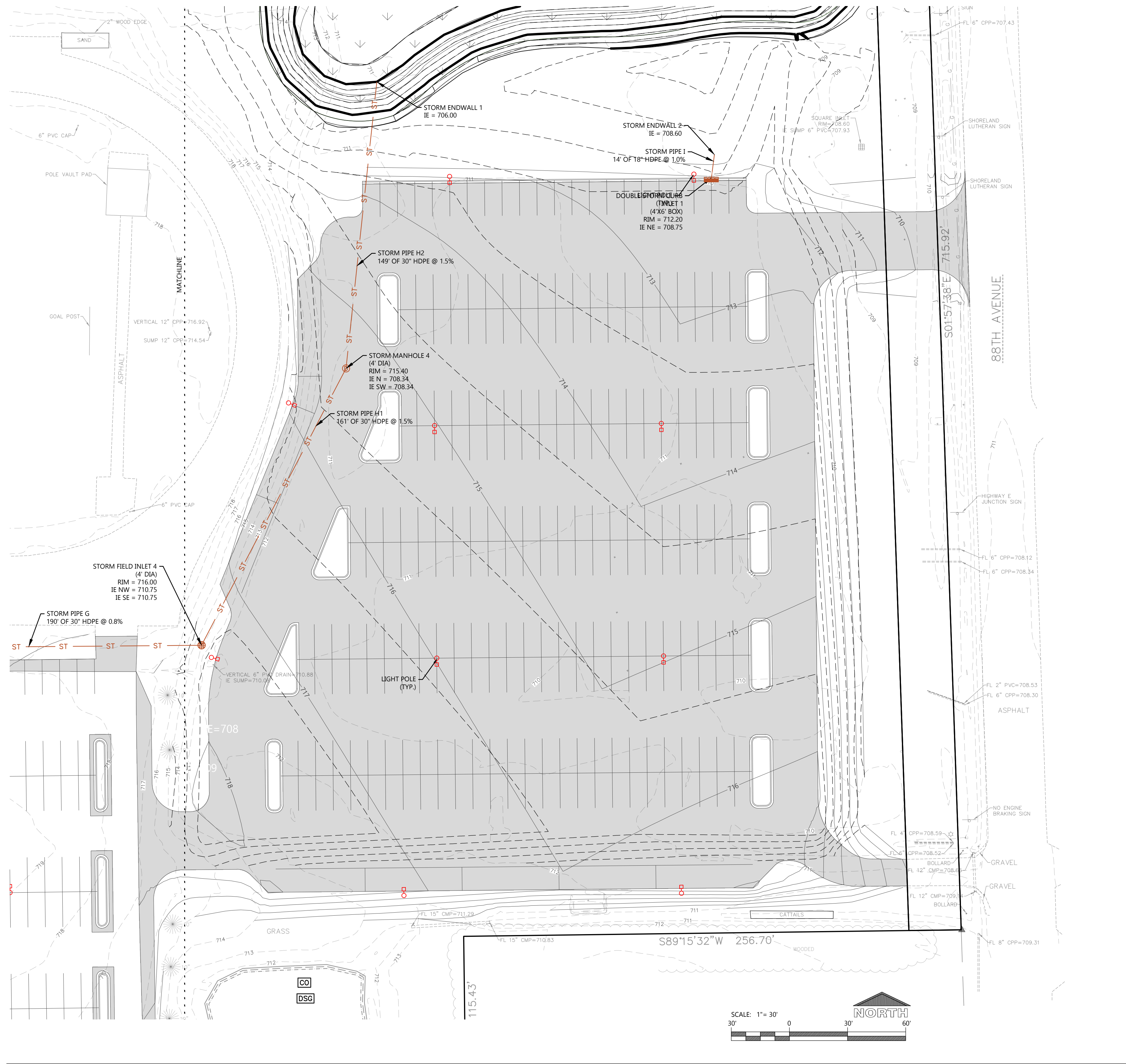
NOT FOR CONSTRUCTION

JOB NUMBER

240018700

SHEET NUMBER

**C1.3**



**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES TECHNICAL STANDARD NO. 1053 (CHANNEL EROSION MAT).

VARIATIONS IN THE DIMENSIONS OR MATERIALS SHOWN HEREON SHALL BE PERMITTED IF THEY PROVIDE EQUIVALENT PROTECTION AND MATERIAL STRENGTH AND IF PRIOR APPROVAL OF THE ENGINEER IS OBTAINED.

LAP JOINTS SHALL NOT BE PLACED IN THE BOTTOM OF V-SHAPED DITCHES.

JUNCTION SLOTS ON ADJACENT STRIPS OF MATTING SHALL BE STAGGERED A MINIMUM OF 4 FEET APART.

EDGES OF EROSION MAT SHALL BE IMPRESSED IN THE SOIL.

EROSION MAT SHALL PAID BY THE SQUARE YARD INSTALLED.

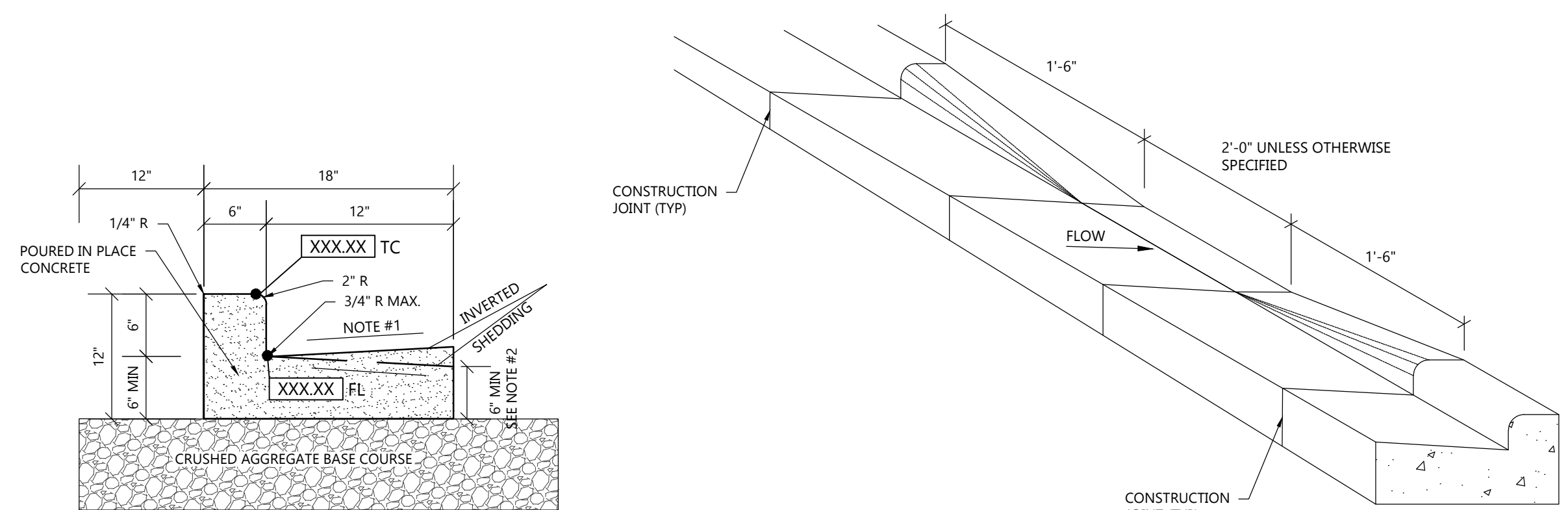
**EROSION MAT OVER SEEDING**

JUNCTION OR ANCHOR SLOTS SHALL BE AT MINIMUM INTERVALS OF 100 FEET ON GRADES UP TO AND INCLUDING 3%, AND 50 FEET ON GRADES EXCEEDING 3%.

NOTE: SEE SPECIFICATIONS FOR MATTING TYPE

**CHANNEL EROSION MAT DETAIL**

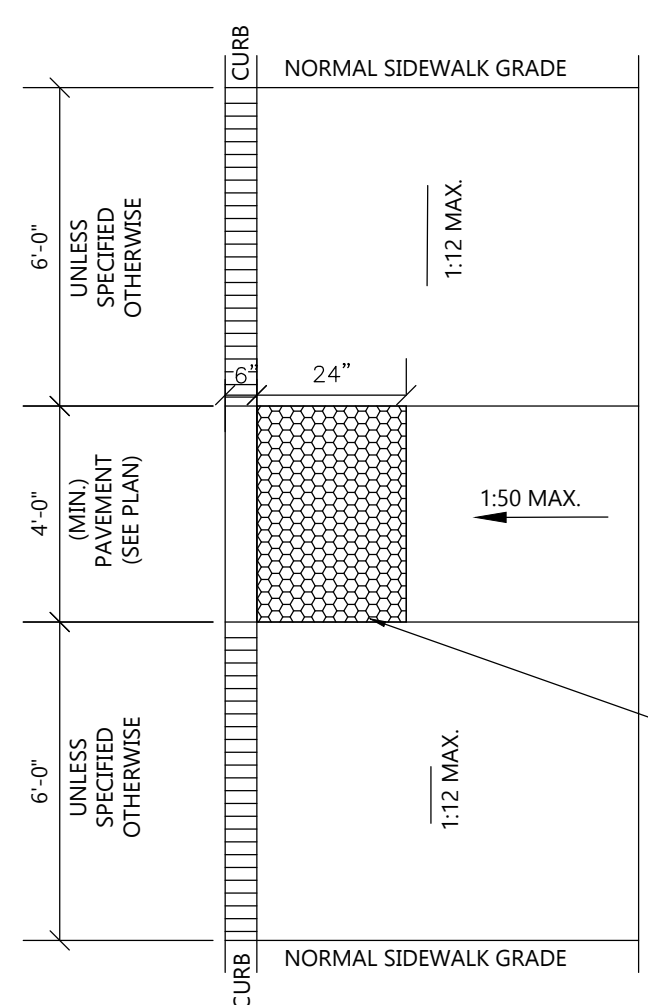
NOT TO SCALE



NOTE:  
1. USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.  
2. THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MIN. GUTTER THICKNESS IS MAINTAINED.  
3. SEE SITE PLAN & GRADING PLAN FOR INVERTED & SHEDDING CURB LOCATIONS

**18" CONCRETE CURB & GUTTER DETAIL**

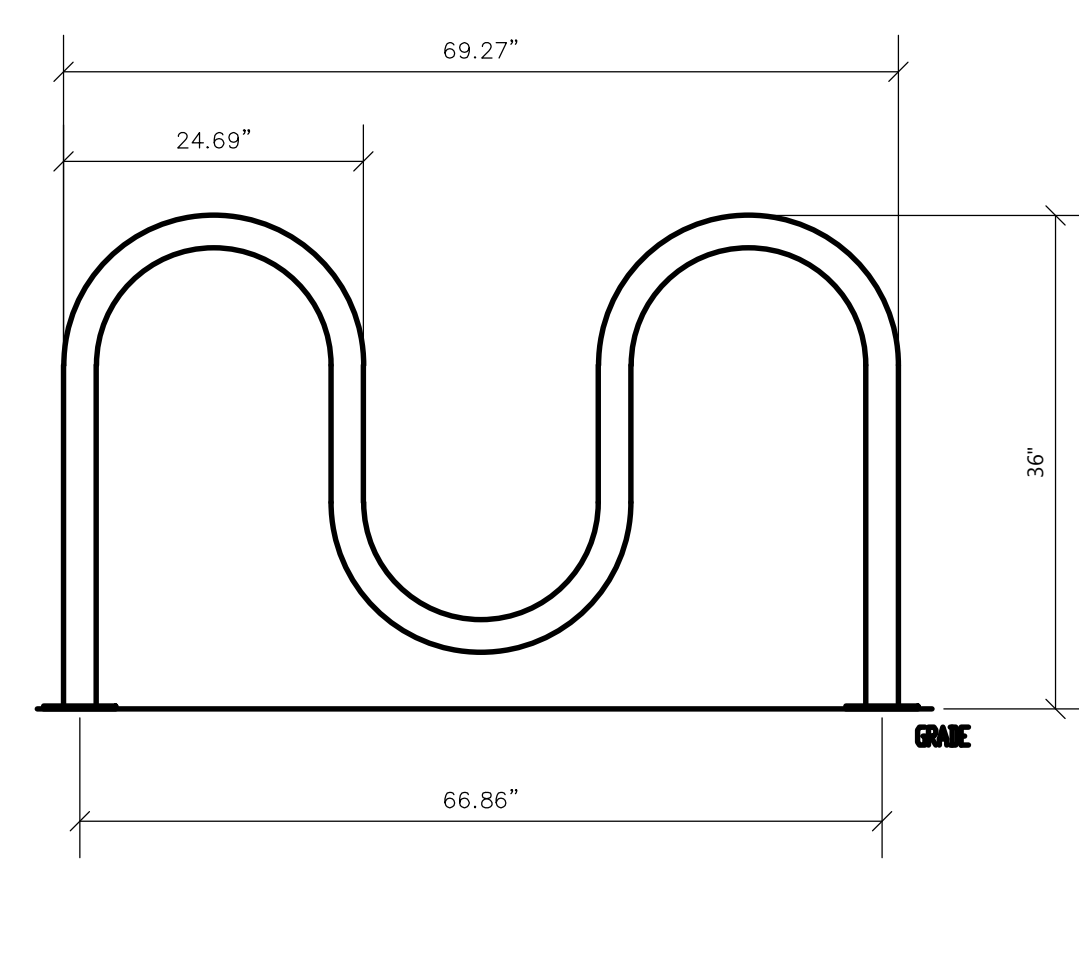
NOT TO SCALE



NOTE:  
ADA CURB RAMP SHALL CONFORM TO THE CURRENT EDITION OF ADA STANDARDS FOR ACCESSIBLE DESIGN FOR ALL REQUIREMENTS.  
DETECTABLE WARNING SURFACE: SURFACE TO BE TRUNCATED DOMES WITH A BASE DIAMETER OF 0.9" MIN. TO 1.4" MAX. AND A TOP DIAMETER OF 0.5" MIN. TO 0.55" MAX. OF THE BASE DIAMETER. HEIGHT OF DOMES SHALL HAVE A HEIGHT OF 0.2" TRUNCATED DOMES SHALL HAVE A CENTER-TO-CENTER SPACING OF 1.6" MIN. AND 2.4" MAX. AND A BASE-TO-BASE SPACING OF 0.65" MIN. MEASURED BETWEEN THE MOST ADJACENT DOMES ON THE GRID. DOMES SHALL BE ALIGNED IN A SQUARE GRID PATTERN.

**ADA SIDEWALK RAMP DETAIL**

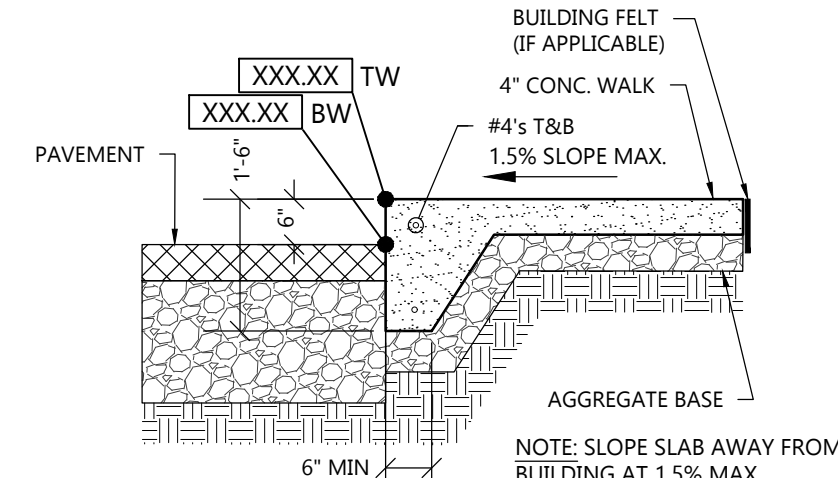
NOT TO SCALE



NOTES:  
1. INSTALL BIKE RACKS ACCORDING TO MANUFACTURER'S SPECIFICATIONS.  
2. OWNER SHALL SELECT COLOR & FINISH.  
3. SEE SITE PLAN FOR APPROX. LOCATION, COORDINATE W/ OWNER PRIOR TO CONSTRUCTION.  
4. MANUFACTURED BY MADRAK; PRODUCT: CS200-S-IGSF; DESCRIPTION: CAPITAL SQUARE BIKE RAKE 5 BIKE

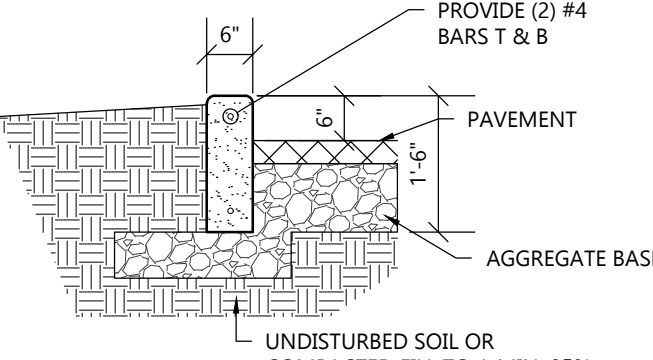
**5 BIKE RACK DETAIL-WAVE TYPE**

NOT TO SCALE



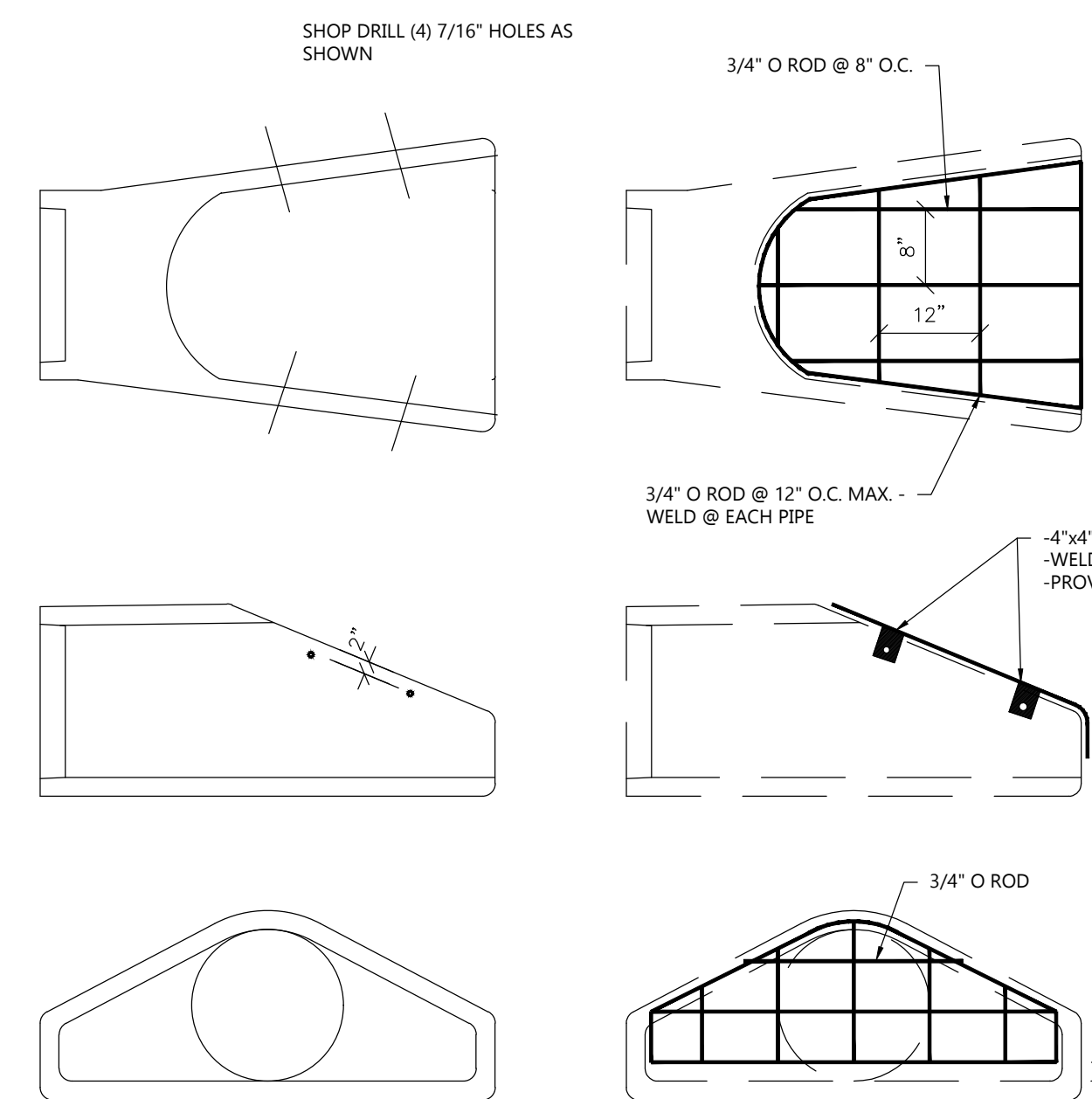
**RAISED WALK DETAIL**

NOT TO SCALE



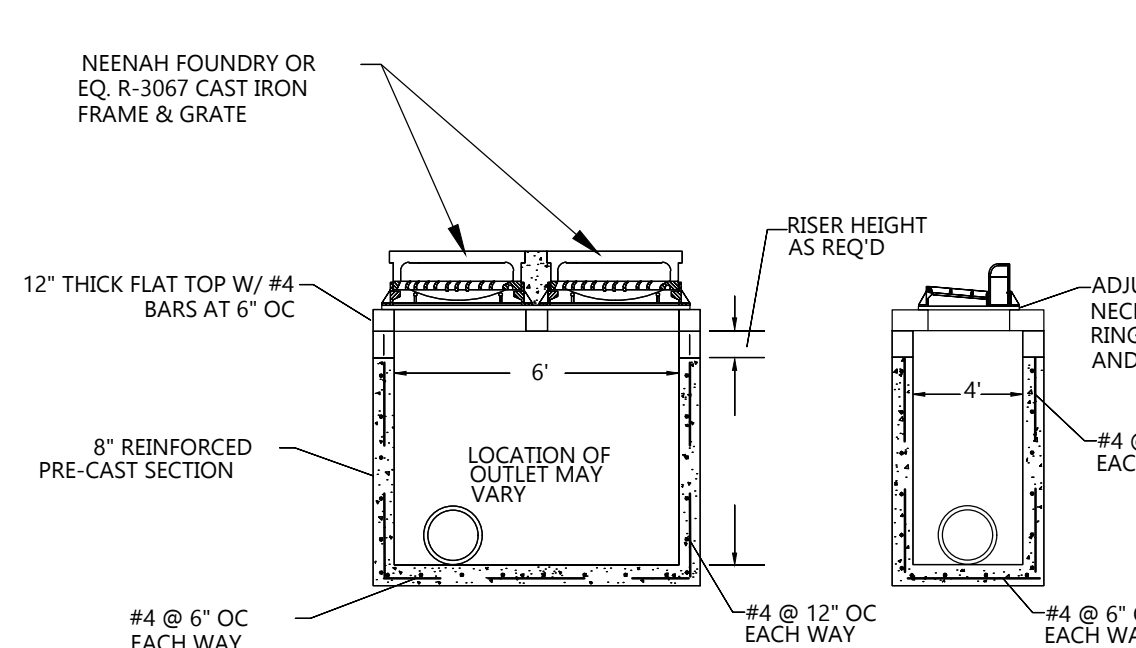
**6" CURB HEAD DETAIL**

NOT TO SCALE



**CURB CUT DETAIL**

NOT TO SCALE



**DOUBLE FRAME CURB INLET**

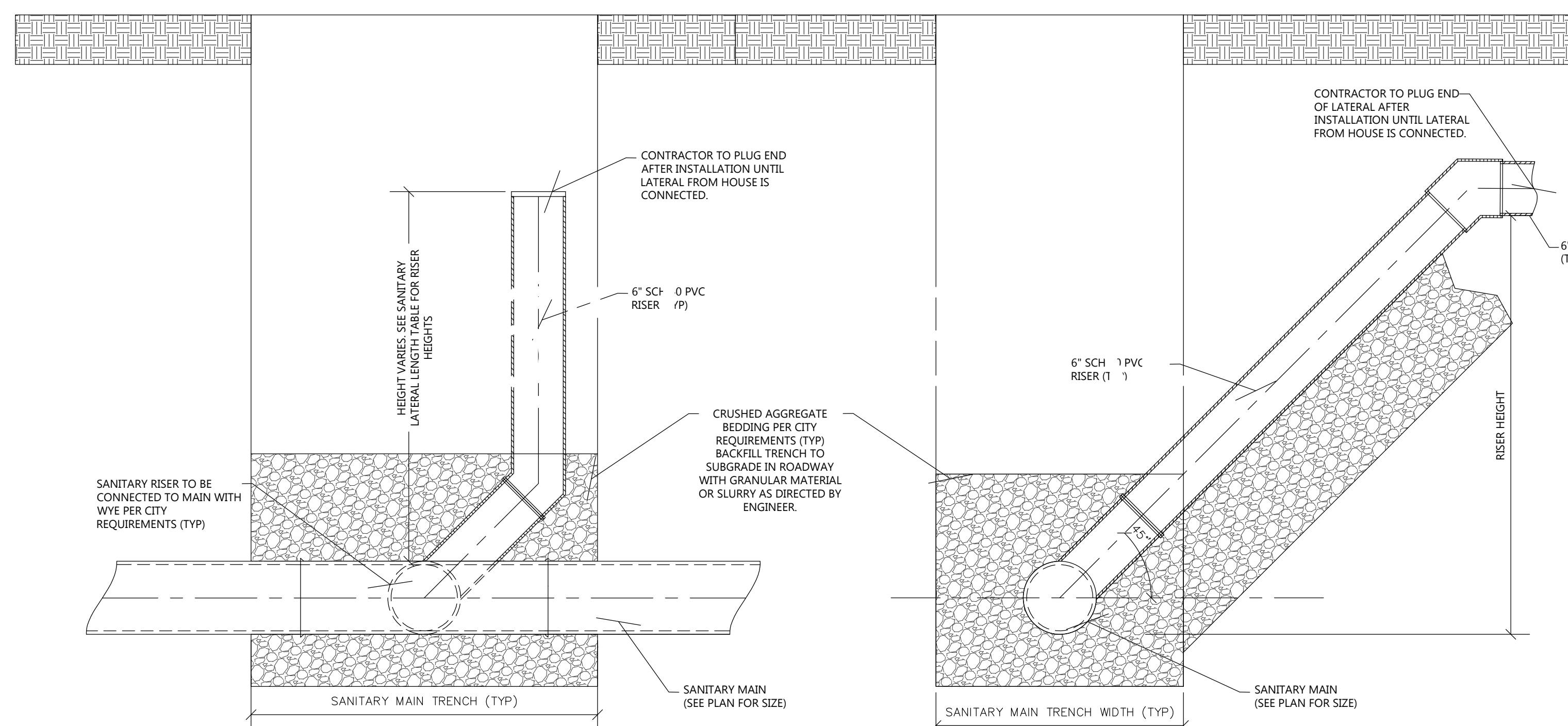
NO SCALE

THE CONTRACTOR SHALL BOLT THE GRATE TO THE CONCRETE END WALL WITH FOUR 3/8" X 6" MACHINE BOLTS WITH NUTS ON INSIDE WALL. APPLICABLE FOR PIPE SIZES 18" AND GREATER.  
PAINTING SPECIFICATIONS:  
THE PIPE GRATE SHALL RECEIVE THE FOLLOWING PREPARATION & PAINTING. SEE NOTES: FIRST COAT- RUST-OLEUM X-60 RED BARE METAL PRIMER OR EQUAL. SECOND COAT- RUST-OLEUM 960 ZINC CHROMATE PRIMER OR EQUAL. THIRD COAT- RUST-OLEUM 1500 HIGH GLOSS 8 METALLIC FINISH OR EQUAL.  
NOTES:  
• BARE SURFACES - AFTER THROUGH SCRAPING, WIRE BRUSHING & CLEANING, APPLY THE THREE COAT SYSTEM LISTED.  
• EACH COAT AN OVERALL COAT  
• ALLOW 24-48 HOURS DRYING TIME BETWEEN COATS.

**GRATE INSTALLATION DETAIL**

NOT TO SCALE

NOTE:  
CONTRACTOR IS RESPONSIBLE TO INSTALL SANITARY RISER PER CITY REQUIREMENTS. IF DISCREPANCIES OCCUR, CONTRACTOR TO CONTACT DESIGN ENGINEER PRIOR TO INSTALLATION.



**PLAN VIEW**

NOT TO SCALE

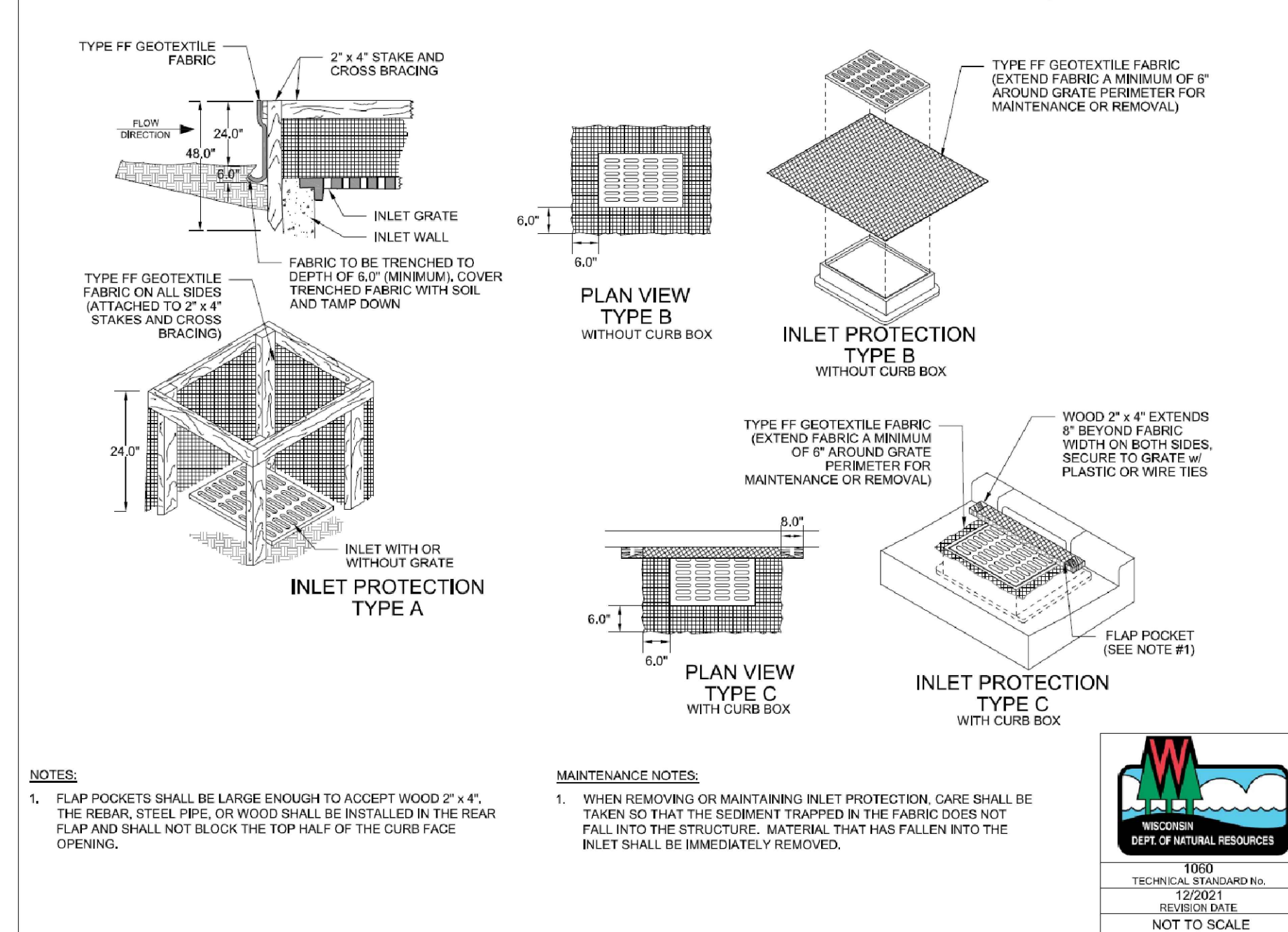
**SANITARY RISER DETAIL**

NOT TO SCALE

**PROFILE VIEW**

NOT TO SCALE

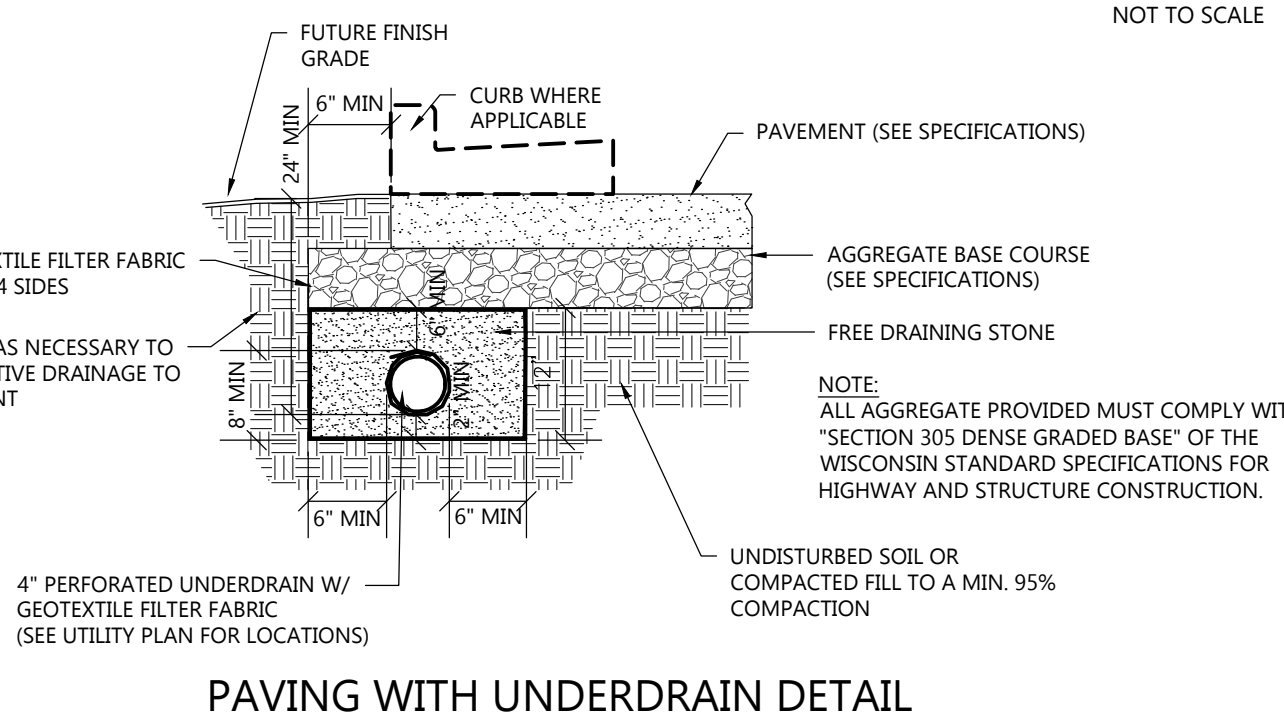
**FIGURE 1. INLET PROTECTION TYPES A, B AND C**



NOTE:  
1. FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2" X 4". THE REAR STEEL PIPE, OR WOOD SHALL BE INSTALLED IN THE REAR FLAP AND SHALL NOT BLOCK THE TOP HALF OF THE CURB FACE OPENING.  
NOTE:  
2. WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEGMENT TRAPPED IN THE FABRIC DOES NOT FALL INTO THE STRUCTURE. MATERIAL THAT HAS FALLEN INTO THE INLET SHALL BE IMMEDIATELY REMOVED.

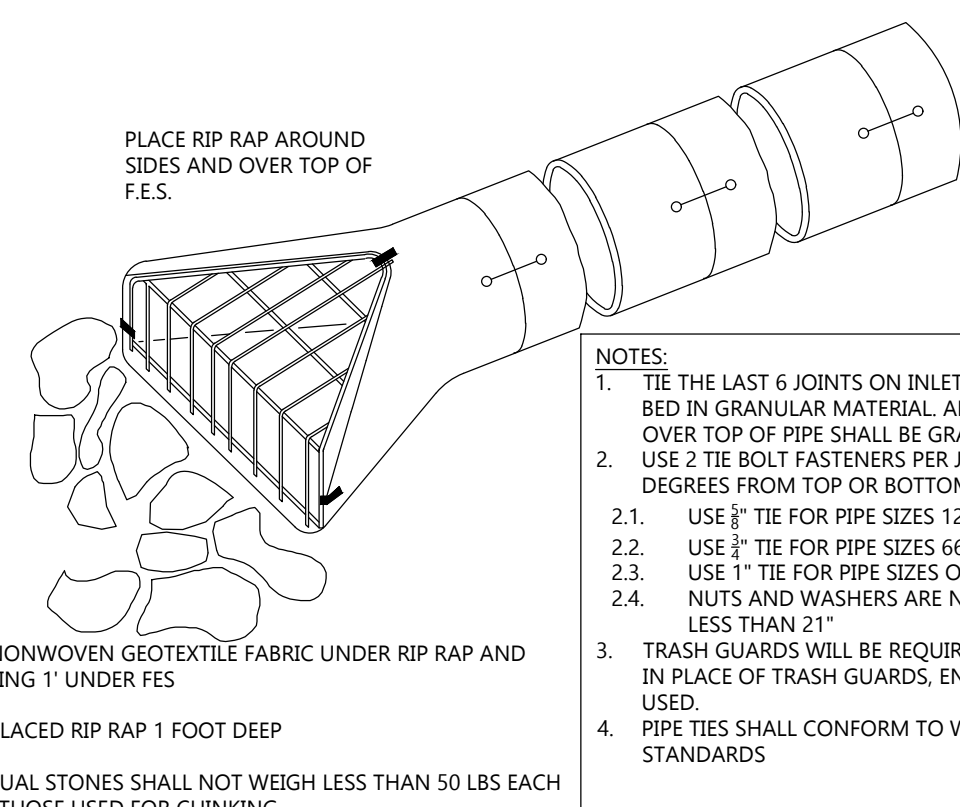
**INLET PROTECTION DETAIL**

NOT TO SCALE



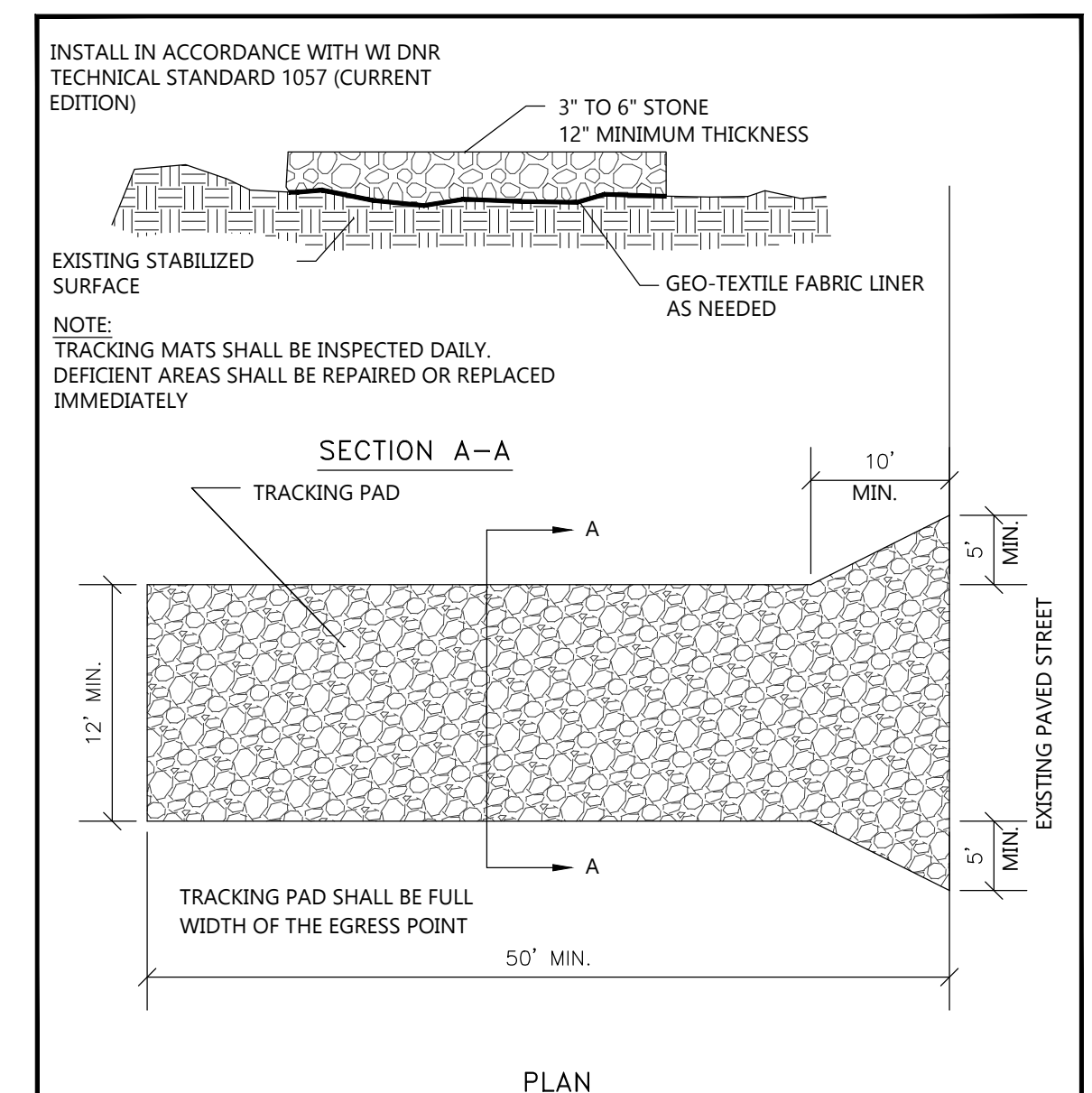
**PAVING WITH UNDERDRAIN DETAIL**

NOT TO SCALE



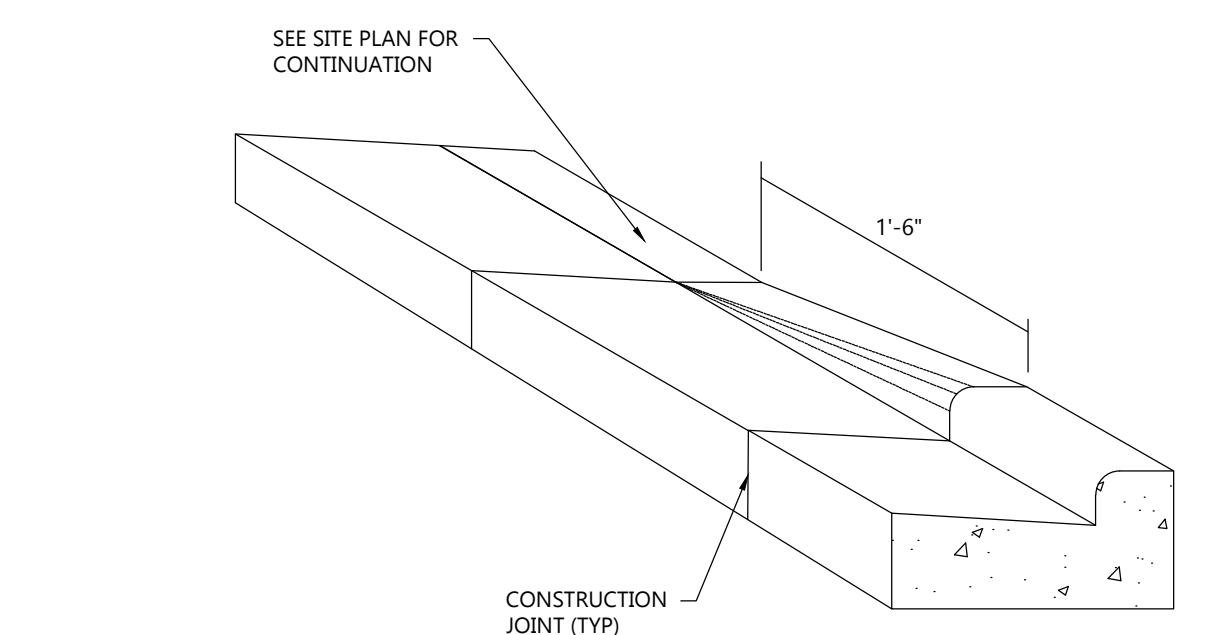
**CONCRETE END SECTION DETAIL**

NOT TO SCALE



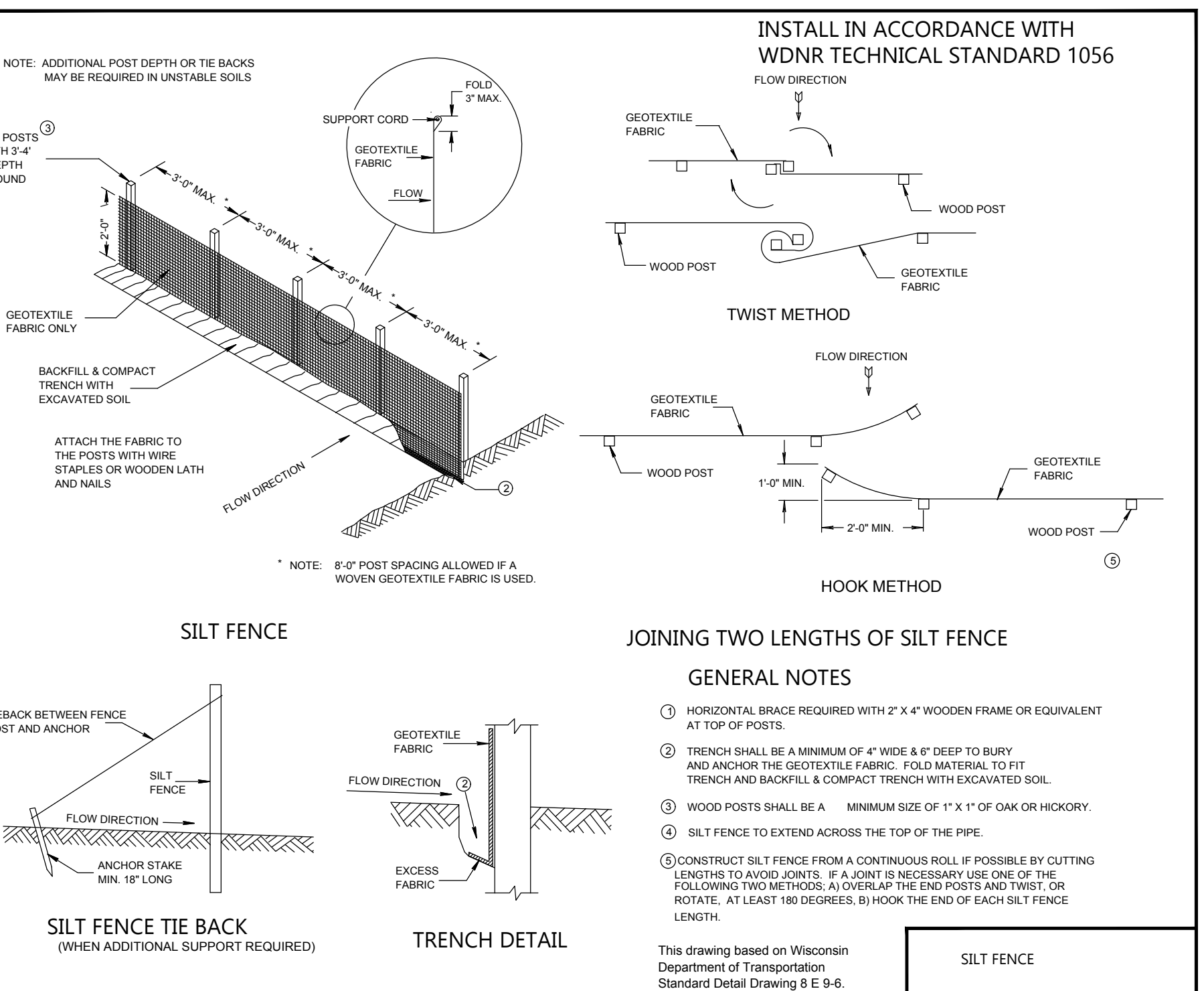
**TRACKPAD DETAILS**

NOT TO SCALE



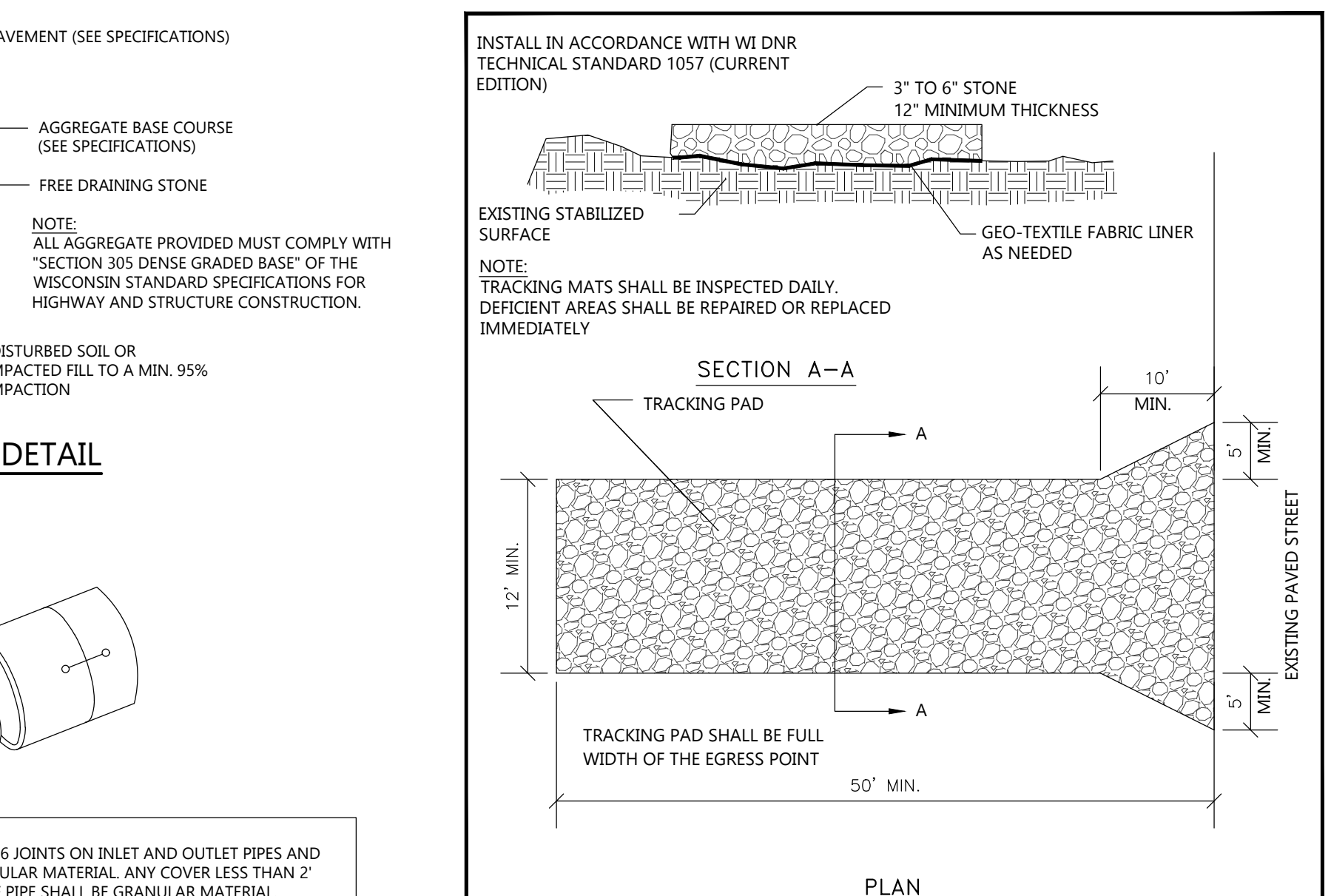
**CURB TAPER DETAIL**

NOT TO SCALE



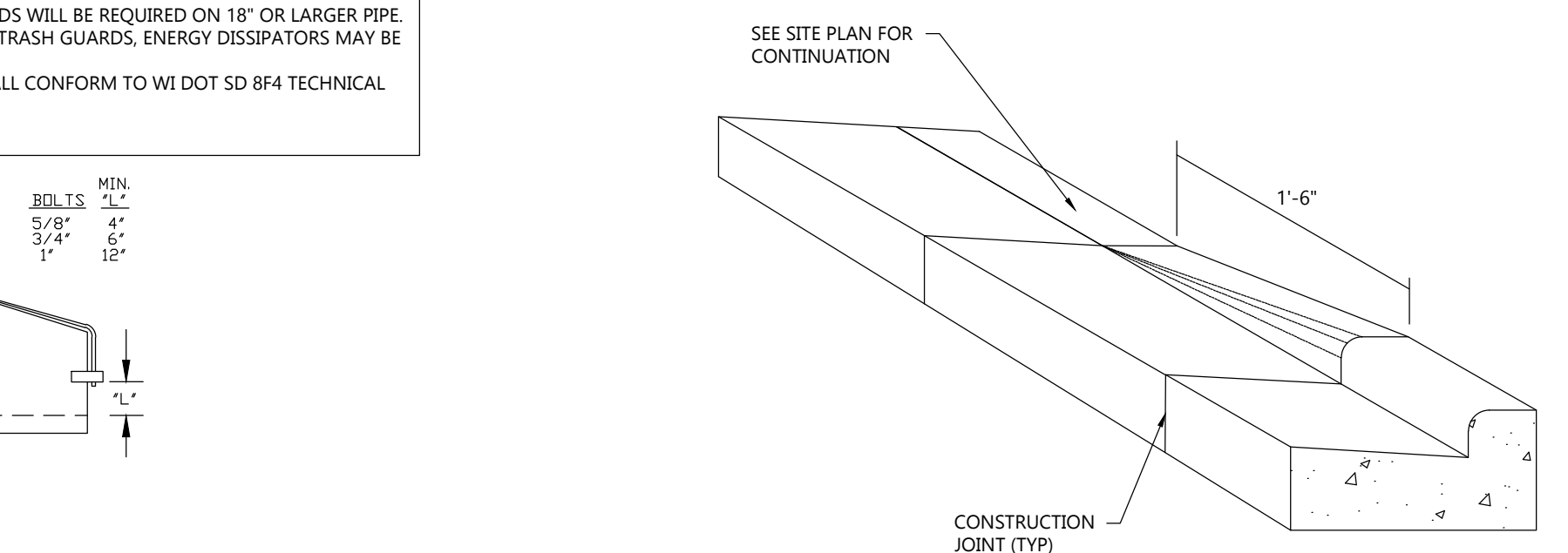
**SILT FENCE - INSTALLATION DETAIL**

NOT TO SCALE



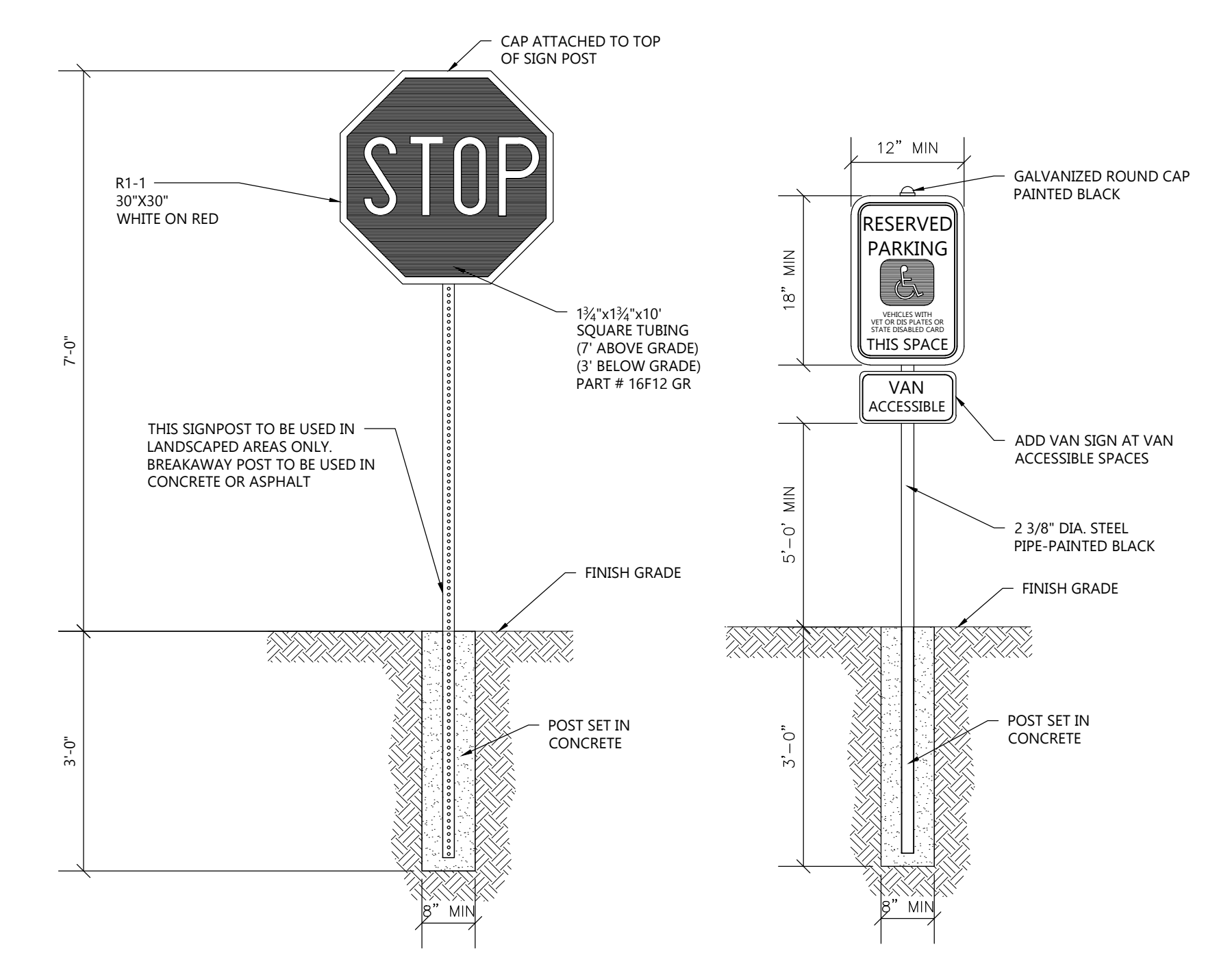
**SILT FENCE**

NOT TO SCALE



**SILT FENCE**

NOT TO SCALE



**STOP SIGN WITH CONCRETE BASE DETAIL**

NOT TO SCALE

**HANDICAP SIGNAGE WITH CONCRETE BASE DETAIL**

NOT TO SCALE

**CIVIL DETAILS**

**PROJECT INFORMATION**

PROPOSED BUILDING ADDITION PHASE ONE FOR:  
**SHORELAND LUTHERAN HIGH SCHOOL**  
9026 12TH STREET • KENOSHA, WI 53144

**PROFESSIONAL SEAL**

**PRELIMINARY DATES**

AUG. 30, 2024

NOT FOR CONSTRUCTION

**JOB NUMBER**

240018700

**SHEET NUMBER**

C2.0

**PROJECT INFORMATION**

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**PROFESSIONAL SEAL**

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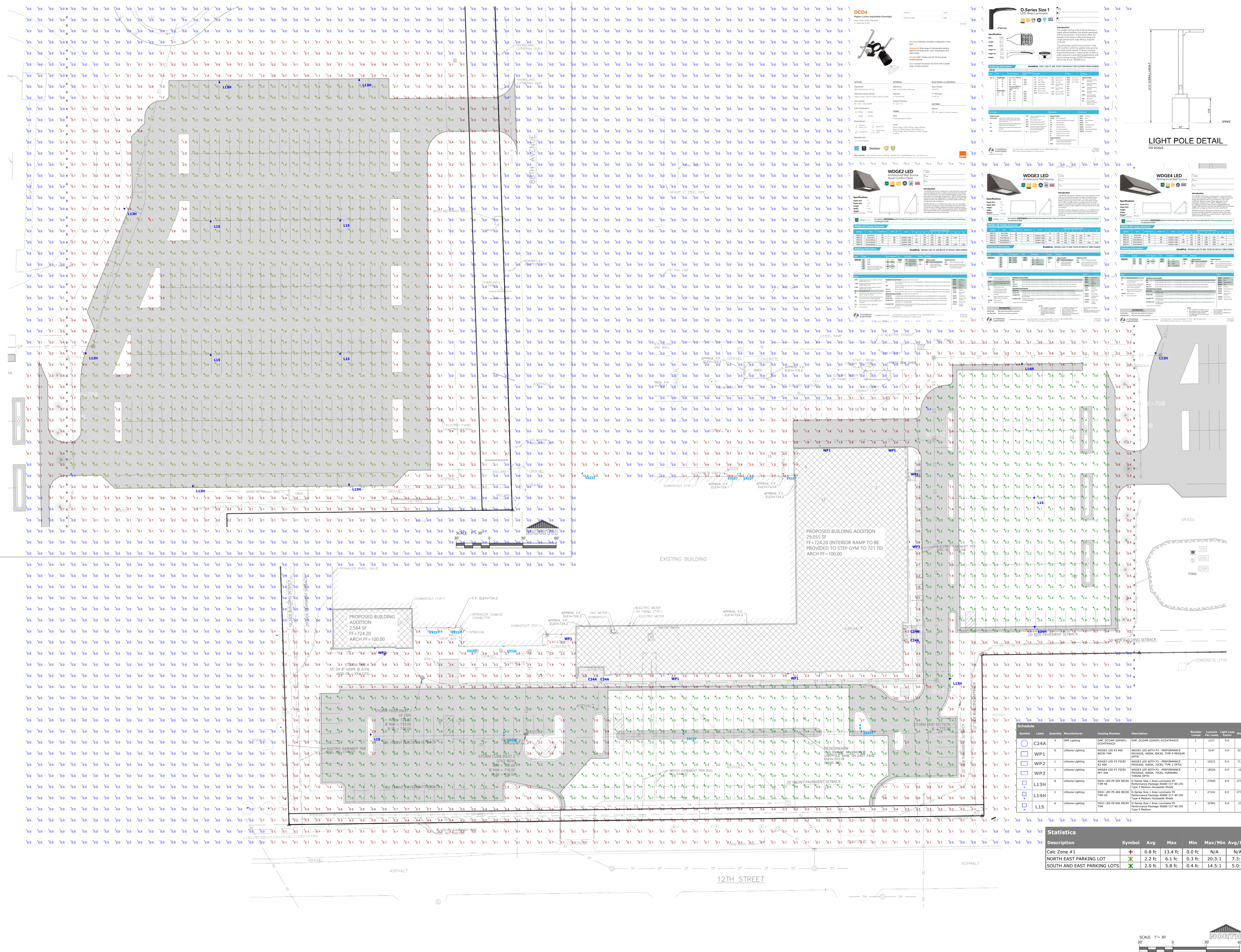
NOT FOR CONSTRUCTION

**JOB NUMBER**

240018700

**SHEET NUMBER**

C3.1



# Stormwater & Erosion Control Memo For: Shoreland Lutheran High School Somers, Wisconsin

Aug 30, 2024



## Introduction

The proposed development includes an addition to the existing high school and a parking lot expansion. The project is in the village of Somers. The project site is located on the corner of 12<sup>th</sup> Street and 88<sup>th</sup> Avenue. The proposed project will include a proposed 29,055 sf gymnasium and classroom addition, a 2,564 sf choir addition, parking lot modifications, and a parking lot expansion to the east. The proposed site is 55.74 acres, and the project will disturb 5.40 acres.

## Stormwater Management Requirements

Stormwater management for the proposed project and future site improvements is currently under review. A proposed regenerative stormwater conveyance system (RSC) has been designed and proposed to be installed along the north side of the site. Stormwater modeling for the RSC was done by Aqualis and construction drawings were completed by Excel Engineering. See Aqualis stormwater management plan dated May 17, 2024, and Excel Engineering stormwater and mass grading plans dated June 11, 2024.

## Storm Sewer Design

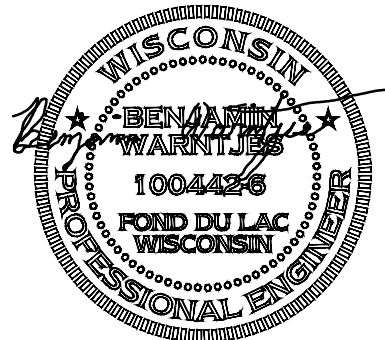
All storm sewers have been designed using DSPS plumbing code section 382.36(5). See attachments for pipe drainage areas and pipe sizing calculations.

## Erosion Control

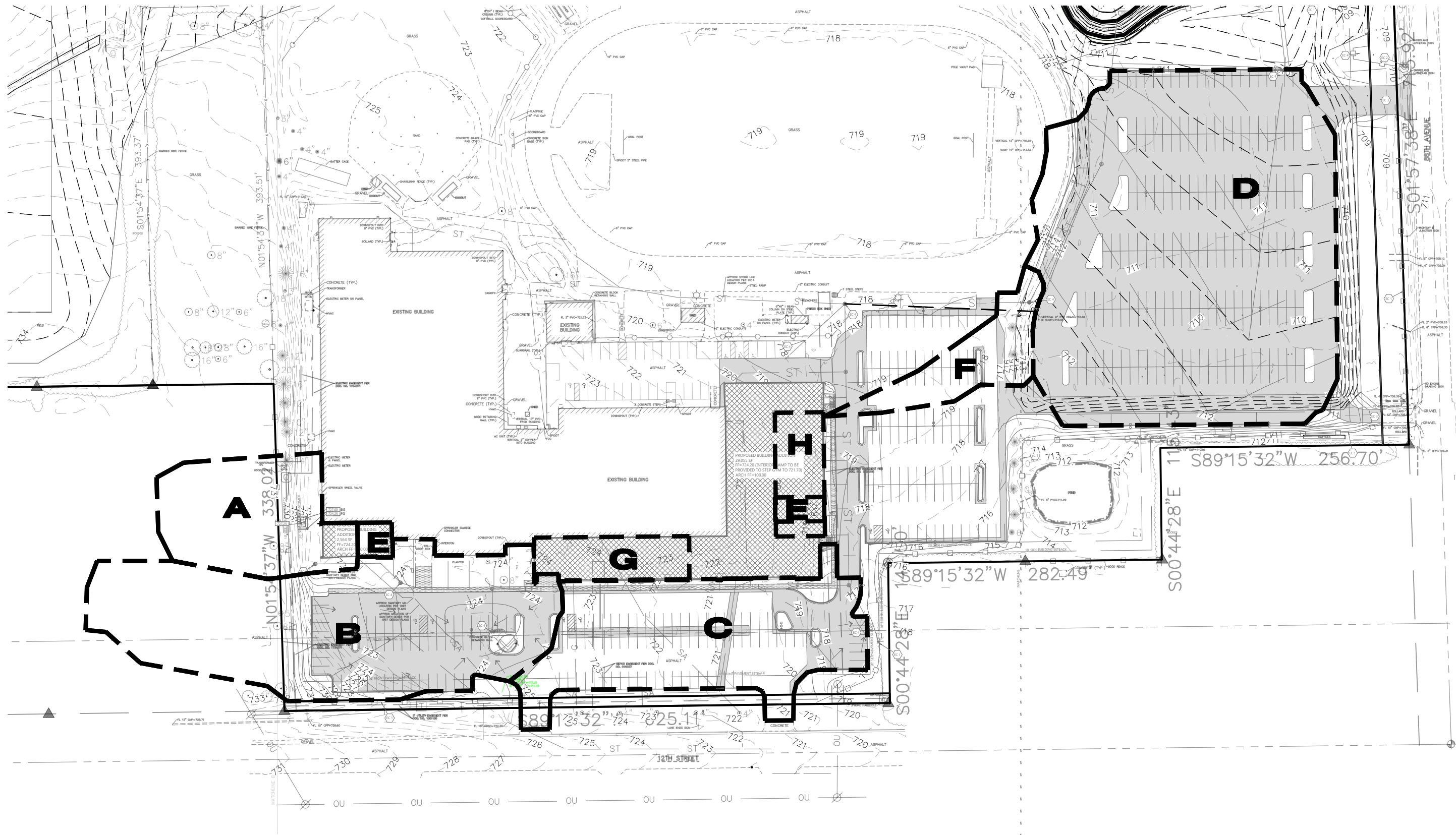
The erosion control specifications, site stabilization notes, dewatering notes, and post construction and maintenance plan will be included on sheet C0.1 of the construction plan set. See sheet L1.3 for seeding notes.

## Attachments

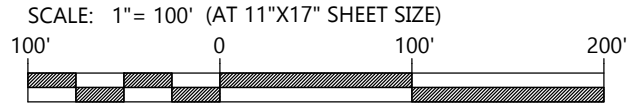
Storm Sewer Basin Map  
DSPS Peak Discharge Calculations  
Storm Sewer Spreadsheet



# Storm Sewer Basin Map



# STORM SEWER BASIN MAP



# DSPS Peak Discharge Calculations

BASIN ID	PERCENT IMPERVIOUS	TOTAL (S.F.)	TOTAL (AC.)	IMP TOTAL (S.F.)	IMP TOTAL (AC.)	BUILDING (S.F.)	BUILDING (AC.)	DSPS (GPM)	DSPS (CFS)
A	100.00%	21,100	0.48	21,100	0.48	21,100	0.48	812	1.81
B	100.00%	59,200	1.36	59,200	1.36	59,200	1.36	2,277	5.07
C	100.00%	38,800	0.89	38,800	0.89	38,800	0.89	1,492	3.33
D	100.00%	107,300	2.46	107,300	2.46	107,300	2.46	4,127	9.20
E	100.00%	1,300	0.03	1,300	0.03	1,300	0.03	50	0.11
F	100.00%	10,600	0.24	10,600	0.24	10,600	0.24	408	0.91
G	100.00%	7,100	0.16	7,100	0.16	7,100	0.16	273	0.61
H	100.00%	6,500	0.15	6,500	0.15	6,500	0.15	250	0.56
EX1	100.00%	81,900	1.88	81,900	1.88	81,900	1.88	3,150	7.02

# Storm Sewer Spreadsheet

Pipe Data					Pipe Capacity DSPS				
Pipe ID	Diameter (FT)	Pipe Qty.	Slope (FT/FT)	Manning's n	Basin ID	Total Flow (cfs)	Total Flow (gpm)	Full Flow Capacity (cfs)	Full Flow Capacity (gpm)
A	1	1	0.010	0.012	A,E	1.92	862	3.87	1,737
B	1.5	1	0.010	0.012	B	5.07	2,277	11.41	5,121
C	2	1	0.009	0.012	A,B,E,G,G	8.21	3,685	23.31	10,463
D	2	1	0.009	0.012	A,B,C,H,G,G	11.98	5,377	23.31	10,463
E	1.25	1	0.010	0.012	C	3.33	1,492	7.02	3,149
F	2	1	0.009	0.012	A,B,C,H,G,G,E,E,E	12.31	5,527	23.31	10,463
G	2.5	1	0.010	0.012	A,B,C,H,G,G,E,E,E,EX1,EX1	26.35	11,827	44.55	19,996
H	2	1	0.020	0.012	A,B,C,H,G,G,H,EX1,EX1,F	27.48	12,335	34.75	15,597
I	1.5	1	0.010	0.012	D	9.20	4,127	11.41	5,121
J	0.5	1	0.010	0.012	E	0.11	50	0.61	274
EX1	1.25	1	0.010	0.012				7.02	3,149

Full Flow Capacity based off Manning's Equation

$$Q = \frac{1.49}{n} R^{2/3} S^{1/2} a$$

Where:

- Q = Full Flow Capacity of Pipe (cfs)
- n = manning's roughness coefficient
- R = hydraulic radius (ft) (D/4)
- s = hydraulic gradient, slope (ft/ft)
- a = flow area (sq. ft.)

Typical Manning's n

- HDPE 0.012
- PVC 0.012
- Concrete 0.013
- CMP 0.024

\*Total Flow calculated via TR-55 hydrologic calculations. Reference Storm Pipe Basin Map & TR-55 Calculations

**PROJECT INFORMATION**

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9026 12TH STREET • KENOSHA, WI 53144

PROFESSIONAL SEAL

**PRELIMINARY DATES**

AUG. 30, 2024

**JOB NUMBER**  
240018700

**SHEET NUMBER**

**A1.1**

NOT FOR CONSTRUCTION

**LAB CLASSROOM**  
1,582 S.F.  
(REMODEL)

**MUSIC ADDITION**  
**TOTAL AREA**  
2,548 S.F.  
(CONVENTIONAL STEEL)  
(LOAD BEARING CMU)

**FITNESS CENTER**  
3,179 S.F.  
(CONVENTIONAL STEEL)  
(METAL STUD WALLS)

**NEW GYMNASIUM**  
12,903 S.F.  
PRECAST WALLS w/  
PREMANUFACTURED METAL  
BUILDING STRUCTURE  
(PEMB)

**CLASSROOM ADDITION**  
12,939 S.F.  
(CONVENTIONAL STEEL)  
(LOAD BEARING PRECAST)

**EXISTING GYMNASIUM REMODEL**

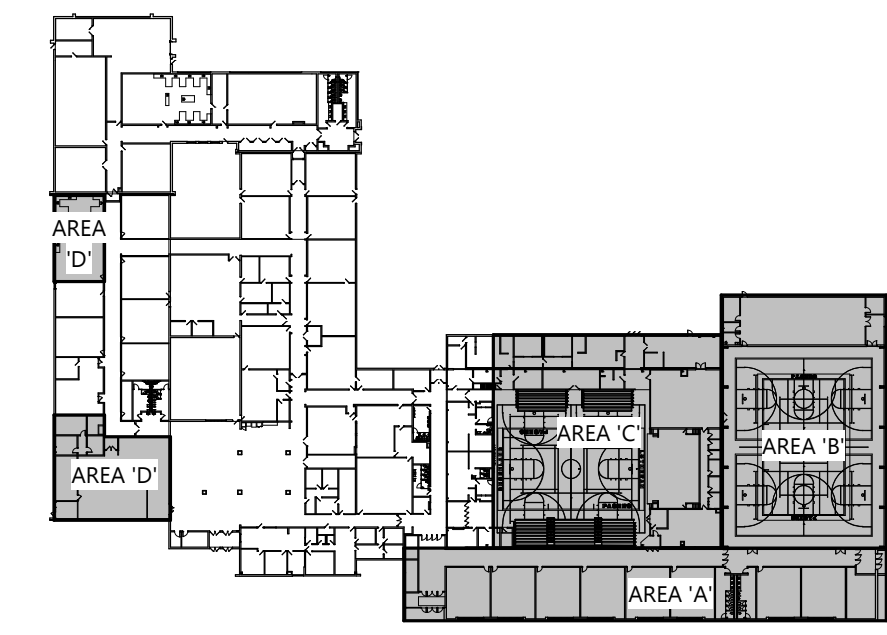
- 9,465 S.F.
- NEW BLEACHERS
  - PAINT WALLS AND PEMB FRAMES
  - REFINISH WOOD FLOOR
  - CEILING IMPROVEMENT
  - OPTION A: THERMAL DESIGN - SYSEAL FP FABRIC
  - OPTION B: THERMAL DESIGN - SYSEAL FP FABRIC WITH FIBERGLASS INSULATION
  - OPTION C: THERMAL DESIGN - SYSEAL FP FABRIC WITH FIBERGLASS INSULATION & ARMSTRONG TECTUM FINALE CEILING PANELS INSTALLED AT 50% OF THE GYM CEILING AREA



**OVERALL FIRST FLOOR PLAN**

SCALE: 1/16" = 1'-0"

**BUILDING ADDITION TOTAL AREA**  
31,569 S.F.



**KEY PLAN**

ARCHITECTURAL OVERALL FIRST FLOOR PLAN

**PROJECT INFORMATION**

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PROFESSIONAL SEAL

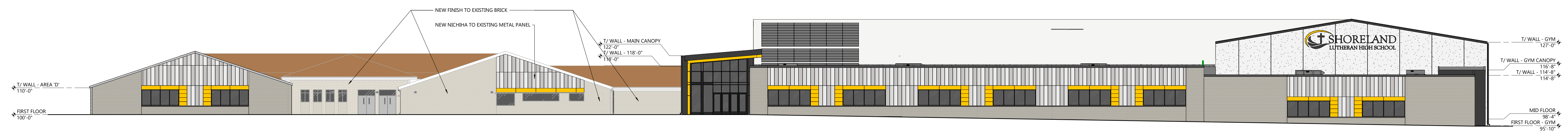
**PRELIMINARY DATES**  
 AUG. 30, 2024

**NOT FOR CONSTRUCTION**

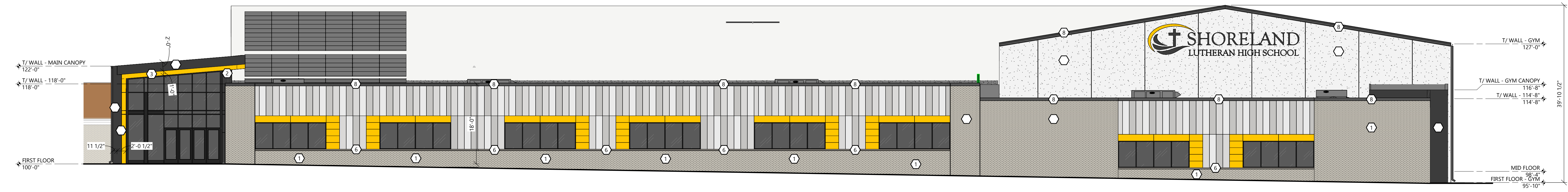
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**SHEET NUMBER**

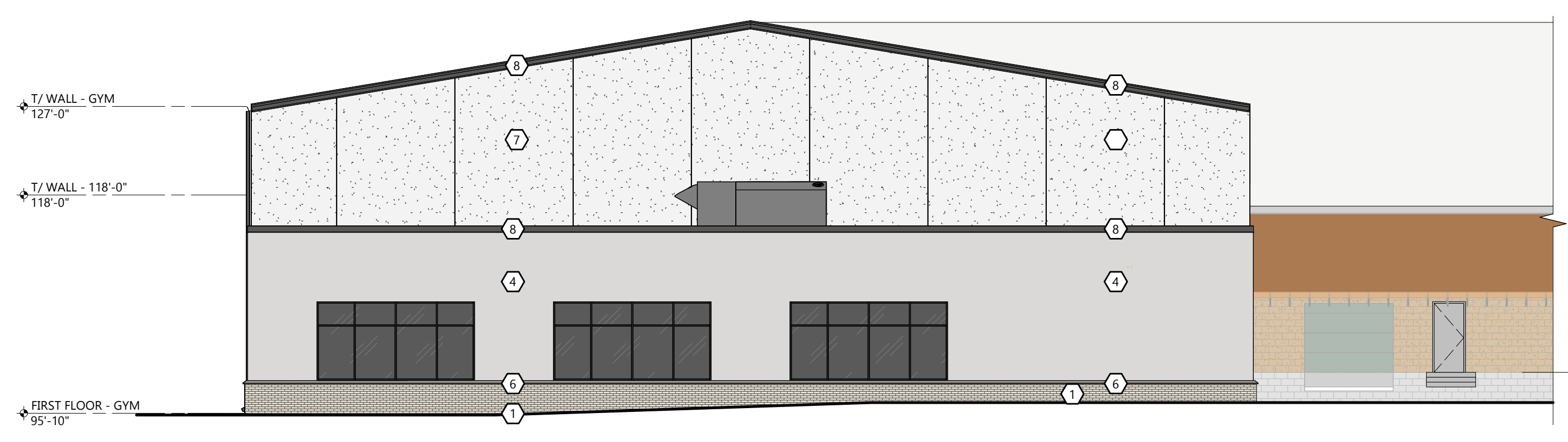
**A2.0**



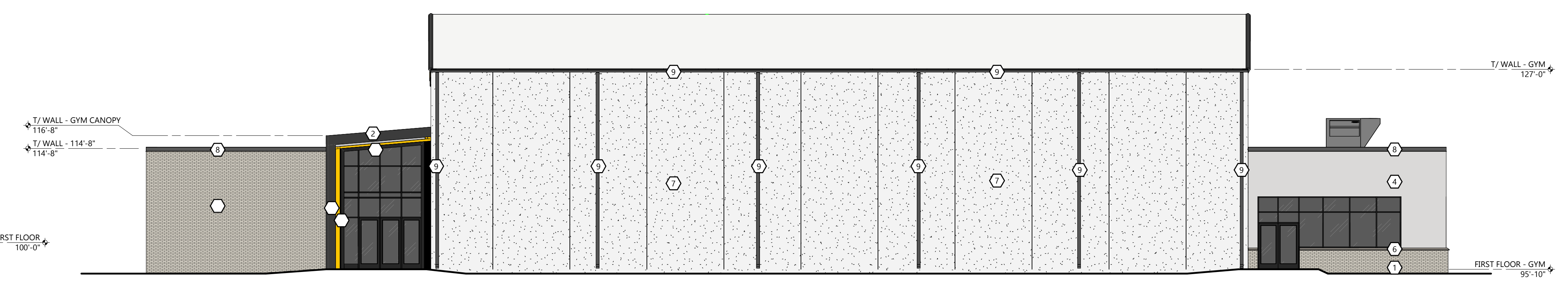
**OVERALL SOUTH ELEVATION**  
 SCALE: 1/16" = 1'-0"



**SOUTH ELEVATION**  
 SCALE: 3/32" = 1'-0"



**NORTH ELEVATION**  
 SCALE: 3/32" = 1'-0"



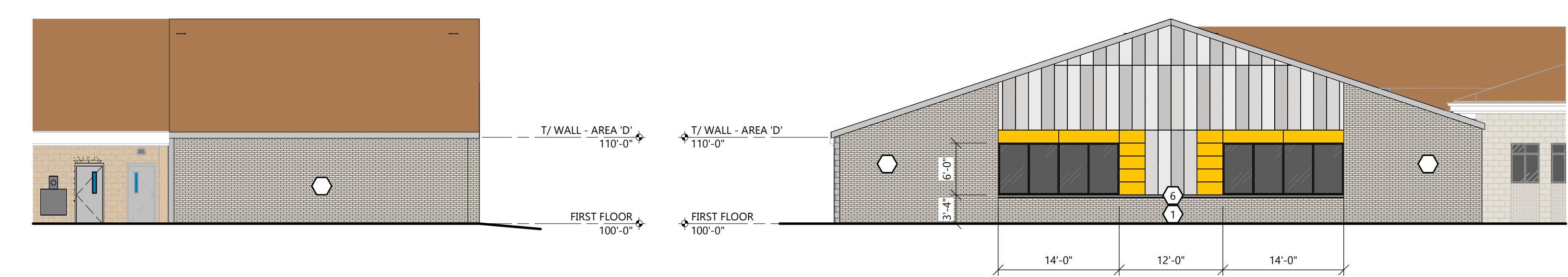
**EAST ELEVATION**  
 SCALE: 3/32" = 1'-0"

**GENERAL NOTES**

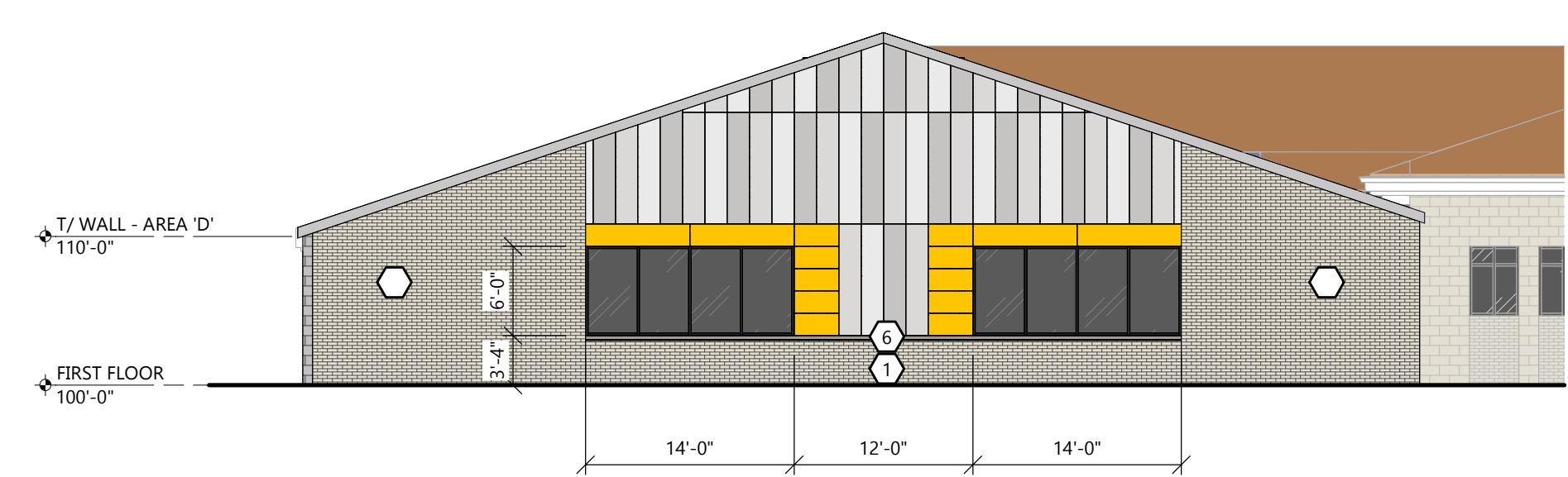
- SEE WALL SECTIONS FOR EXTERIOR WALL CONSTRUCTION
- REFERENCE EXTERIOR ELEVATIONS FOR VENEER CONTROL JOINTS. SEE STRUCTURAL PLANS FOR CMU CONTROL JOINT LOCATIONS. VENEER CONTROL JOINTS AND CMU CONTROL JOINTS ARE NOT REQUIRED TO ALIGN.

**EXTERIOR MATERIAL KEY**

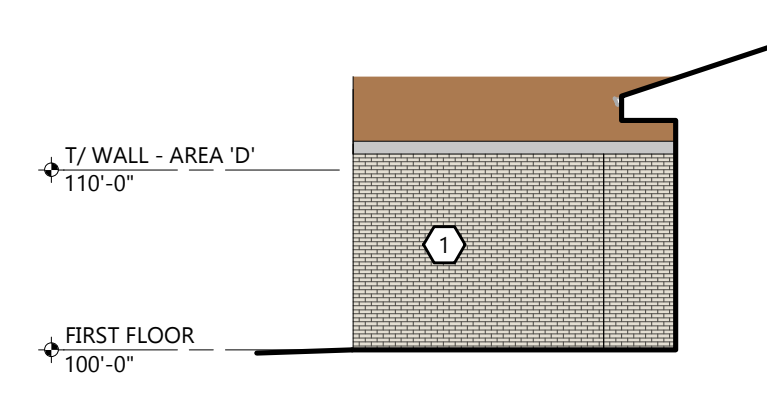
1	<b>BRICK VENEER</b> MFR: T.B.D. PRODUCT: T.B.D. COLOR: SEE RENDERING
2	<b>ALUMINUM COMPOSITE MATERIALS</b> MFR: MUZA PRODUCT: MZ-2000 COLOR: BLACK
3	<b>ALUMINUM COMPOSITE MATERIALS</b> MFR: MUZA PRODUCT: MZ-2000 COLOR: YELLOW
4	<b>FIBER CEMENT PANELS</b> MFR: NICHHA PRODUCT: ILLUMINATION DESIGN SERIES (AWP 3030) VERTICAL COLOR: LIGHT GRAY, DARK GRAY, WHITE PATTERN
5	<b>FIBER CEMENT PANELS</b> MFR: NICHHA PRODUCT: ILLUMINATION DESIGN SERIES (AWP 3030) HORIZONTAL COLOR: YELLOW
6	<b>PRECAST SILL</b> MFR: T.B.D. PROFILE: SEE DETAILS COLOR: SEE RENDERING
7	<b>PRECAST WALL PANEL</b> MFR: T.B.D. FINISH: PAINTED COLOR: WHITE
8	<b>METAL COPING / FLASHING</b> MFR: PAC-CLAD PROFILE: SEE DETAILS COLOR: BLACK
9	<b>GUTTERS / DOWNSPOUTS</b> MFR: T.B.D. PROFILE: SEE ROOF PLAN COLOR: BLACK



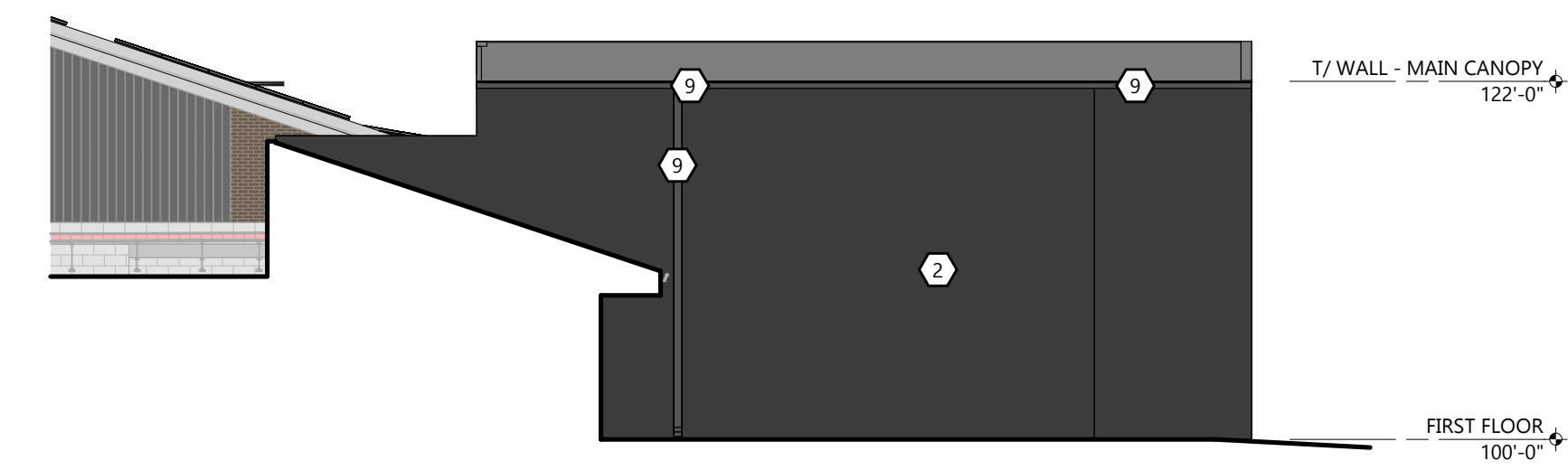
**WEST ELEVATION - AREA 'D'**  
 SCALE: 3/32" = 1'-0"



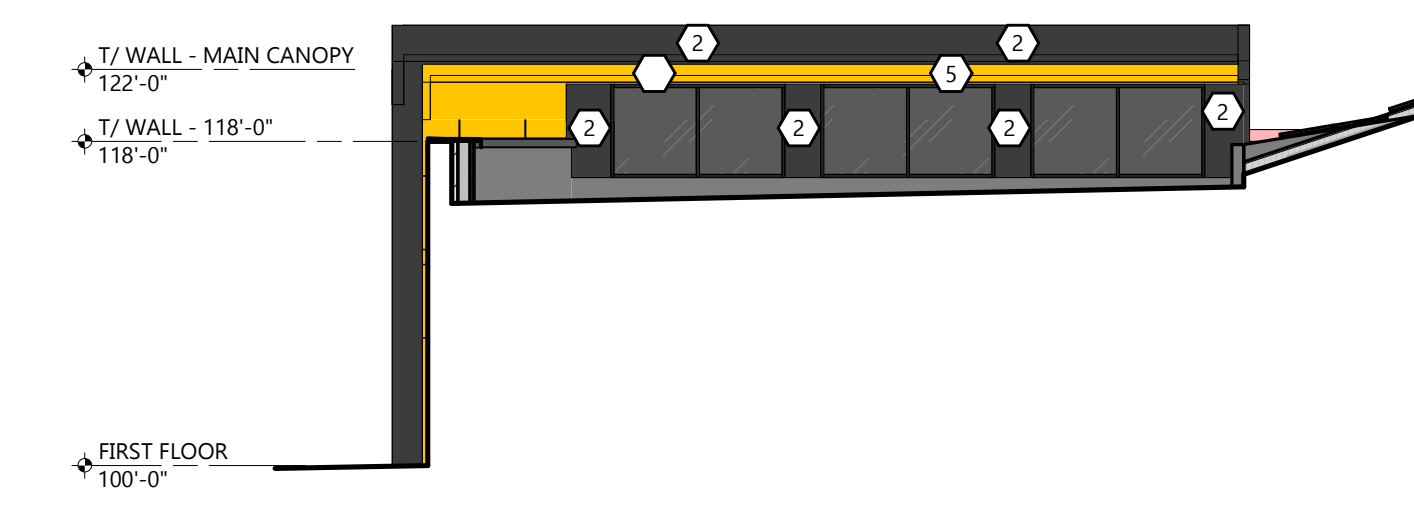
**SOUTH ELEVATION - AREA 'D'**  
 SCALE: 3/32" = 1'-0"



**EAST ELEVATION - AREA 'D'**  
 SCALE: 3/32" = 1'-0"



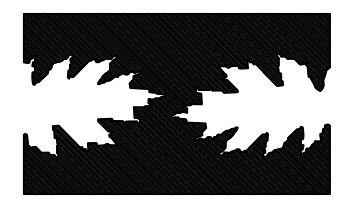
**WEST ELEVATION - ENTRY**  
 SCALE: 3/32" = 1'-0"



**EAST ELEVATION - ENTRY**  
 SCALE: 3/32" = 1'-0"

**DIGGERS HOTLINE**  
 TO OBTAIN LOCATIONS OF PARTICIPANTS UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN  
 CALL DIGGERS HOTLINE  
 811 OR 1-800-442-8811  
 MILWAUKEE AREA 226-1181  
 MS. STATUTE 19.10(7)(10) & (11)  
 REQUIRES MIN. 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE

**HELLER & ASSOCIATES, LLC**  
 LANDSCAPE ARCHITECTURE  
 P.O. Box 1359  
 Lake Geneva, Wisconsin 53147-1359  
 ph 262.639.9733  
 david@wdavidheller.com  
 www.wdavidheller.com



PROJECT

# SHORELAND LUTHERAN HIGH SCHOOL

9026 12th Street  
 Somers, WI 53144

ISSUANCE AND REVISIONS

DATE	DESCRIPTION
8.27.24	FIRST ISSUE

Information contained herein is based on Survey Information, Field Inspection, and believed to be accurate.

SHEET TITLE

# OVERALL LANDSCAPE PLAN

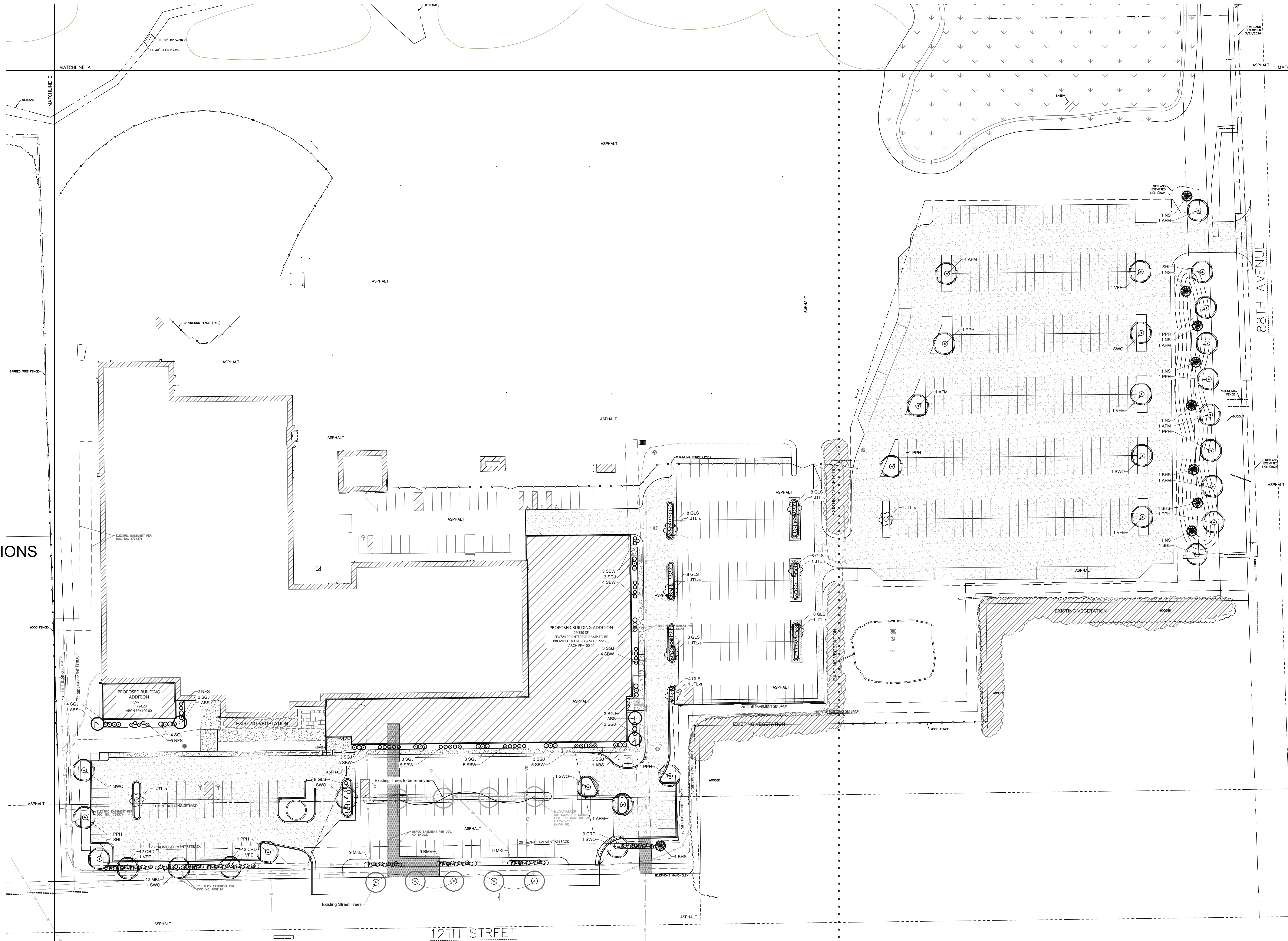
PROJECT MANAGER WDH

PROJECT NUMBER 24-042

DATE 08.27.24

SHEET NUMBER

# L 1.0

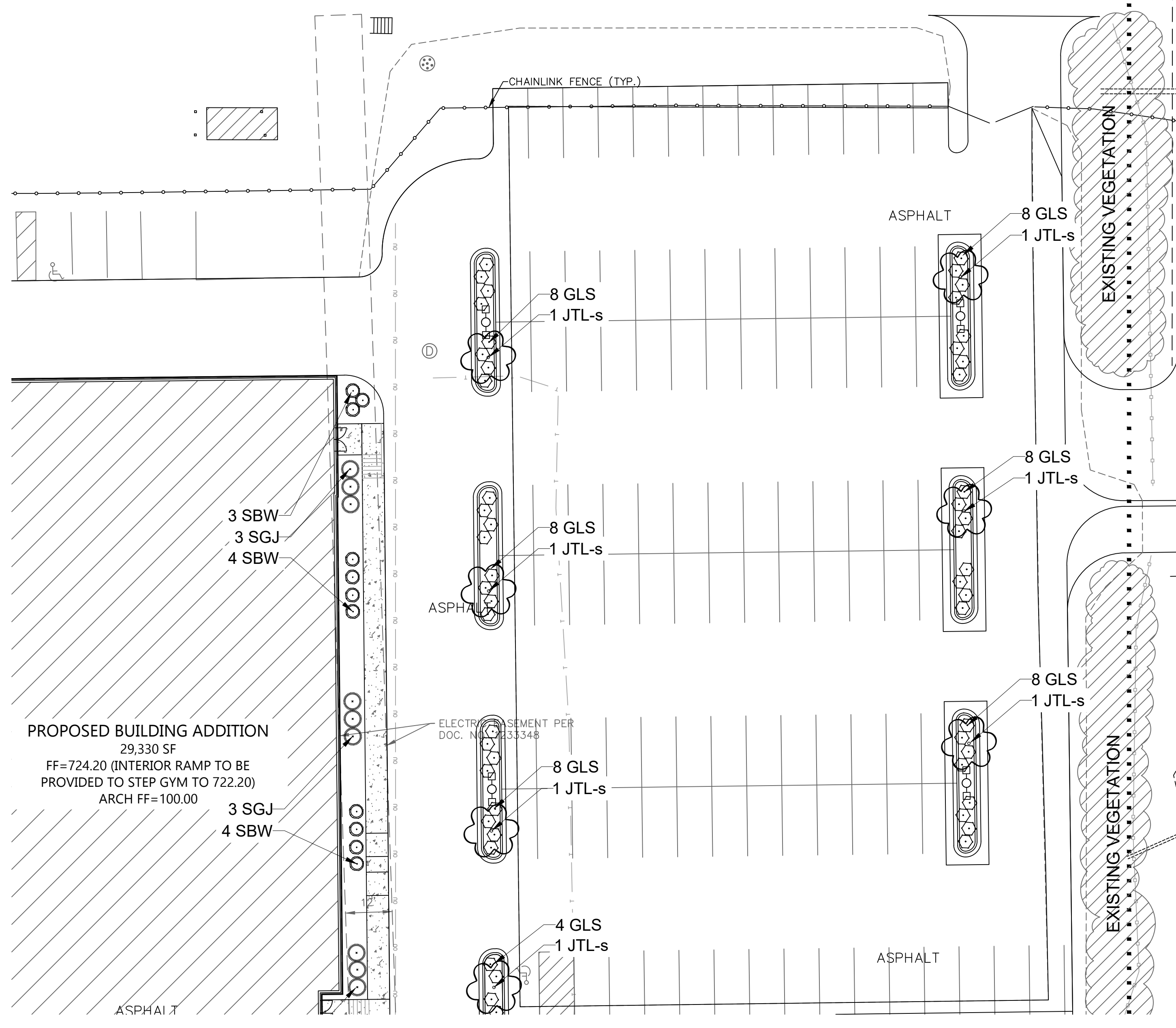


PLANT ABBREVIATIONS

**OVERALL LANDSCAPE PLAN**  
 Scale: 1" = 40'0"  
 NORTH



**PLANT ABBREVIATIONS**



**HELLER & ASSOCIATES, LLC**  
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 ph 262.639.9733  
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**PROJECT**  
**SHORELAND LUTHERAN HIGH SCHOOL**  
 9026 12th Street  
 Somers, WI 53144

**ISSUANCE AND REVISIONS**

DATE	DESCRIPTION
8.27.24	FIRST ISSUE

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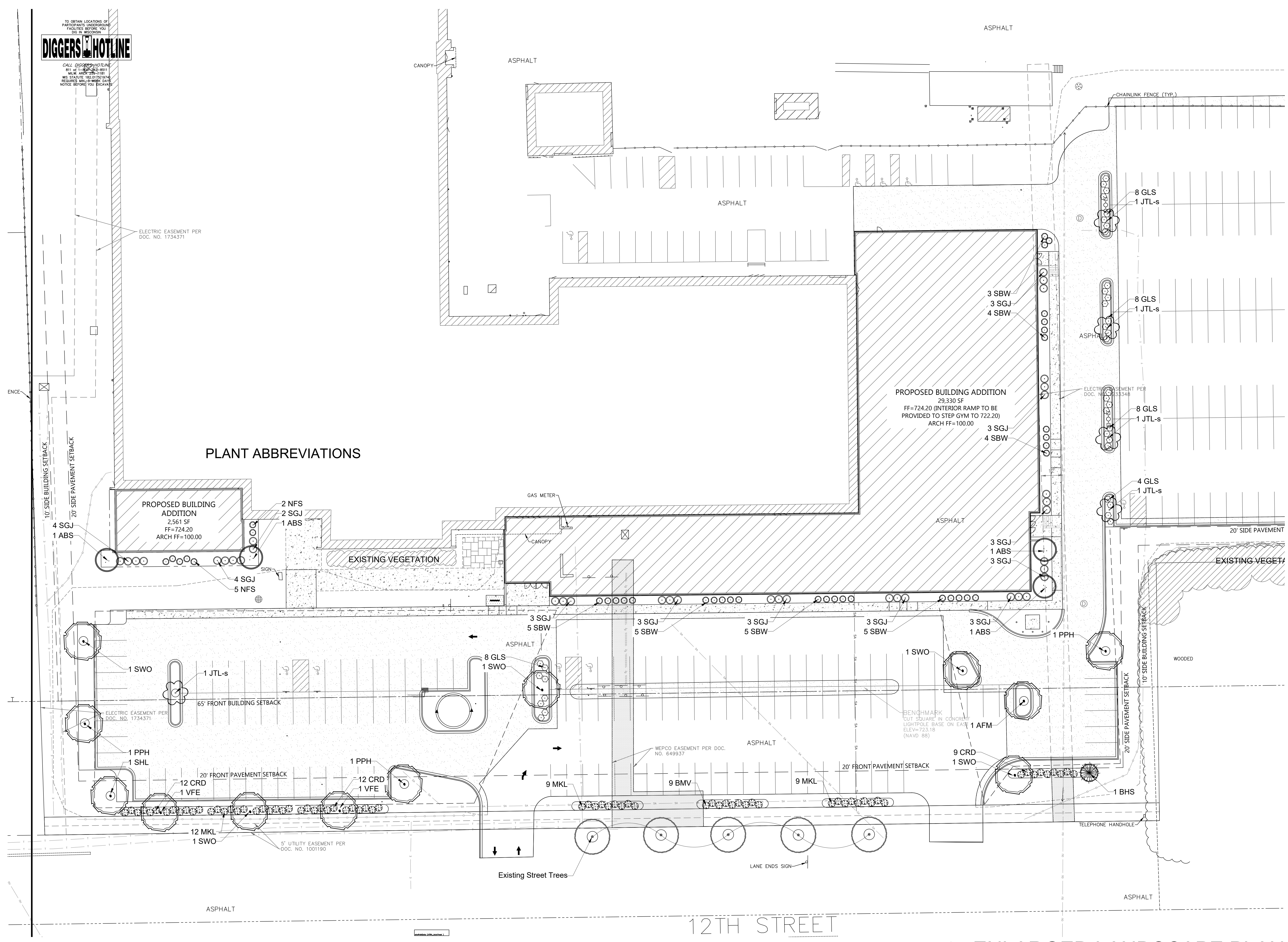
**SHEET TITLE**  
**ENLARGED LANDSCAPE PLAN**

PROJECT MANAGER	WDH
PROJECT NUMBER	24-042
DATE	08.27.24
SHEET NUMBER	

**L 1.1**

**ENLARGED LANDSCAPE PLAN**  
 Scale: 1" = 20'0"  
 NORTH  
 0 10 20 40

**DIGGERS HOTLINE**  
 CALL DIGGERS HOTLINE  
 811 - 800-4-A-HEAD  
 MON-FRI 8AM-5PM  
 WE 9AM-12PM  
 SA 10AM-5PM  
 REQUIRES MIN. 7-WORK DAY  
 NOTICE BEFORE YOU EXCAVATE



**PLANT ABBREVIATIONS**

**HELLER & ASSOCIATES, LLC**  
 LANDSCAPE ARCHITECTURE  
 P.O. Box 1359  
 Lake Geneva, Wisconsin 53147-1359  
 ph 262.639.9733  
 david@wdavidheller.com  
 www.wdavidheller.com

**PROJECT**

**SHORELAND LUTHERAN HIGH SCHOOL**

9026 12th Street  
 Somers, WI 53144

**ISSUANCE AND REVISIONS**

DATE	DESCRIPTION
8.27.24	FIRST ISSUE

**SHEET TITLE**

**ENLARGED LANDSCAPE PLAN**

PROJECT MANAGER **WDH**

PROJECT NUMBER **24-042**

DATE **08.27.24**

SHEET NUMBER

**L 1.2**

**ENLARGED LANDSCAPE PLAN**  
 Scale: 1" = 20'0"  
 NORTH  
 0 10 20 40



- Contractor responsible for contacting Diggers Hotline (811 or 800-242-8511) to have site marked prior to excavation or planting.
- Contractor to verify all plant quantities shown on Plant & Material List and landscape planting symbols and report any discrepancies to Landscape Architect or General Contractor.
- All plantings shall comply with standards as described in American Standard of Nursery Stock - Z60.1 ANSI (latest version). Landscape Architect reserves the right to inspect, and potentially reject any plants that are inferior, compromised, undersized, diseased, improperly transported, installed incorrectly or damaged. No sub-standard "B Grade" or "Park Grade" plant material shall be accepted. Plant material shall originate from nursery(ies) with a similar climate as the planting site.
- Any potential plant substitutions must be approved by Landscape Architect or Owner. All plants must be installed as per sizes indicated on Plant & Material Schedule, unless approved by Landscape Architect. Any changes to sizes shown on plan must be submitted in writing to the Landscape Architect prior to installation.
- Topsoil in Parking Lot Islands (if applicable): All parking lot islands to be backfilled with topsoil to a minimum depth of 12" to insure long-term plant health. Topsoil should be placed within 3" of finish grade by General Contractor / Excavation Contractor during rough grading operations/activity. The landscape contractor shall be responsible for the fine grading of all disturbed areas, planting bed areas, and lawn areas. Crown all parking lot islands a minimum of 4" to provide proper drainage, unless otherwise specified.
- Tree Planting: Plant all trees slightly higher than finished grade at the root flare. Remove excess soil from the top of the root ball, if needed. Remove and discard non-biodegradable ball wrapping and support wire. Removed biodegradable burlap and wire cage (if present) from the top 1/3 of the rootball and carefully bend remaining wire down to the bottom of the hole. Once the tree has been placed into the hole and will no longer be moved, score the remaining 2/3 of the burlap and remove the twine. Provide one slow release fertilizer packets (per 1" caliper) for each tree planted.
- Tree Planting: Backfill tree planting holes 80% existing soils removed from excavation and 20% Soil Amendments (see Note 11). Avoid air pockets and do not tamp soil down. Discard any gravel, rocks, heavy clay, or concrete pieces. When hole is full, trees shall be watered thoroughly, and water left to soak in before proceeding to fill the remainder of the hole. Water again to full soak in the new planting. Each tree shall receive a 3" deep, 4-5' diameter (see planting details or planting plan) shredded hardwood bark mulch ring / saucer around all trees. Do not build up any mulch onto the trunk of any tree. Trees that are installed incorrectly will be replaced at the time and expense of the Landscape Contractor.
- Shrub Planting: All shrubs to be planted in groupings as indicated on the Landscape Plan. Install with the planting of shrubs a 50/50 mix of Soil Amendments with blended, pulverized topsoil. Install topsoil into all plant beds as needed to achieve proper grade and displace undesirable soils (see planting detail). Remove all excessive gravel, clay and stones from plant beds prior to planting. When hole(s) are 2/3 full, shrubs shall be watered thoroughly, and water left to soak in before proceeding. Provide slow-release fertilizer packets at the rate of 1 per 24" height/diameter of shrub at planting.
- Mulching: All tree rings to receive a 3" deep layer of high quality shredded hardwood bark mulch (not pigment dyed or enviro-mulch). All shrub planting and perennial planting bed areas (groupings) shall receive a 2" layer of double-shredded hardwood bark mulch, and groundcover areas a 2" layer of the same mulch. Do not mulch annual flower beds (if applicable). Do not allow mulch to contact plant stems and tree trunks.
- Edging: All planting beds shall be edged with a 4" deep spade edge using a flat landscape spade or a mechanical edger. Bedlines are to be cut crisp, smooth as per plan. A clean definition between landscape beds and lawn is required. Pack mulch against lawn edge to hold in place.
- Plant bed preparation/Soil Amendment composition: All perennial, groundcover and annual areas (if applicable) are required to receive a blend of organic soil (Soil Amendments) amendments prior to installation. Roto-till the following materials at the following ratio, into existing soil beds or installed topsoil beds to a depth of approximately 8"-10". Containerized and balled & burlapped plant material should be back-filled with amended soil:
  - Per 100 SF of bed area (Soil Amendment composition):
    - 3/4 CY Peat Moss or Mushroom Compost
    - 3/4 CY blended/pulverized Topsoil
    - 1/4 CY composted manure
  - In roto-tilled beds only, also include in above mixture:
    - 2 lbs Starter Fertilizer

Per 100 SF of bed area (Soil Amendment composition):

- 3/4 CY Peat Moss or Mushroom Compost
- 3/4 CY blended/pulverized Topsoil
- 1/4 CY composted manure

In roto-tilled beds only, also include in above mixture:  
2 lbs Starter Fertilizer

- Installation preparation for all seeded areas: remove/kill off any existing unwanted vegetation prior to seeding. Prepare the topsoil (if adequate or provide as in item #6 above) and seed bed by removing all surface stones 1" or larger. Apply a starter fertilizer (20-10-5, or approved comparable) and specified seed uniformly at the specified rate, and provide mulch covering suitable to germinate and establish turf. Provide seed and fertilizer specifications to Landscape Architect and Owner prior to installation. Erosion control measures are to be used in swales and on slopes in excess of 1:3 and where applicable (see Civil Engineering Drawings). Methods of installation may vary at the discretion of the Landscape Contractor on his/her responsibility to establish and guarantee a smooth, uniform, quality turf. A minimum of 2" of blended, prepared and non-compacted topsoil is required for all lawn areas. If straw mulch is used as a mulch covering, a tackifier may be necessary to avoid wind dispersal of mulch covering. Marsh hay containing reed canary grass is NOT acceptable as a mulch covering.

An acceptable quality seed installation is defined as having:

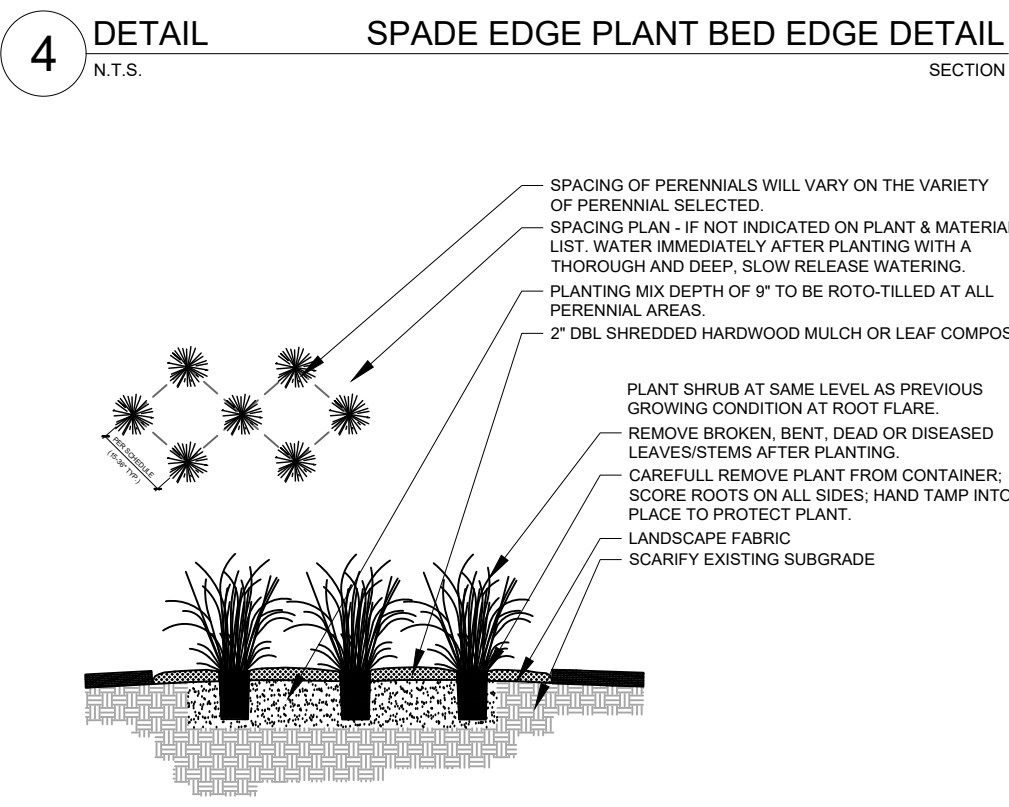
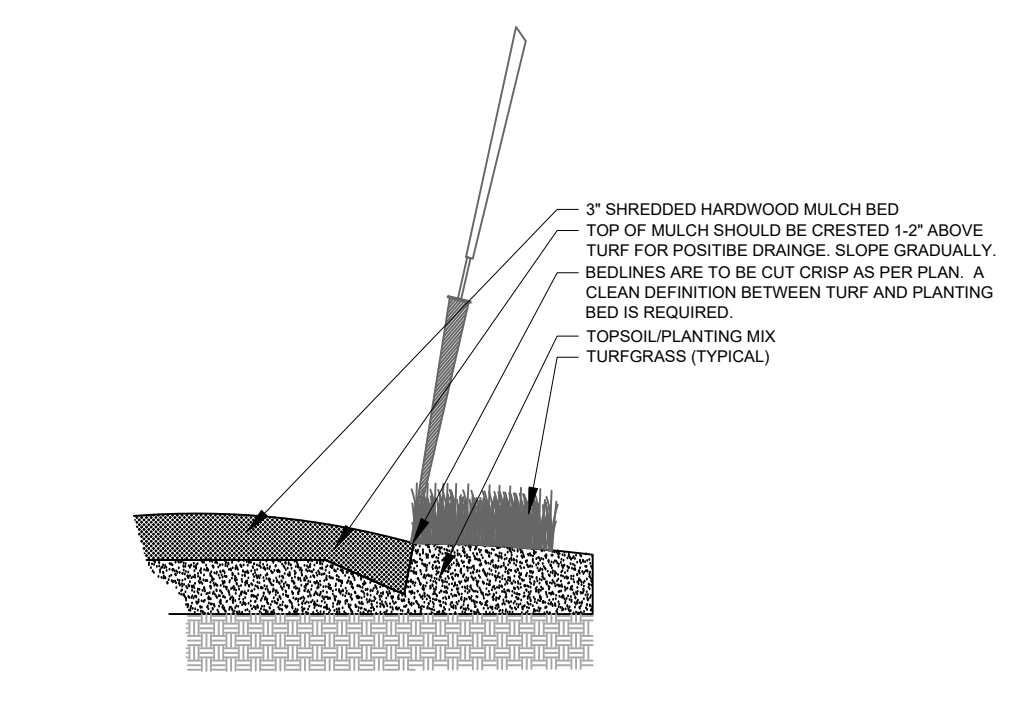
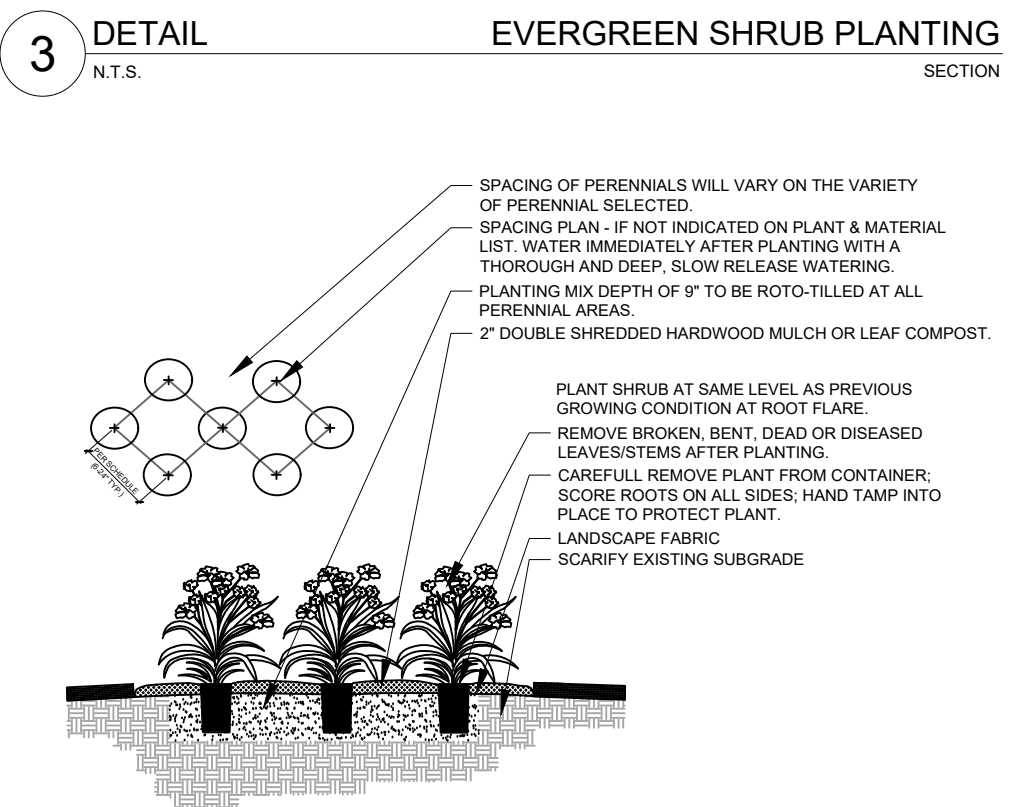
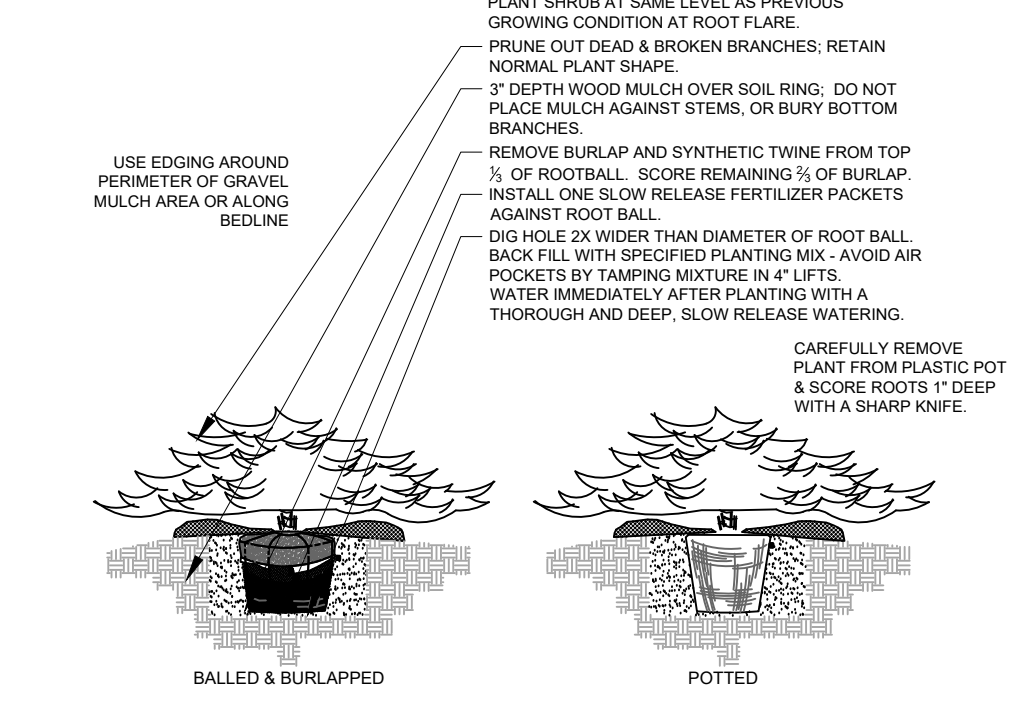
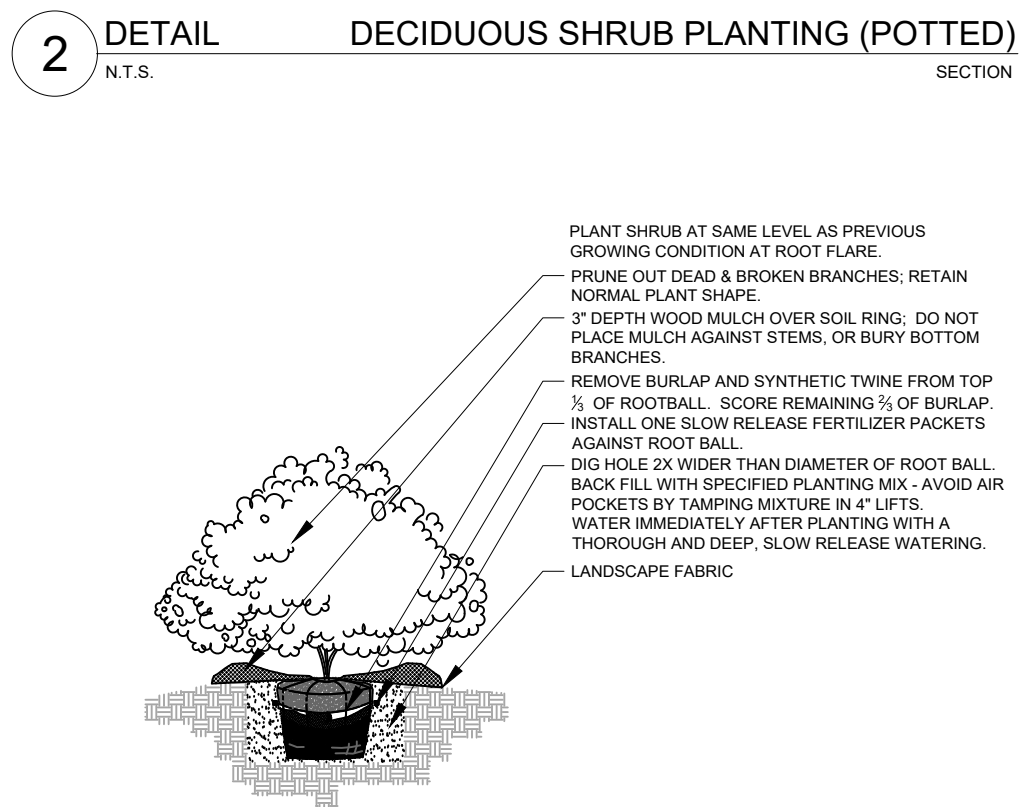
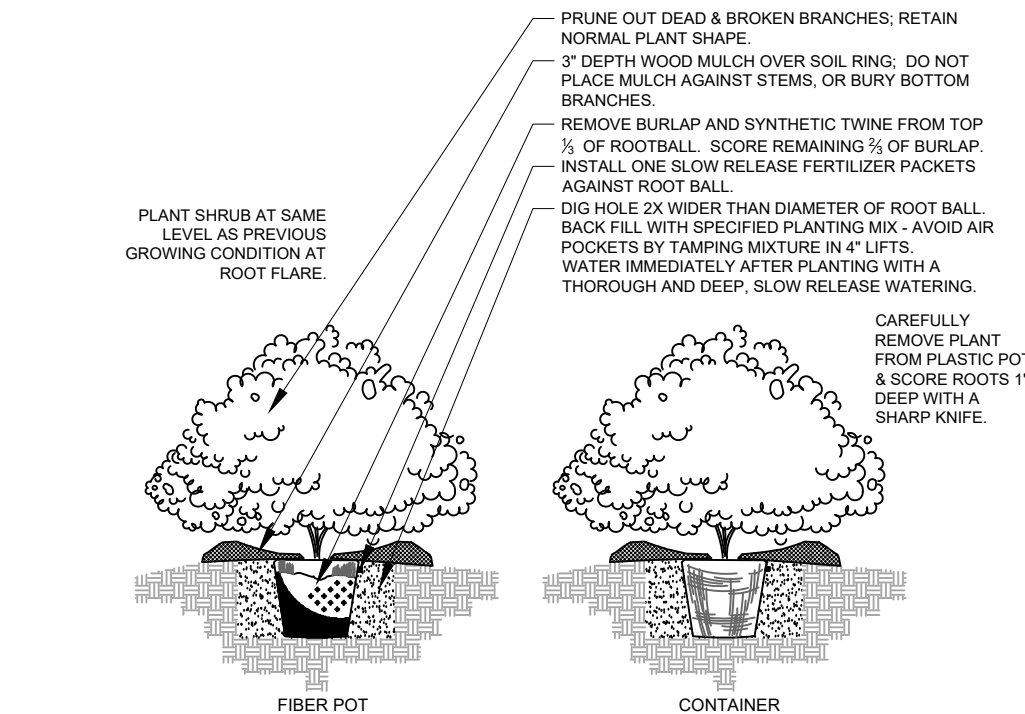
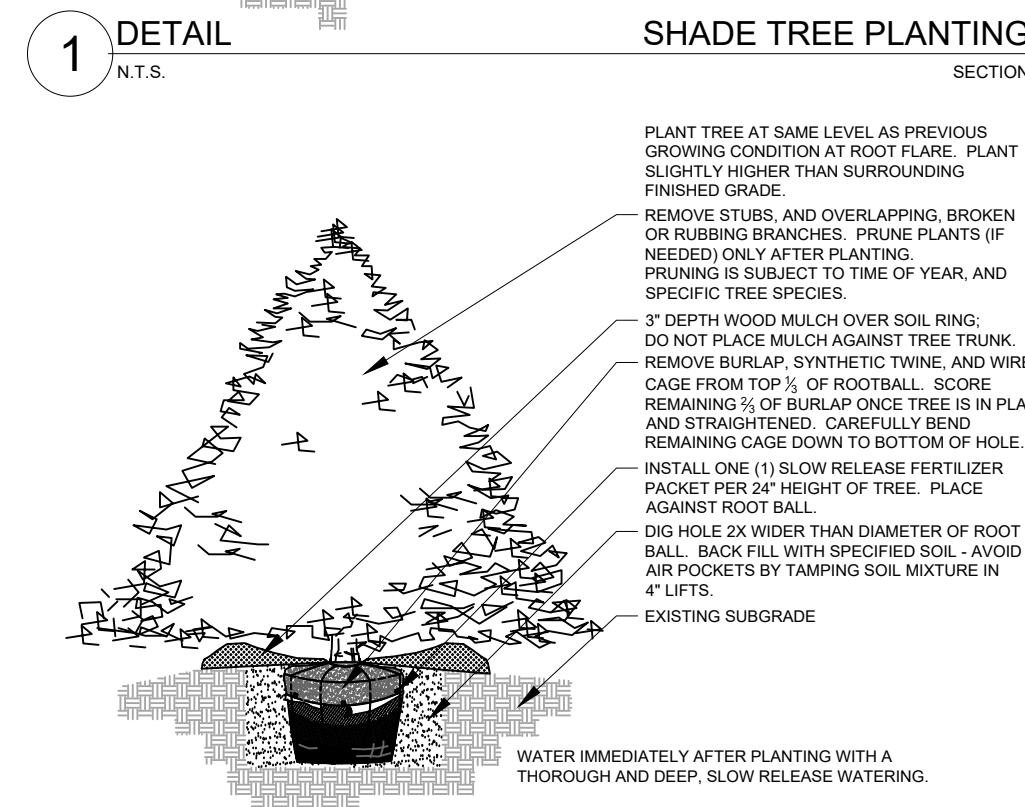
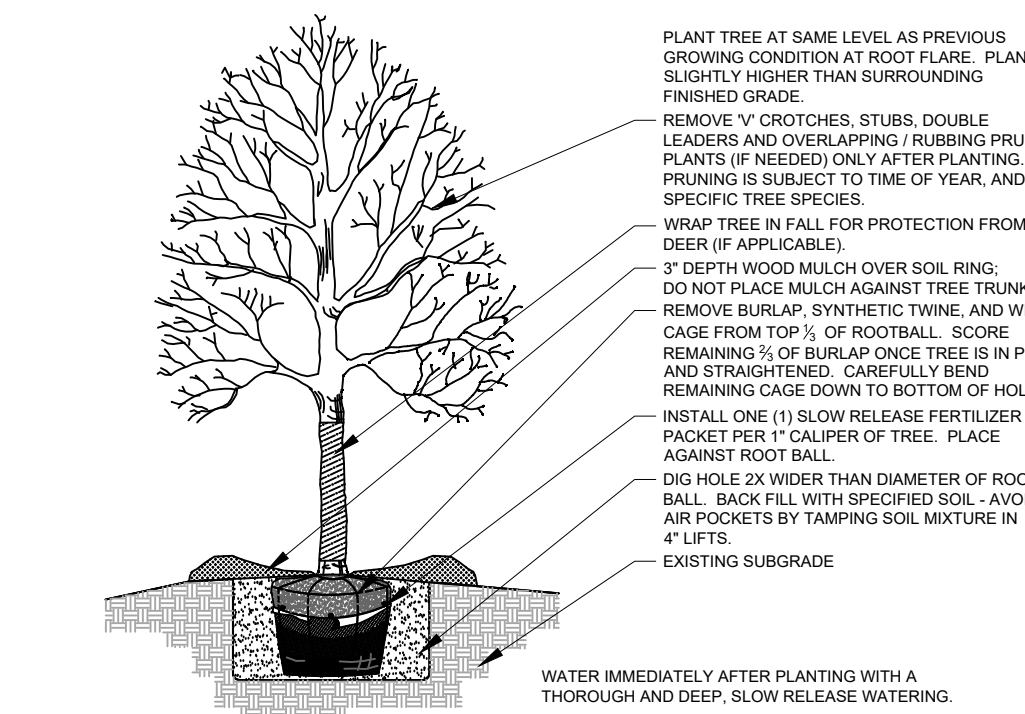
- No bare spots larger than one (1) square foot
- No more than 10% of the total area with bare areas larger than one (1) square foot
- A uniform coverage through all turf areas

- Warranty and Replacements: All plantings are to be watered thoroughly at the time of planting, through construction and upon completion of project as required. Trees, Evergreens, and Shrubs (deciduous and evergreen) shall be guaranteed (100% replacement) for a minimum of one (1) year from the date of project completion. Perennials, groundcovers, and ornamental grasses shall be guaranteed for a minimum of one (1) growing season. Perennials, groundcovers, and ornamental grasses planted after September 15th shall be guaranteed through May 31st of the following year. Only one replacement per plant will be required during the warranty period, except for losses or replacements due to failure to comply with specified requirements. Watering and general ongoing maintenance instructions are to be supplied by the Landscape Contractor to the Owner upon completion of the project.

- The Landscape Contractor is responsible for the watering and maintenance of all landscape areas for a period of 45 days after the substantial completion of the landscape installation. This shall include all trees, shrubs, evergreens, perennials, ornamental grasses, turf grass, no-mow grass, and native prairie seed mix / stormwater seed mix. Work also includes weeding, edging, mulching (only if required), fertilizing, trimming, sweeping up grass clippings, pruning and deadheading.

- Project Completion: Landscape Contractor is responsible to conduct a final review of the project, upon completion, with the Landscape Architect, Client or Owner / Client Representative, and the General Contractor to answer questions, provide written care instructions for new plantings and turf, and insure that all specifications have been met.

## LANDSCAPE GENERAL NOTES



## PLANTING & HARDSCAPE DETAILS



### PROJECT

# SHORELAND LUTHERAN HIGH SCHOOL

9026 12th Street  
Somers, WI 53144

### ISSUANCE AND REVISIONS

DATE	DESCRIPTION
8.27.24	FIRST ISSUE

Information contained herein is based on Survey Information, Field Inspection, and believed to be accurate.

## SHEET TITLE LANDSCAPE DETAILS, NOTES & SCHEDULES

PROJECT MANAGER WDH

PROJECT NUMBER 24-042

DATE 08.27.24

SHEET NUMBER

# L 1.3

# VILLAGE OF SOMERS

---

## Department of Planning and Development

### VILLAGE OF SOMERS SITE PLAN REVIEW PROCEDURES

---

1. Contact the Kenosha County Department of Planning & Development and check with staff to determine if your proposed zoning change meets the requirements of the Multi-Jurisdictional Comprehensive Plan for Kenosha County: 2035, Village of Somers General Zoning and Shoreland/Floodplain Zoning Ordinance and the Village of Somers Land Division and Platting Control Ordinance.

2. Contact the Kenosha County Department of Planning & Development and schedule a pre-conference meeting, which is required for all site plan review requests.

Meeting Date: \_\_\_\_\_

3. Contact the Village of Somers to determine if your site plan review application requires concept review by the Village Plan Commission. If so, contact the Village of Somers clerk to schedule a concept meeting with the Village Plan Commission.

Meeting Date: \_\_\_\_\_

4. Complete and submit to the Kenosha County Department of Planning & Development the Village of Somers Site Plan Review Application by the filing deadline.

Filing Deadline: \_\_\_\_\_

5. Upon submission you will be given two copies of the date-stamped application. Submit a copy of the date-stamped application to the Village of Somers clerk for placement on the agendas of the Village of Somers Plan Commission and the Village of Somers Board. Keep the other copy for your records.

7. Attend the Village Plan Commission and the Village Board meetings. **NOTE:** You must attend or the Village will not be able to act on your request. At these meetings you will be asked to brief the committee on your request.

Village Plan Commission meeting date (tentative): \_\_\_\_\_

Village Board meeting date (tentative): \_\_\_\_\_

8. Village clerk will provide written notice of final action to property owner/applicant.

**SITE PLAN REVIEW**  
**CHECKLIST**

Owner: Shoreland Lutheran High School Federation, Inc Date 8/30/24

Mailing Address: 9206 21st Street Phone # 262-515-4527

Kenosha, WI 53144 Phone # \_\_\_\_\_

Agent: Excel Engineering Inc Phone # \_\_\_\_\_

Mailing Address: 100 Camelot Dr Phone # 920-322-1575

Fond du Lac, WI 54935

Architect/Engineer: Excel Engineering Inc - Ben Warntjes Phone # 920-322-1575

Mailing Address: 100 Camelot Dr Phone # \_\_\_\_\_

Fond du Lac, WI 54935

Tax Parcel Number(s): 82-4-222-084-0272 Acreage of Project: \_\_\_\_\_

Existing Zoning: I-1 Institutional Proposed Zoning: I-1 Institutional

Conditional Use Permit: CUP is required for the gym

Description of Project: (include the following when applicable):

Description of project: Gym and classroom addition on the east side of the building , choir room addition to the southwest corner, parking lot modifications and parking lot expansion to the east.

Size of existing building(s): 88,202 SF

Size of new building(s) and/or addition(s): 31,619 SF

Number of current and projected full-time and part-time employees, number of shifts: \_\_\_\_\_

60 current, 10 proposed. All full-time.

Number of proposed units: \_\_\_\_\_ Description of units: \_\_\_\_\_

Density: \_\_\_\_\_

Plat of Survey Submitted:

Covenants and Restrictions Submitted

**A. BUILDING PLANS SUBMITTED? (BUILDING APPEARANCE)**

- x No building shall be permitted the design or exterior appearance of which is of such unorthodox or abnormal character in relation to its surroundings as to be unsightly or offensive to generally accepted taste and community standards.
- x No building shall be permitted the design or exterior appearance of which is so identical with those adjoining as to create excessive monotony or drabness.
- x No building shall be permitted where any exposed facade is not constructed or faced with a finished material which is aesthetically compatible with the other facades and presents an attractive appearance to the public and to surrounding properties.
- x Building Scale and Mass. The relative proportion of a building to its neighboring buildings, to pedestrians and observers, or to other existing buildings shall be maintained or enhanced when new buildings are built or when existing buildings are remodeled or altered.
- x Building Rooflines and Roof Shapes. The visual continuity of roofs and their contributing elements (parapet walls, coping, and cornices) shall be maintained in building development and redevelopment.
- x Since the selection of building colors has a significant impact upon the public and neighboring properties, color shall be selected in general harmony with existing neighborhood buildings.
- x No building or sign shall be permitted to be sited on the property in a manner which would unnecessarily destroy or substantially damage the natural beauty of the area, particularly insofar as it would adversely affect values incident to ownership of land in that area, or which would unnecessarily have an adverse effect on the beauty and general enjoyment of existing structures on adjoining properties.
- x The facade of all buildings which face upon a street right-of-way shall be finished with an aesthetically pleasing material. A minimum of 50 percent of a facade facing an existing or future street shall be finished with a combination of brick, decorative masonry material, decorative pre-cast concrete panels, decorative glass panels, wood, or decorative metal or vinyl siding. Such finished material shall extend for a distance of at least 10 feet along the sides of the structure. All buildings on corner lots shall have the required finished facade facing each street.

**B. SITE PLAN SUBMITTED?**

- x Building locations shall maintain required setbacks from property lines and road rights-of-way.
- x Buildings and uses shall provide for safe traffic circulation and safe driveway locations.
- x Buildings and uses shall be provided with adequate public services as approved by the appropriate utility.

**SITE PLAN REVIEW  
CHECKLIST**

- x Buildings and uses shall provide adequate parking and loading areas. No loading dock or overhead doors shall face upon a street right-of-way without approval of the zoning administrator.
  
- x Parking areas shall maintain required setbacks and parking spaces shall be of minimum required size (9' x 18'). Aisle widths within parking lots shall be a minimum of 24' between the ends of the parking spaces.
  
- x Each parking space shall be on the same lot or parcel as the principal use, and all parking lots shall have the same zoning district as the principal use. Parking spaces and driveways shall be a minimum of 20 feet from the established highway right-of-way and a minimum of 10 feet from all rear and side lot lines. Curbs or barriers shall be installed so as to prevent vehicles from extending beyond designated parking or driving areas.
  
- x Parking areas for five or more vehicles adjoining a residential use, shall be screened from such use by a solid wall, fence, evergreen planting of equivalent visual density or other effective means, and built and maintained at a minimum height of four feet at the time of planting or installation.
  
- x Adequate access to a public or private roadway shall be provided for each parking space. For all non-residential properties, driveways for vehicular ingress and egress shall be a minimum of 10 feet in width and not exceed 35 feet in width at the property line.
  
- x All off-street parking of more than five vehicles shall be graded and surfaced with asphalt or concrete.

**C. LANDSCAPING AND LIGHTING PLAN SUBMITTED?**

- x Buildings and uses shall make appropriate use of open spaces. The zoning administrator or Village Board may require appropriate landscaping and planting screens. A landscaping maintenance program, together with appropriate assurances, shall be submitted.
  
- x Dumpsters and other trash receptacles shall be fenced and/or screened from view from street rights-of-way and adjacent residential uses.
  
- x Exterior lighting used for parking lots, recreation facilities, product display, and security shall not spill-over on operators of motor vehicles, pedestrians, and/or adjacent residential property (cut-off type luminaries only).

Appropriate buffers shall be provided between dissimilar uses.

**D. SEWER VERIFICATION/SANITATION EVALUATION OR APPLICATION SUBMITTED?**

**E. UTILITY PLANS SUBMITTED?**

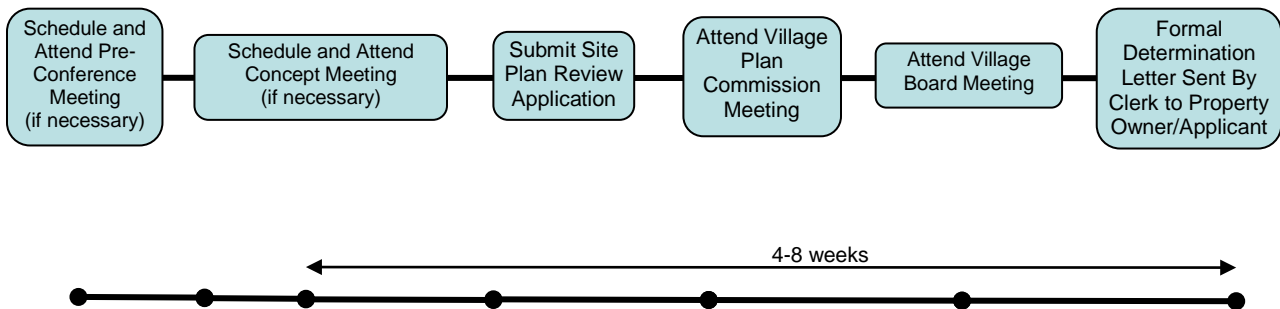
**F. STORMWATER DRAINAGE AND EROSION CONTROL PLAN SUBMITTED?**

Appropriate erosion control and stormwater management measures shall be utilized in all new development. Buildings and uses shall maintain existing topography, drainage patterns, and vegetative cover insofar as is practical. The zoning administrator or Village Board may require that drainage easements be executed.

**IMPORTANT TELEPHONE NUMBERS**

Kenosha County Center	
Department of Planning & Development	
19600 - 75 <sup>th</sup> Street, Post Office, Suite 185-3	
Bristol, Wisconsin 53104-9772	
Division of County Development (including Sanitation & Land Conservation).....	<b>857-1895</b>
Facsimile #.....	857-1920
Public Works Division of Highways .....	857-1870
Administration Building	
Division of Land Information.....	653-2622
Village of Somers .....	859-2822
Wisconsin Department of Natural Resources - Sturtevant Office .....	884-2300
Wisconsin Department of Transportation - Waukesha Office .....	548-8722

**Site Plan Review Procedure Timeline**



For Reference Purposes



# Shoreland Lutheran High School Traffic Impact Analysis

Village of Somers  
Kenosha County, Wisconsin

October 24, 2024



TRAFFIC IMPACT STUDY FOR:

## SHORELAND LUTHERAN HIGH SCHOOL

VILLAGE OF SOMERS, KENOSHA COUNTY, WISCONSIN

DATE SUBMITTED: October 24, 2024

**PREPARED FOR:**

Excel Engineering  
100 Camelot Drive  
Fond du Lac, WI 54935  
Phone: (920) 926-9800  
Contact Person: Ben Warntjes. P.E.

**PREPARED BY:**

Traffic Analysis & Design, Inc.  
P.O. Box 128  
Cedarburg, WI 53012  
Phone: (800) 605-3091  
Contact Person: Donald J. Lee, P.E. (WisDOT TIA Certification # SE05-804-046)  
John Bieberitz, P.E., PTOE (WisDOT TIA Certification # SE05-804-044)

*“I certify that this Traffic Impact Analysis has been prepared by me or under my immediate supervision and that I have experience and training in the field of traffic and transportation engineering.”*

---

Donald J. Lee, P.E.  
Wisconsin Registration #35214-006  
Traffic Analysis & Design, Inc.

**Shoreland Lutheran High School  
Traffic Impact Analysis  
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- Exhibit 1-1 .....Project Overview Map
- Exhibit 1-2A.....Conceptual Site Plan – Initial Build
- Exhibit 1-2B.....Conceptual Site Plan – Full Build
- Exhibit 1-3 .....Year 2025 Recommended Modifications
- Exhibit 1-4 .....Year 2035 Recommended Modifications
  
- Exhibit 2-1 .....Project Overview Map
- Exhibit 2-2A.....Conceptual Site Plan – Initial Build
- Exhibit 2-2B.....Conceptual Site Plan – Full Build
  
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## CHAPTER I – INTRODUCTION & EXECUTIVE SUMMARY

### PART A – PURPOSE OF REPORT AND STUDY OBJECTIVES

Shoreland Lutheran High School is proposing an expansion to their existing high school located on the north side of County Trunk Highway (CTH) E/Somers Road, immediately west of 88<sup>th</sup> Avenue/CTH H within the Village of Somers, Kenosha County. The initial build includes expansion of the existing gymnasium as well as additional classroom space. Additional parking is also expected under the initial build scenario. As part of the full build-out plans, within the next 5 to 10 years, a football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) are also proposed in the western portion of the site.

TADI conducted this traffic impact analysis (TIA) to determine the expected weekday morning arrival and weekday afternoon dismissal peak hour operating conditions and recommendations at the study area intersections for the Existing, Background (with identified off-site development), Initial Build and Full Build (with on-site and off-site developments) traffic operating conditions. The current student population is expected to increase from the current enrollment of 450 students up to 475 students in the next few years and potentially up to 550 students by 2035.

This report documents the procedures, findings, and conclusions of the TIA. The analysis identifies recommended modifications based on existing intersection geometrics, existing traffic volumes, and additional traffic expected to be generated by the proposed Shoreland Lutheran High School and the previously approved off-site development.

### PART B – EXECUTIVE SUMMARY

The executive summary includes a description of the study area, descriptions of the proposed on-site and off-site developments and conclusions based on the findings of the TIA.

#### B1. Location of Study Site with Respect to Area Roadway Network

A street map illustrating the location of the proposed Shoreland Lutheran High School campus is shown in [Exhibit 1-1](#) at the end of this chapter. As discussed with Kenosha County and the Village of Somers, the study area for the proposed school includes the following existing intersections:

- Somers Road/CTH E & 97<sup>th</sup> Avenue (one-way stop control)
- Somers Road/CTH E & Existing West School Driveway (one-way stop control)
- Somers Road/CTH E & Existing East School Driveway (one-way stop control)
- Somers Road/CTH E & 88<sup>th</sup> Avenue/CTH H (all-way stop control)
- 88<sup>th</sup> Avenue/CTH H & Existing South School Driveway (one-way stop control)

In addition to these existing intersections, the following proposed driveways were also included in the study:

- Somers Road/CTH E & Proposed Sports Field Driveway (one-way stop control)
- 88<sup>th</sup> Avenue/CTH H & Proposed North School Driveway (one-way stop control)

The Shoreland Lutheran High School expansion site is bordered by residential land uses to the north and northeast. Additional residential properties and neighborhoods exist on both the north and south sides of Somers Road to the east and west of the site. Agriculture/farming land uses

also exist beyond most of the residential areas in all directions. IH-94 exists further to the west with access ramps to the freeway located about 2 miles west of the school.

## **B2. On-Site Development Description**

Shoreland Lutheran High School is proposing expansion of the existing gymnasium as well as additional classroom space. Some parking on the east side of the school is being displaced as part of the expansion plans; however, a new parking lot is proposed as part of the initial build scenario with about 254 new parking spaces as well as 5 bus stalls proposed on the northeast portion of the school site and 57 additional spaces to the southwest, in front of the school. Full buildout plans, within the next 5 to 10 years, include construction of a new football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) proposed to be located on the western portion of the site. Parking is also proposed near the future sports fields. Copies of the conceptual site plans for the initial build and future full build are included in [Exhibits 1-2A&B](#), respectively.

## **B3. Off-Site Development Description**

One off-site development has been identified within the immediate study area. A residential development, the Beta Terra 45 Residential Neighborhood development, including 45 single family lots, has previously been approved for a parcel of land on the south side of Somers Road. Access to the site is proposed via a new access drive onto Somers Road, immediately east of 93<sup>rd</sup> Avenue.

## **B4. On-Site Generated Traffic**

The hourly vehicular traffic volumes expected to be generated by the Shoreland Lutheran High School expansion are estimated based on trip rates derived using the existing driveway trips counted as part of this project. Using the existing student population and the traffic counts conducted for this study during a typical weekday in mid-September of 2024, weekday morning arrival and weekday afternoon discharge peak hour rates were calculated. To calculate the expected new daily trips, the daily rate for a private high school was taken from the Institute of Transportation Engineer's (ITE) *Trip Generation Manual, 11<sup>th</sup> Edition*.

Based on the assumptions above, the Shoreland Lutheran High School expansion is expected to generate about 20 new vehicular trips (15 in/5 out) during the weekday morning arrival peak hour and 10 new vehicular trips (5 in/5 out) during the weekday afternoon dismissal peak hour under Initial Build conditions. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 50 new vehicle trips (25 in/25 out) under initial build conditions.

Under the future year projected full build out, the High School expansion is expected to generate about 80 new vehicular trips (55 in/25 out) during the weekday morning arrival peak hour and 40 new vehicular trips (10 in/30 out) during the weekday afternoon dismissal peak hour. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 220 new vehicle trips (110 in/110 out) under the future year projected full build out.

## **B5. Off-Site Generated Traffic**

The Beta Terra 45 Residential Neighborhood off-site development is expected to generate about 35 vehicular trips (10 in/25 out) during the weekday morning arrival peak hour, 45 vehicular trips (30 in/15 out) during the weekday afternoon dismissal peak hour and 500 new trips over a typical weekday (24-hour period) under full build out conditions. Trip generation and traffic

assignment information for the Beta Terra 45 Residential Neighborhood off-site developments was taken from the previously completed Beta Terra 45 Residential Neighborhood Development TIA dated June 24, 2021.

## **B6. Proposed Access to Shoreland Lutheran High School**

Two new access points are proposed for the Shoreland Lutheran High School site. A new full access driveway, referred to as “North Driveway” is proposed along 88<sup>th</sup> Avenue/CTH H, about 350 feet north of the existing South Driveway onto CTH H. This new driveway, which is expected to provide access to a new northeast parking lot area on the east side of the school, is proposed as part of the initial build phase of the project. A second access driveway, referred to as “Sports Field Drive” is proposed under the full build conditions. This driveway is proposed to provide access to the sports fields proposed on the western portion of the site. Cross access between the school and the sports field is also proposed as part of the full build development plans; however, because of the proximity of the connection and the curved nature of the connection roadway, the Sports Field Drive is not expected to be utilized as a primary access for typical school day traffic. This second access is proposed as a full access driveway and is proposed to be located about 220 feet east of 97<sup>th</sup> Avenue.

## **B7. Recommended Modifications**

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual (HCM) 6<sup>th</sup> Edition*. Intersection operation is defined by “level of service.” Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A,’ to very poor, represented by LOS ‘F.’ For the purpose of this study, LOS D or better was used to define acceptable peak hour operating conditions.

Modifications to address traffic impacts are included for the Existing, Background, and Build traffic conditions and have been shown for the following five scenarios:

- “Existing Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions, without the proposed school.
- “Year 2025 Background Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions plus full build out of the identified off-site development, without the proposed school.
- “Year 2025 Initial Build Traffic” – These modifications are expected to be necessary to accommodate the Initial Build traffic volumes, which includes the initial build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development.
- “Year 2035 Background Traffic” – These modifications are expected to be necessary to accommodate the future background traffic volume conditions which include typical background growth along the highways plus full build out of the identified off-site development, without the proposed school.
- “Year 2035 Full Build Traffic” – These modifications are expected to be necessary to accommodate the Full Build traffic volumes, which includes the full build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development, including typical background growth along the highways.

The analysis was conducted using existing intersection geometrics and traffic control. The following modifications, as shown in Exhibits 1-3 & 1-4, are recommended to accommodate the respective Background and Build traffic volume scenarios for the years 2025 and 2035, respectively. *Modifications are for jurisdictional consideration and are not legally binding. Kenosha County and the Village of Somers reserve the right to determine alternative solutions.*

Node 100: Somers Road/CTH E intersection with 97<sup>th</sup> Avenue

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 200: Somers Road/CTH E intersection with :Proposed Sports Field Drive

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:*
  - Provide a new full access drive on the north approach as shown on the site plan.
  - Provide stop sign control on the north approach.
  - Provide an eastbound bypass lane on the south side of CTH E at the new intersection.
  - Connect the existing westbound bypass lanes on the north side of CTH E between 93<sup>rd</sup> Avenue and 97<sup>th</sup> Avenue.

Node 300: Somers Road/CTH E intersection with West School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 400: Somers Road/CTH E intersection with East School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

Node 500: Somers Road/CTH E intersection with 88<sup>th</sup> Avenue/CTH H

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:*
  - Consider providing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2025 Initial Build Traffic:* No additional modifications.
- *Year 2035 Background Traffic:*
  - If not previously constructed, consider constructing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
  - Consider providing an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection (see sensitivity analysis discussion below). If implemented, the additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2035 Full Build Traffic:* No additional modifications.

Node 600: 88<sup>th</sup> Avenue/CTH H intersection with North Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:*
  - Provide a new full access drive on the west approach as shown on the site plan.
  - Provide stop sign control on the west approach.
- *Year 2035 Background Traffic:* No additional modifications.
- *Year 2035 Full Build Traffic:* No additional modifications.

Node 700: Somers Road intersection with South School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

With a new north access driveway onto CTH H proposed, two access scenarios were evaluated as part of this study to analyze the operation of the two driveways along CTH H with and without the existing south school driveway operational. Regardless of the access scenario, both the north and south driveways are expected to operate acceptably at LOS B or better under both

weekday peak periods; therefore, since providing the south driveway is not expected to cause an unsafe condition, allowing the south access driveway is recommended to allow for additional access options to/from the site. However, if it is decided to remove the driveway, the north driveway is expected to operate acceptably as the sole access onto CTH H.

All intersections are expected to operate acceptably with the proposed initial build out of the Shoreland Lutheran High School campus with the recommended modifications. Without the recommended modifications under the current year 2025 background traffic conditions, longer delays and queues are expected for the eastbound and westbound movements at the all-way stop-controlled Somers Road intersection with 88<sup>th</sup> Avenue during the weekday morning arrival and weekday afternoon discharge peak periods, with school in session. The higher delays and queues increase under the initial build conditions. However, the increases in delay under the background and initial build conditions are expected to be slightly higher (15 seconds for westbound vehicles and 7 seconds for eastbound vehicle) during the typical weekday morning (for westbound) and afternoon (for eastbound) discharge peak period under the initial build traffic conditions with the existing lane configurations on all approaches, that is, with no additional eastbound/westbound lanes along Somers Road. The increases in queues under the existing lane configurations are expected to be 2 to 3 vehicles. It is noted that these delays and increases in queue lengths would only be expected for less than 30 minutes during the typical weekday morning and afternoon school “surge” peak periods with lower delays and queues all other hours of the day.

In addition, with the recommended modifications under future year 2035 background traffic conditions; that is, with additional lanes on the eastbound and westbound approaches but without additional lanes on the northbound and southbound approaches at the Somers Road intersection with 88<sup>th</sup> Avenue, slightly higher delays (2 seconds for northbound vehicles and 8 seconds for southbound vehicle) would be expected on the north and south approaches during the typical weekday afternoon discharge peak period under the full build traffic conditions with single lanes on the north and south approaches. Queue lengths on the north and south approaches would be expected to be 7 to 9 vehicles; however, these queue lengths would only be expected for less than 30 minutes. To alleviate the expected longer delays and queue lengths, additional northbound and southbound lanes would be required; therefore, a sensitivity analysis was completed to document this geometric condition, as described below.

Because modifications are recommended at the all-way stop-controlled Somers Road intersection with 88<sup>th</sup> Avenue under current and background conditions and because the modifications are further driven by projected future year volumes during peak “surge” traffic periods during the typical weekday morning school arrival and afternoon school discharge peak periods, a sensitivity analysis was completed for this project to document the operations if dual lanes were not only constructed along Somers Road (eastbound/westbound lanes) but also along 88<sup>th</sup> Avenue (northbound/southbound lanes) at the all-way stop controlled intersection. With the modifications, all movements are expected to operate safely with moderate queue lengths. Therefore, the following modifications could be considered as optional under the design year background and full build conditions.

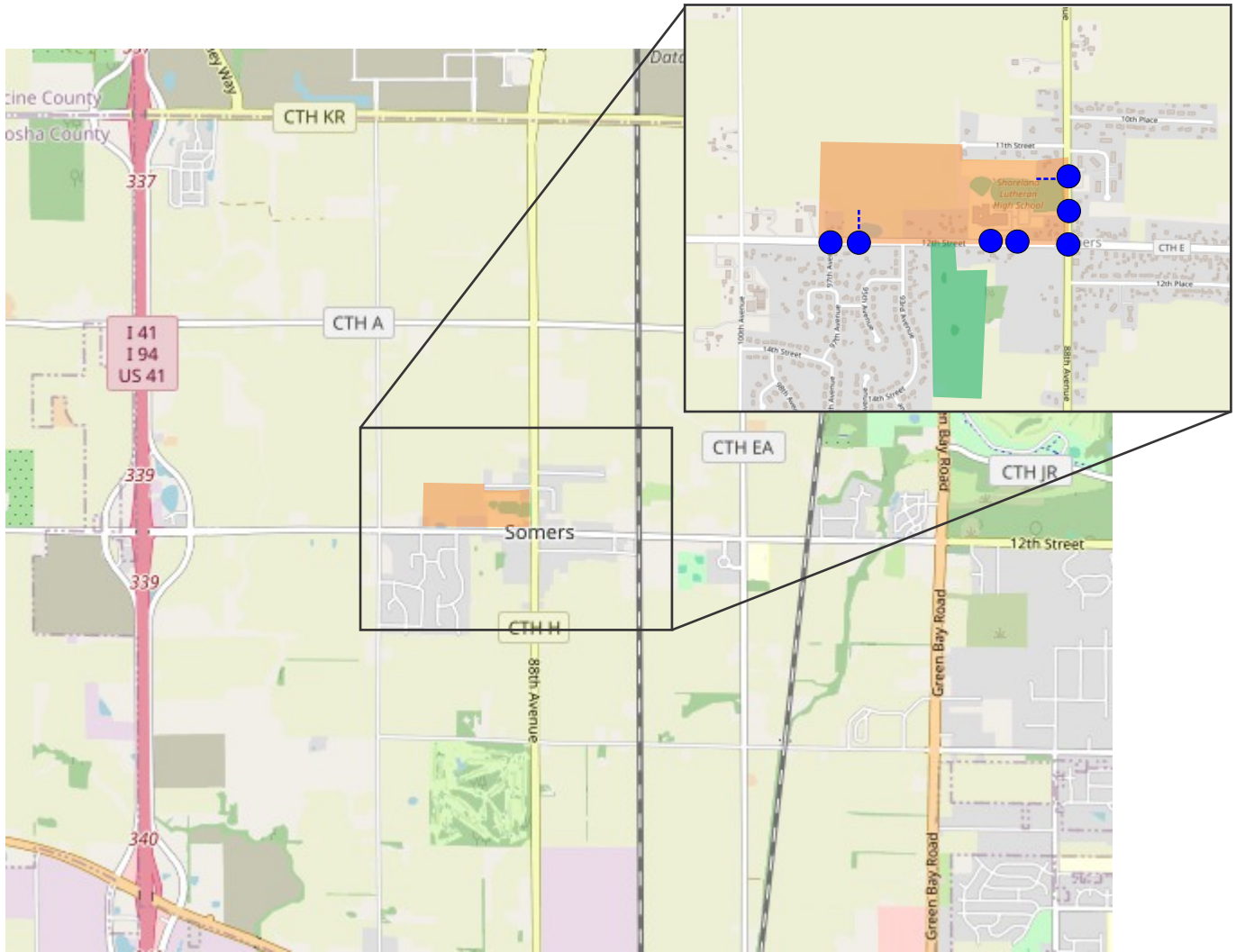
*Node 500: Somers Road/CTH E intersection with 88<sup>th</sup> Avenue/CTH H*

- *Year 2035 Background Traffic:*
  - Provide an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.

- *Year 2035 Full Build Traffic:* No additional modifications.

## **B8. Conclusion**

All movements at the study area intersections are expected to operate safely and efficiently with the development assumptions outlined in this TIA and with the identified recommended modifications if properly designed and implemented through the design year of the development.



**LEGEND**

- Study Area Intersection
- School Expansion Site Location
- Beta Terra 45 Residential Site Location





PROJECT INFORMATION

PROPOSED BUILDING ADDITION PHASE ONE FOR:  
**SHORELAND LUTHERAN HIGH SCHOOL**  
 9026 12TH STREET • KENOSHA, WI 53144

PRELIMINARY DATES  
 JULY 8, 2024  
 JULY 18, 2024

NOT FOR CONSTRUCTION

JOB NUMBER  
 240018700

DATE REVISION  
**C1.1**



KEYNOTES		LEGEND	
(1)	CONCRETE FLOOR AND STRUCTURAL FLOOR FOR OFFICE	1	PROPOSED ASPHALT
(2)	ASPHALT DRIVE AND DRIVEWAY	2	EXISTING ASPHALT
(3)	GRAVEL DRIVE AND DRIVEWAY	3	EXISTING GRAVEL
(4)	CONCRETE DRIVE AND STRUCTURAL FLOOR FOR DRIVEWAY	4	EXISTING CONCRETE
(5)	CONCRETE DRIVE AND DRIVEWAY	5	EXISTING CONCRETE DRIVEWAY
(6)	CONCRETE DRIVE AND DRIVEWAY	6	EXISTING CONCRETE DRIVEWAY
(7)	CONCRETE DRIVE AND DRIVEWAY	7	EXISTING CONCRETE DRIVEWAY
(8)	CONCRETE DRIVE AND DRIVEWAY	8	EXISTING CONCRETE DRIVEWAY
(9)	CONCRETE DRIVE AND DRIVEWAY	9	EXISTING CONCRETE DRIVEWAY
(10)	CONCRETE DRIVE AND DRIVEWAY	10	EXISTING CONCRETE DRIVEWAY
(11)	CONCRETE DRIVE AND DRIVEWAY	11	EXISTING CONCRETE DRIVEWAY
(12)	CONCRETE DRIVE AND DRIVEWAY	12	EXISTING CONCRETE DRIVEWAY
(13)	CONCRETE DRIVE AND DRIVEWAY	13	EXISTING CONCRETE DRIVEWAY
(14)	CONCRETE DRIVE AND DRIVEWAY	14	EXISTING CONCRETE DRIVEWAY
(15)	CONCRETE DRIVE AND DRIVEWAY	15	EXISTING CONCRETE DRIVEWAY
(16)	CONCRETE DRIVE AND DRIVEWAY	16	EXISTING CONCRETE DRIVEWAY
(17)	CONCRETE DRIVE AND DRIVEWAY	17	EXISTING CONCRETE DRIVEWAY
(18)	CONCRETE DRIVE AND DRIVEWAY	18	EXISTING CONCRETE DRIVEWAY
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(25)	CONCRETE DRIVE AND DRIVEWAY	25	EXISTING CONCRETE DRIVEWAY
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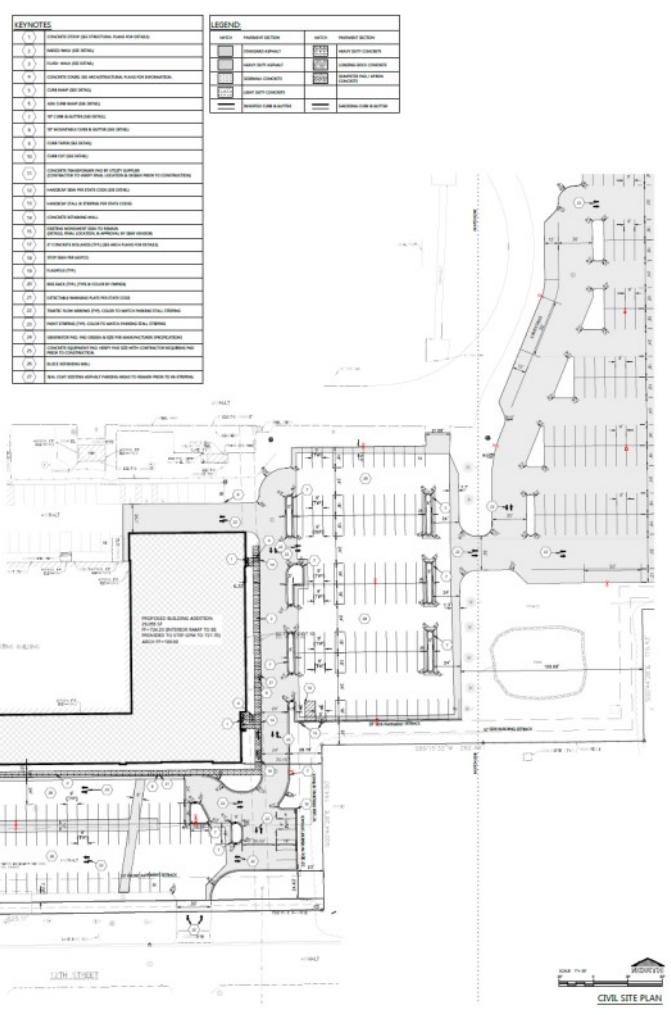
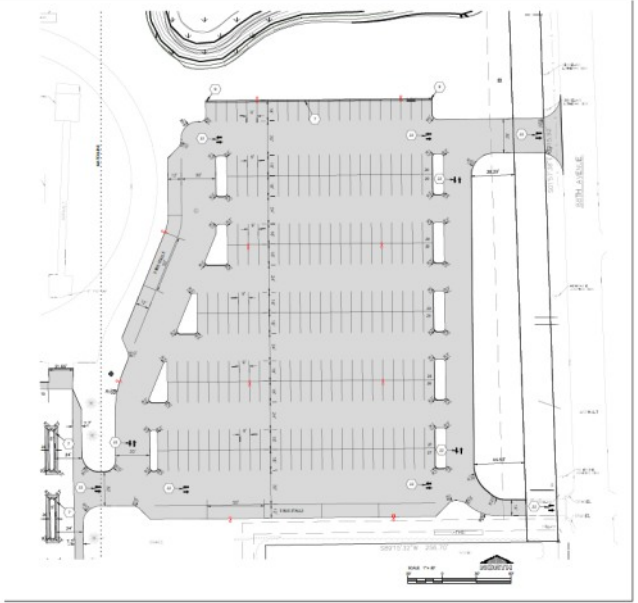


EXHIBIT 1-2A  
 CONCEPTUAL SITE PLAN  
 INITIAL BUILD

SOMERS, WISCONSIN



PROJECT INFORMATION

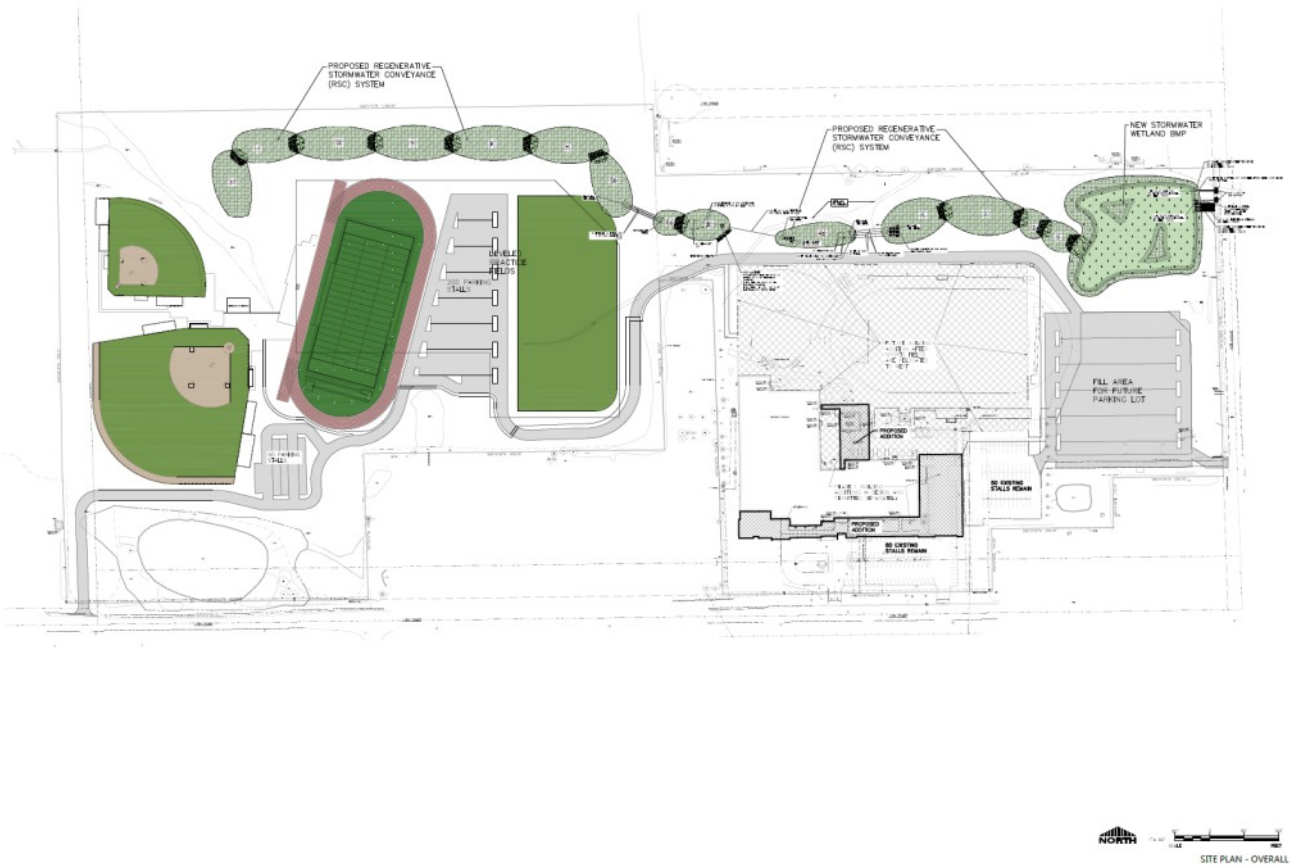
PROPOSED MASTERPLAN FOR:  
**SHORELAND LUTHERAN HIGH SCHOOL**  
9026 12TH STREET • KENOSHA, WI 53144

REVISIONS
AUG. 20, 2023
MAR. 23, 2023
MAR. 15, 2023
APR. 19, 2023
NOV. 8, 2022
NOV. 18, 2022

NOT FOR CONSTRUCTION

JOB NUMBER  
2187400



SHEET NUMBER  
**C100**

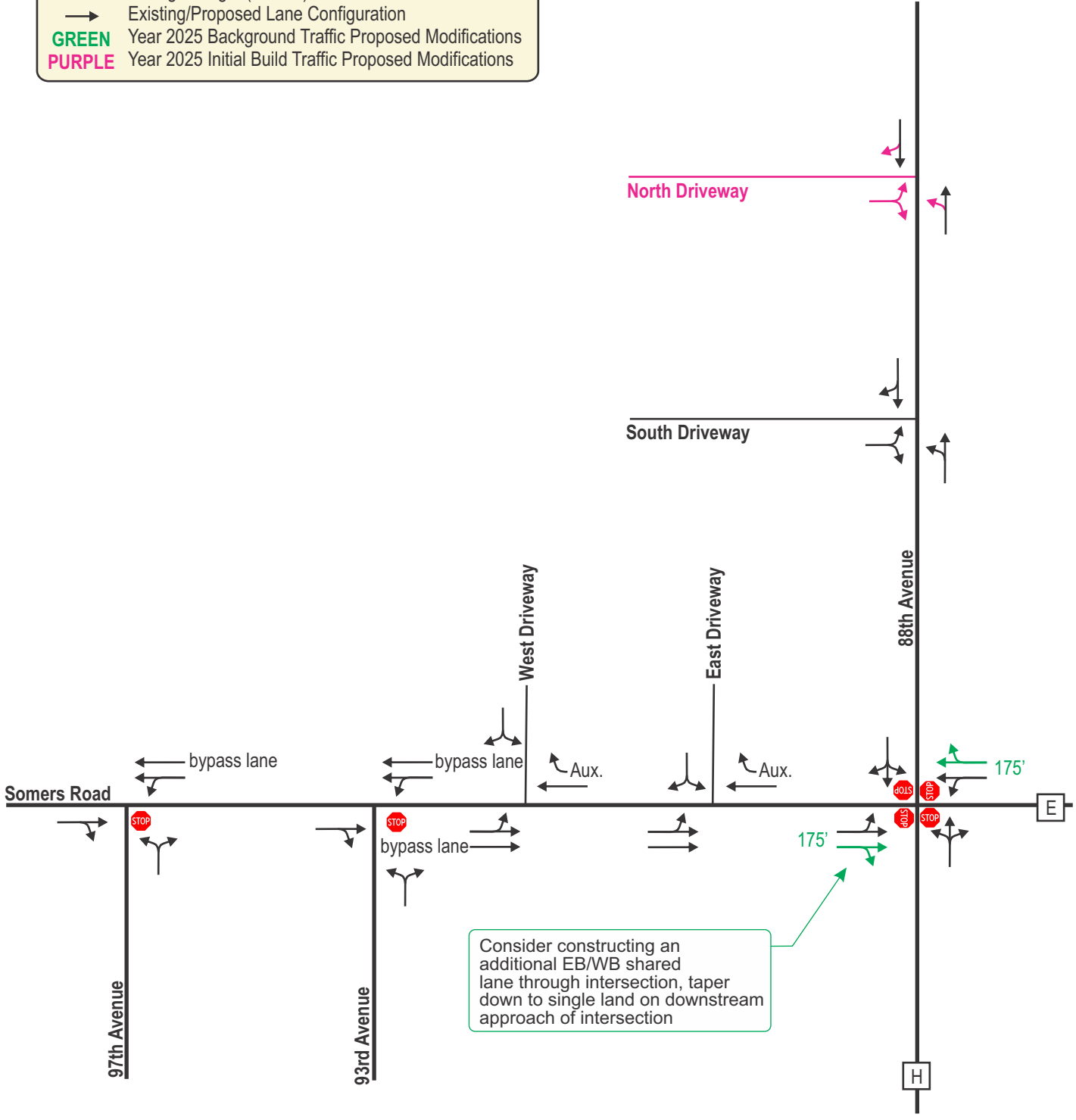


**EXHIBIT 1-2B**  
**CONCEPTUAL SITE PLAN**  
**FULL BUILD**

**SOMERS, WISCONSIN**

**LEGEND**



-  Stop Sign Control
- XX' Storage Length (In Feet)
-  Existing/Proposed Lane Configuration
- GREEN** Year 2025 Background Traffic Proposed Modifications
- PURPLE** Year 2025 Initial Build Traffic Proposed Modifications



**EXHIBIT 1-3  
YEAR 2025 RECOMMENDED MODIFICATIONS**

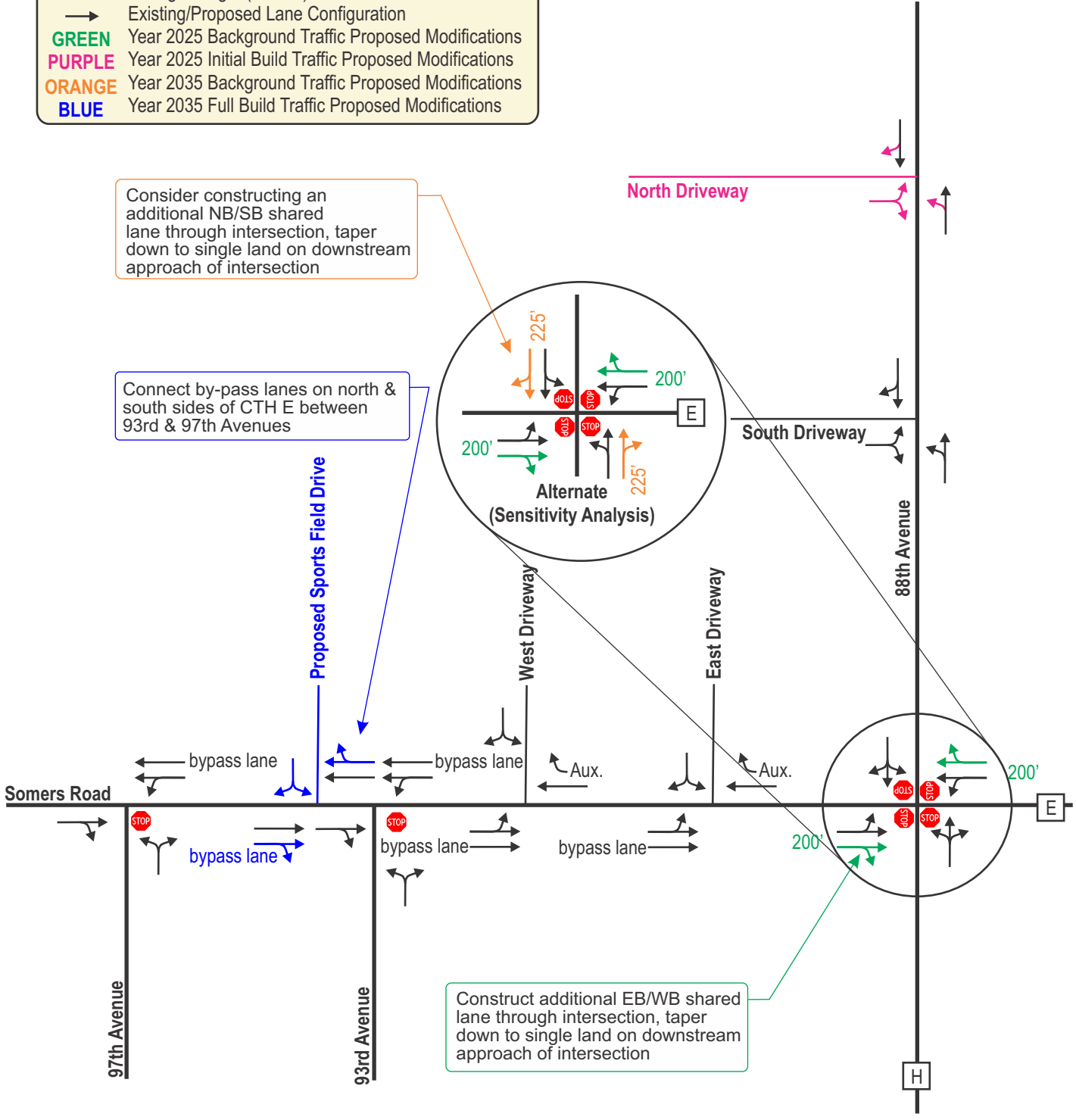
**SOMERS, WISCONSIN**

**LEGEND**

-  Stop Sign Control
- XX'** Storage Length (In Feet)
-  Existing/Proposed Lane Configuration
- GREEN** Year 2025 Background Traffic Proposed Modifications
- PURPLE** Year 2025 Initial Build Traffic Proposed Modifications
- ORANGE** Year 2035 Background Traffic Proposed Modifications
- BLUE** Year 2035 Full Build Traffic Proposed Modifications

Consider constructing an additional NB/SB shared lane through intersection, taper down to single land on downstream approach of intersection

Connect by-pass lanes on north & south sides of CTH E between 93rd & 97th Avenues



Construct additional EB/WB shared lane through intersection, taper down to single land on downstream approach of intersection



**EXHIBIT 1-4  
YEAR 2035 RECOMMENDED MODIFICATIONS**

## CHAPTER II – PROPOSED DEVELOPMENT

### PART A – ON-SITE DEVELOPMENT

#### A1. Development Site Location

Shoreland Lutheran High School is proposing an expansion to their existing high school located on the north side of County Trunk Highway (CTH) E/Somers Road, immediately west of 88<sup>th</sup> Avenue/CTH H within the Village of Somers, Kenosha County. The initial build includes expansion of the existing gymnasium as well as additional classroom space. Additional parking is also expected under the initial build scenario. As part of the full build-out plans, within the next 5 to 10 years, a football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) are also proposed in the western portion of the site. A street map illustrating the location of the proposed Shoreland Lutheran High School is shown in [Exhibit 2-1](#) at the end of this chapter. Copies of the conceptual site plans for the initial build and future full build are included in [Exhibits 2-2A&B](#), respectively.

#### A2. Surrounding Land Use

The Shoreland Lutheran High School expansion site is bordered by residential land uses to the north and northeast. Additional residential properties and neighborhoods exist on both the north and south sides of Somers Road to the east and west of the site. Agriculture/farming land uses also exist beyond most of the residential areas in all directions. IH-94 exists further to the west with access ramps to the freeway located about 2 miles west of the school.

#### A3. On-Site Development Description

Shoreland Lutheran High School is proposing expansion of the existing gymnasium as well as additional classroom space. Some parking on the east side of the school is being displaced as part of the expansion plans; however, a new parking lot is proposed as part of the initial build scenario with about 254 new parking spaces as well as 5 bus stalls proposed on the northeast portion of the school site and 57 additional spaces to the southwest, in front of the school. Full buildout plans, within the next 5 to 10 years, include construction of a new football/soccer/track facility as well as additional sports fields (soccer/softball/baseball) proposed to be located on the western portion of the site. Parking is also proposed near the future sports fields.

#### A4. On-Site Development Access

Two new access points are proposed for the Shoreland Lutheran High School site. A new full access driveway, referred to as “North Driveway” is proposed along 88<sup>th</sup> Avenue/CTH H, about 350 feet north of the existing South Driveway onto CTH H. This new driveway, which is expected to provide access to a new northeast parking lot area on the east side of the school, is proposed as part of the initial build phase of the project. A second access driveway, referred to as “Sports Field Drive” is proposed under the full build conditions. This driveway is proposed to provide access to the sports fields proposed on the western portion of the site. Cross access between the school and the sports field is also proposed as part of the development plans; however, because of the proximity of the connection and the curved nature of the connection roadway, the Sports Field Drive is not expected to be utilized as a primary access for typical school day traffic. This second access is proposed as a full access driveway and is proposed to be located about 220 feet east of 97<sup>th</sup> Avenue.

## **PART B – STUDY AREA**

### **B1. Influence Area**

The campus is expected to draw from the greater Milwaukee, Racine, and Kenosha County areas. Statistics and a map showing the location of the feeder families, by existing school district, are provided in the appendix of this report.

### **B2. Area of Significant Traffic Impact**

As discussed with Kenosha County and the Village of Somers, the study area for the proposed school includes the following existing intersections:

- Somers Road/CTH E & 97<sup>th</sup> Avenue (one-way stop control)
- Somers Road/CTH E & Existing West School Driveway (one-way stop control)
- Somers Road/CTH E & Existing East School Driveway (one-way stop control)
- Somers Road/CTH E & 88<sup>th</sup> Avenue/CTH H (all-way stop control)
- 88<sup>th</sup> Avenue/CTH H & Existing South School Driveway (one-way stop control)

In addition to these existing intersections, the following proposed driveways were also included in the study:

- Somers Road/CTH E & Proposed Sports Field Driveway (one-way stop control)
- 88<sup>th</sup> Avenue/CTH H & Proposed North School Driveway (one-way stop control)

## **PART C – OFF-SITE LAND USE AND DEVELOPMENT**

One off-site development has been identified within the immediate study area. A residential development, the Beta Terra 45 Residential Neighborhood development, including 45 single family lots, has previously been approved for a parcel of land on the south side of Somers Road. Access to the site is proposed via a new access drive onto Somers Road, immediately east of 93<sup>rd</sup> Avenue. The off-site development traffic was included in the Year 2025 and Year 2035 Background Traffic scenarios.

## **PART D – SITE ACCESSIBILITY**

### **D1. Study Area Roadways**

The study area roadways are discussed below:

*Somers Road (CTH E)* is an east/west two-lane undivided Major Collector with a posted speed limit of 35-mph through the project limits. The Year 2021 WisDOT annual average daily traffic (AADT) volumes on Somers Road were approximately 6,100 vehicles per day (vpd) west of 88<sup>th</sup> Avenue, and 7,000-vpd east of 58<sup>th</sup> Avenue. Sidewalks are not currently provided along either side of Somers Road within the limits of the study area.

*88<sup>th</sup> Avenue (CTH H)* is a north-south two-lane undivided Minor Arterial with a posted speed limit of 45-mph through the project limits. The Year 2021 WisDOT AADT volumes on 88<sup>th</sup> Avenue were approximately 2,100-vpd to the north of Somers Road and 3,200-vpd to the south. Sidewalks are not currently provided along either side of 88<sup>th</sup> Avenue within the limits of the study area.

*97<sup>th</sup> Avenue* is a north-south two-lane undivided local street with a posted speed limit of 25-mph that intersects Somers Road as the south approach of a conventional three-legged stop sign

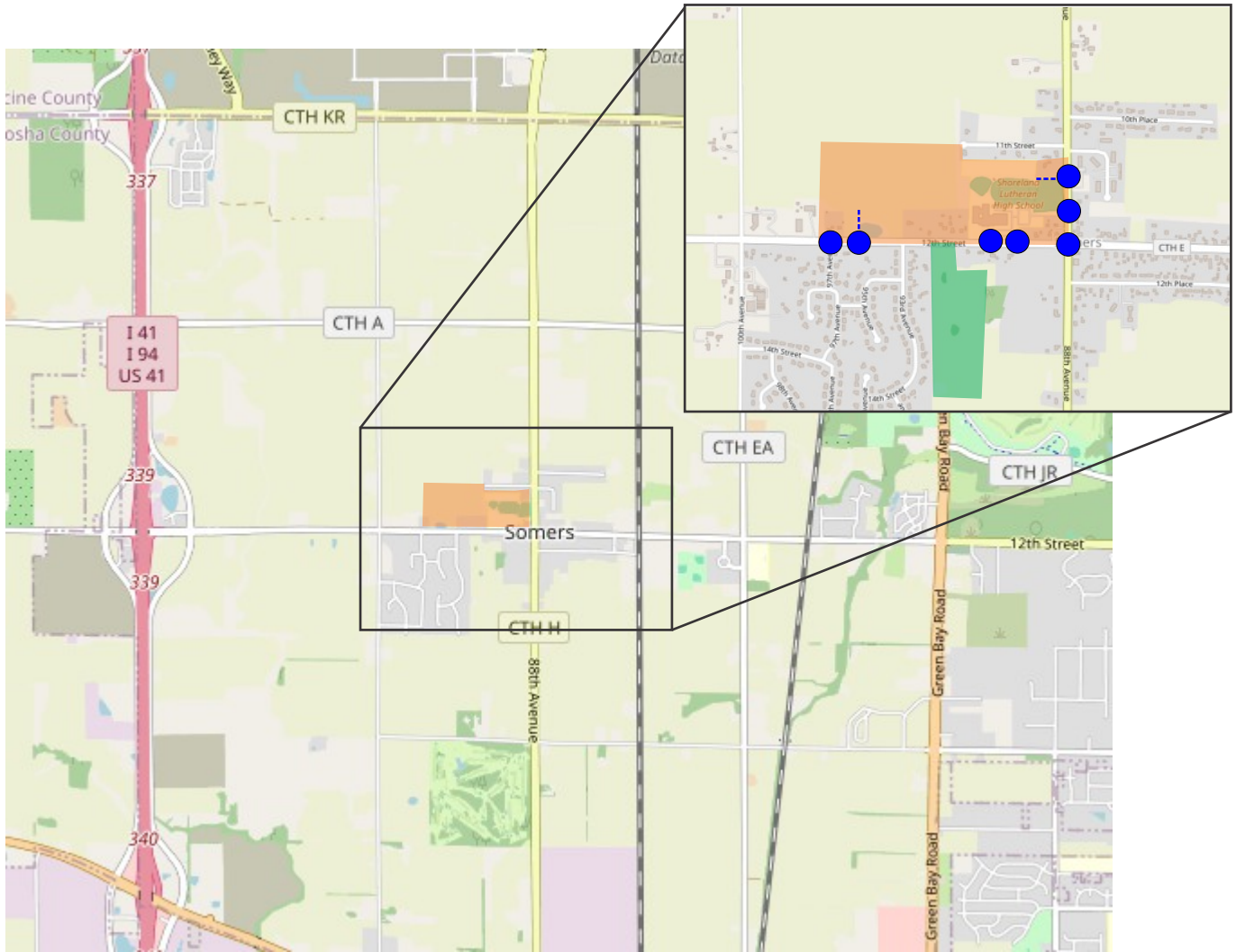
controlled intersection. There are currently no WisDOT AADT volumes available along 97<sup>th</sup> Avenue. Sidewalks are not currently provided along either side of 97<sup>th</sup> Avenue within the limits of the study area.

## **D2. Alternative Modes of Transportation**

Sidewalks do not currently exist along any of the roadways within the limits of the study area. No designated on-street or off-street bicycle facilities were identified.

Pedestrians and bicyclists may use their respective modes to access the proposed development. However, to allow for a conservative (highest vehicular volume) analysis, these modes were assumed to make up a relatively small portion of the overall trips to/from the study area. For the purpose of this TIA, all trips to/from the proposed development site were assumed to occur via motor vehicle.

Transit is not present within the community; however, up to 5 existing private buses are expected to provide service for some students within southeast Wisconsin.



**LEGEND**

- Study Area Intersection
- School Expansion Site Location
- Beta Terra 45 Residential Site Location







PROJECT INFORMATION

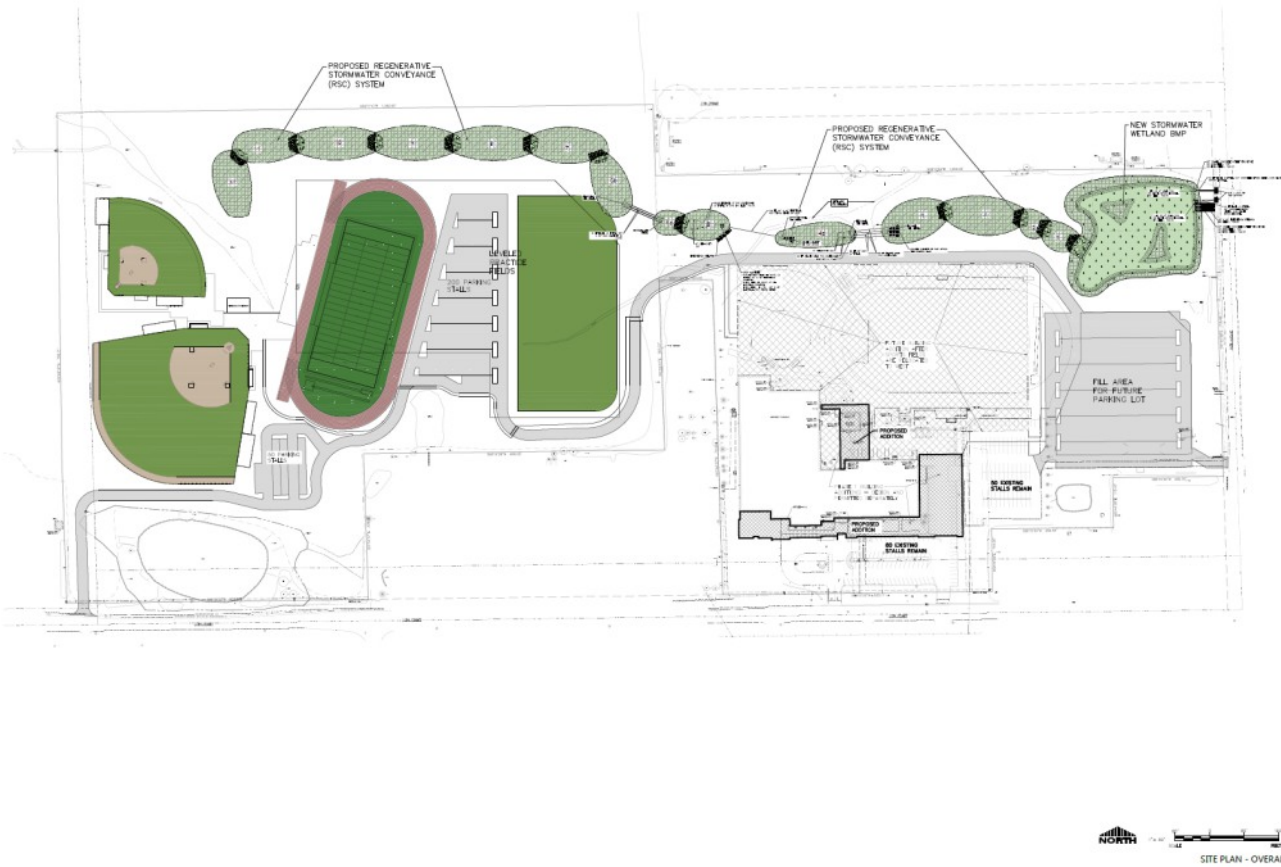
PROPOSED MASTERPLAN FOR:  
**SHORELAND LUTHERAN HIGH SCHOOL**  
 9026 12TH STREET • KENOSHA, WI 53144


REVISIONS
JULY 20, 2023
MARCH 23, 2023
MARCH 15, 2023
APRIL 19, 2023
NOV. 8, 2022
NOV. 18, 2022

NOT FOR CONSTRUCTION

JOB NUMBER  
2187400

SHEET NUMBER  
**C100**



**EXHIBIT 2-2B**  
**CONCEPTUAL SITE PLAN**  
**FULL BUILD**

**SOMERS, WISCONSIN**

## CHAPTER III – ANALYSIS OF EXISTING CONDITIONS

### PART A – EXISTING PHYSICAL CHARACTERISTICS

[Exhibit 3-1](#) shows the existing transportation detail for the study area intersections. More specifically, the exhibit illustrates intersection lane configurations, intersection traffic controls, posted speed limits, and approximate intersection spacing.

### PART B – EXISTING TRAFFIC VOLUMES

The weekday morning school arrival and weekday afternoon school discharge peak hours are expected to drive the improvements needed to adequately accommodate the proposed school, as they represent the highest trip generation for the site. Therefore, in mid-September of 2024, TADI conducted weekday morning arrival (7:00 to 8:00 am) and weekday afternoon (2:30 to 4:30 pm) turning movement traffic counts at the four Somers Road study area intersections with 97<sup>th</sup> Avenue, West school driveway, East school driveway and 88<sup>th</sup> Avenue as well as at the 88<sup>th</sup> Avenue intersection with the south school driveway.

Based on the turning movement counts and the expected school bell schedules, the weekday morning and weekday afternoon peak school hours were identified as being 7:00 to 8:00am and 3:00 to 4:00pm; respectively. These peak hours coincide with the school start and end times at the school of 7:45am and 3:00pm (closing prayer); respectively. Copies of the existing traffic counts and related calculations are included in [Appendix A](#). The Existing peak hour traffic volumes, balanced along the Somers Road and 88<sup>th</sup> Avenue corridors, are shown in [Exhibit 3-2](#).

### PART C – EXISTING CAPACITY LEVEL OF SERVICE

#### C1. Level of Service Definitions

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual (HCM) 6<sup>th</sup> Edition*. Intersection operation is defined by “level of service.” Level of service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A,’ to very poor, represented by LOS ‘F.’ For the purpose of this study, LOS D was used to define acceptable peak hour operating conditions. Descriptions of the various levels of service are as follows:

**LOS A** is the highest level of service that can be achieved. Under this condition, intersection approaches appear quite open, turning movements are easily made, and nearly all drivers find freedom of operation. At unsignalized intersections, average delays are less than **10** seconds.

**LOS B** represents stable operation. At unsignalized intersections, average delays are **10 to 15** seconds.

**LOS C** still represents stable operation, but periodic backups of a few vehicles may develop behind turning vehicles. Most drivers begin to feel restricted, but not objectionably so. At unsignalized intersections, average delays are **15 to 25** seconds.

**LOS D** represents increasing traffic restrictions as the intersection approaches instability. Delays to approaching vehicles may be substantial during short peaks within the peak period, but periodic clearance of long lines occurs, thus preventing excessive backups. At unsignalized intersections, average delays are **25 to 35** seconds.

**LOS E** represents the capacity of the intersection. At unsignalized intersections, average delays are **35 to 50** seconds.

**LOS F** represents jammed conditions where the intersection is over capacity and acceptable gaps for unsignalized intersections in the mainline traffic flow are minimal. At unsignalized intersections, average delays exceed 50 seconds.

## **C2. Existing Traffic Operations**

[Exhibit 3-3](#) show the Existing traffic peak hour operating conditions at the study area intersections for the typical school day. The Existing traffic analysis was conducted using the existing lane configurations shown in [Exhibit 3-1](#) and the Existing traffic volumes shown in [Exhibit 3-2](#).

As shown in [Exhibit 3-3](#), all movements at the study area intersections are currently operating acceptably with LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours except the westbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are currently operating at LOS E during the typical weekday morning arrival peak hour.



## **PART D – SOURCES OF DATA**

The following sources of data were obtained for use in conducting this traffic study:

- Turning movement traffic counts – TADI
- Historic AADT information to calculate background growth rates – WisDOT
- Existing transportation detail – TADI along with Google Earth
- On-site development information – Excel Engineering and Shoreland Lutheran High School
- Off-site development information – TADI

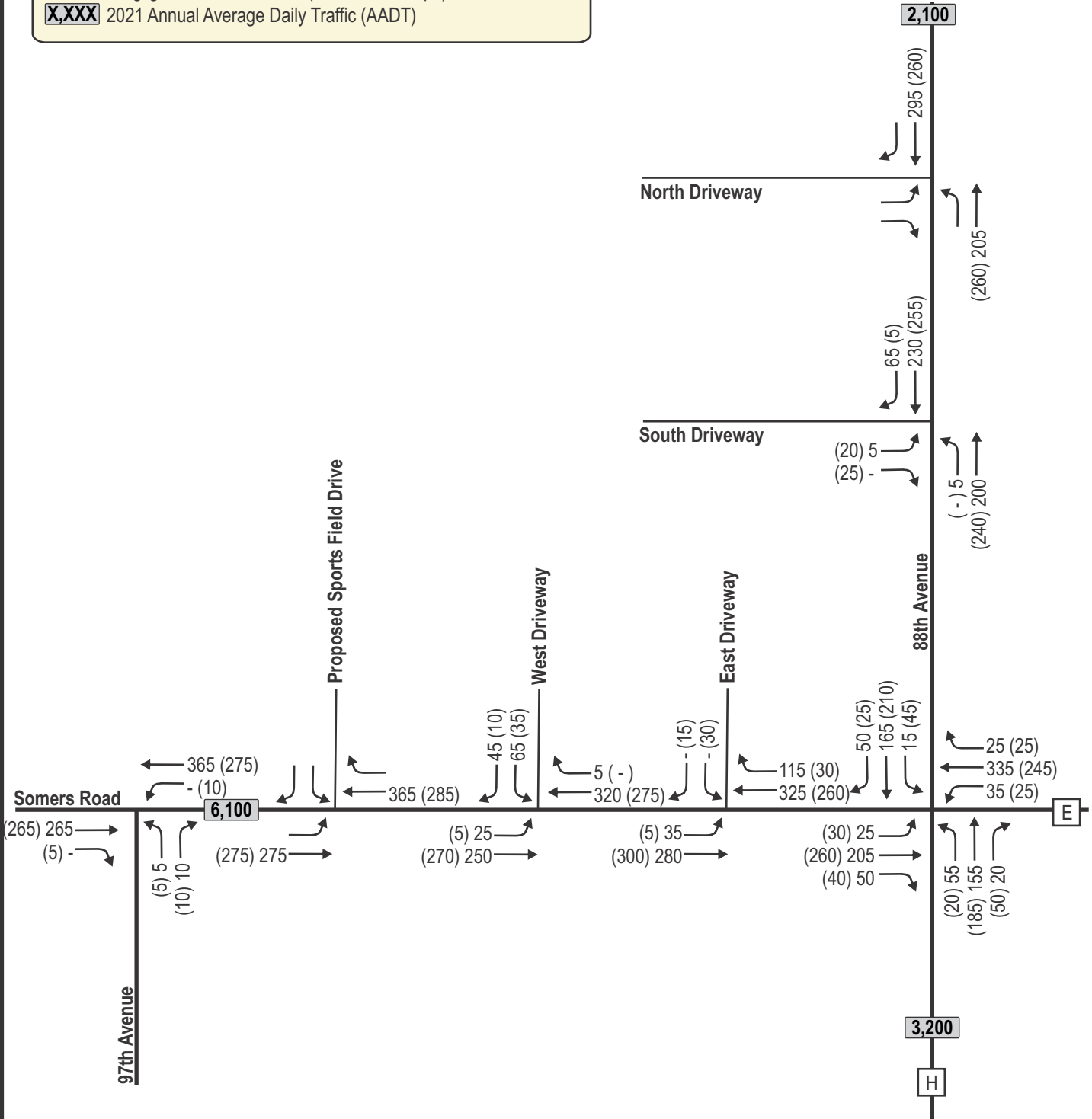


**LEGEND**

-  Stop Sign Control
-  Existing Lane Configuration
- XX'** Existing Storage Length (in Feet)
- XX'** Distance Between Roadways (in Feet)

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)
- X,XXX 2021 Annual Average Daily Traffic (AADT)



**EXHIBIT 3-2  
EXISTING TRAFFIC VOLUMES**

**SOMERS, WISCONSIN**

**Exhibit 3-3**  
**Year 2024 Existing Traffic Peak Hour Operating Conditions**  
**With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	Lanes->		-	1		1	-		1					
	AM	LOS	-	*		A	-		B					
		Delay	-	*		7.8	-		11.5					
		Queue	-	*		25'	-		25'					
	PM	LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.2					
Queue		-	*		25'	-		25'						
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	Lanes->		1	-	-	1			-			1		
	AM	LOS	A	-	-	*			-			C		
		Delay	8.3	-	-	*			-			17.9		
		Queue	25'	-	-	*			-			35'		
	PM	LOS	A	-	-	*			-			B		
		Delay	7.9	-	-	*			-			13.3		
Queue		25'	-	-	*			-			25'			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	Lanes->		1	-	-	1			-			1		
	AM	LOS	A	-	-	*			-			C		
		Delay	8.9	-	-	*			-			15.0		
		Queue	25'	-	-	*			-			25'		
	PM	LOS	A	-	-	*			-			B		
		Delay	8.0	-	-	*			-			13.3		
Queue		25'	-	-	*			-			25'			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	Lanes->		1			1			1			1		
	AM	LOS	C			E			C			C		
		Delay	23.5			42.8			20.8			20.4		
		Queue	120'			245'			90'			90'		
	PM	LOS	D			D			C			D		
		Delay	31.6			26.9			22.6			25.3		
Queue		170'			140'			105'			125'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	Lanes->		1			-			1	-	-	1		
	AM	LOS	B			-			A	*	-	*		
		Delay	12.3			-			8.1	*	-	*		
		Queue	25'			-			25'	*	-	*		
	PM	LOS	B			-			A	*	-	*		
		Delay	12.2			-			8.0	*	-	*		
Queue		25'			-			25'	*	-	*			

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

## CHAPTER IV – DEVELOPMENT TRAFFIC

### PART A – DEVELOPMENT TRAFFIC FORECASTING

To determine the background traffic growth rate to use when calculating the design year traffic volumes, historic WisDOT AADT volumes from the year 1990 through year 2023 were collected along CTH E and CTH H at the WisDOT count stations located along the corridors in close proximity to the study area. Using this data, historical AADT trend lines were developed to determine the expected annual growth of traffic that may occur on the corridor between the present and design year 2035. Based on the traffic projections (1990 – 2023) and assuming the historic trends will continue at a linear rate, traffic volumes along the CTH E and CTH H corridors would be expected to increase at a rate of 1.5-percent per year. Therefore, this growth rate was applied to the existing traffic volumes collected as part of this study. The Year 2035 Projected traffic volumes, which account for this yearly growth rate, are shown in [Exhibit 4-2A](#). The growth rate calculations have been included in the [Appendix](#) of this study.

#### A1. Trip Generation & Distribution

To address future traffic impacts along study area roadways and at the intersections adjacent to the development, it is necessary to identify the hourly and daily volume of traffic generated by the development. The hourly vehicular traffic volumes expected to be generated by the Shoreland Lutheran High School expansion are estimated based on trip rates derived using the existing driveway trips counted as part of this project. Using the existing student population and the traffic counts conducted for this study during a typical weekday in mid-September of 2024, weekday morning arrival and weekday afternoon discharge peak hour rates were calculated. Based on the calculations, the weekday morning arrival and weekday afternoon discharge peak hour rates were calculated at 0.81 vehicles/student and 0.40 vehicles/student, respectively. To calculate the expected new daily trips, the daily rate for a private high school was taken from the Institute of Transportation Engineer's (ITE) *Trip Generation Manual, 11<sup>th</sup> Edition*.

The trip generation and distribution for the Shoreland Lutheran High School expansion typical weekday school day Initial Build and Full Build scenarios are shown in [Exhibits 4-3A&B](#). The trip generation and distribution for the Beta Terra 45 Residential Neighborhood off-site development is shown in [Exhibit 4-3C](#). The trip distribution for the school expansion, which is listed below, and shown graphically in [Exhibit 4-4](#), was determined based on the location of anticipated school feeder families as well as the existing turning movement counts conducted as part of this study.

- 50-percent to/from the east on Somers Road
- 35-percent to/from the west on Somers Road
- 10-percent to/from the north on CTH H
- 5-percent to/from the south on CTH H

As shown in [Exhibit 4-3A](#), the Shoreland Lutheran High School expansion is expected to generate about 20 new vehicular trips (15 in/5 out) during the weekday morning arrival peak hour and 10 new vehicular trips (5 in/5 out) during the weekday afternoon dismissal peak hour under Initial Build conditions. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 50 new vehicle trips (25 in/25 out) under Initial Build conditions. Note that the ITE trip generation peak hour volumes do take into account teacher and staff trips which generally fall outside the peak hour.

As shown in [Exhibit 4-3B](#), the High School expansion is expected to generate about 80 new vehicular trips (55 in/25 out) during the weekday morning arrival peak hour and 40 new vehicular trips (10 in/30 out) during the weekday afternoon dismissal peak hour. On a typical weekday (24-hour period) with school in session, the High School expansion is expected to generate approximately 220 new vehicle trips (110 in/110 out) under the future year projected full build out.

The Beta Terra 45 Residential Neighborhood off-site development is expected to generate about 35 vehicular trips (10 in/25 out) during the weekday morning arrival peak hour, 45 vehicular trips (30 in/15 out) during the weekday afternoon dismissal peak hour and 500 new trips over a typical weekday (24-hour period) under full build out conditions. Trip generation and traffic assignment information for the Beta Terra 45 Residential Neighborhood off-site developments were taken from the previously completed Beta Terra 45 Residential Neighborhood Development TIA dated June 24, 2021. All relevant traffic information for this off-site development area is provided in [Appendix B](#).

## **A2. Mode Split**

The trip generation above provides estimates of vehicular traffic based on the student population assumptions provided by Shoreland Lutheran High School as well as the trip generation rates derived using the existing driveway trips counted as part of this project. All trips to/from the proposed development site were assumed to occur via motor vehicle.

## **A3. Determination of Linked and Pass-by Trip Traffic**

Linked trips occur when a patron or employee visits more than one land use within a site without leaving the site. Since the school is the only use on-site, no linked trips are expected.

Pass-by trips occur when motorists on the adjacent roadway system stop off at a development prior to continuing on their intended route. For example, a motorist eastbound on Somers Road stops at Shoreland Lutheran High School prior to continuing eastbound on Somers Road. Due to the proposed land use, no pass-by trips were assumed as part of this study.

## **A4. Trip Assignment**

New trips expected to be generated by Shoreland Lutheran High School initial build and full build were assigned based on the trip distribution described above, shown in tabular format in [Exhibits 4-3A&B](#) and graphically in [Exhibit 4-4](#). As shown in the table at the bottom of the trip generation exhibit, new trips were assigned and broken out for the typical school day. Per direction from Kenosha County, two access scenarios were evaluated as part of this study to analyze the operation of the driveways along CTH H with and without the existing south school driveway operational. The following scenarios were evaluated:

**Scenario 1** – Both driveways along CTH H operational. Initial build and Full build new trips for this scenario are shown on [Exhibits 4-5A and 4-8A](#), respectively.

**Scenario 2** – Only the north driveway along CTH H operational with the south driveway removed. Initial build and Full build new trips for this scenario are shown on [Exhibit 4-5B and 4-8B](#), respectively.

The Beta Terra 45 Residential Neighborhood off-site development new trips are shown in [Exhibit 4-9](#).

Under the traffic scenarios with and without the south school driveway operational, existing traffic patterns will change with vehicles expecting to divert to the north driveway, if the south

driveway were eliminated. Therefore, [Exhibits 4-10A and 4-10B](#) show the expected diverted traffic under the two scenarios listed above, respectively.

## **PART B – DETERMINATION OF BACKGROUND AND BUILD TRAFFIC**

### **B1. Background Traffic**

Background traffic volumes include the full build-out of the Beta Terra 45 Residential Neighborhood off-site development.

The Year 2025 Background traffic volumes were determined by adding the Existing traffic volumes ([Exhibit 3-2](#)) to the Beta Terra 45 Residential Neighborhood new trips ([Exhibit 4-9](#)). The Year 2025 Background traffic volumes are shown in [Exhibit 4-1](#).

The Year 2035 Background traffic volumes were determined by adding the Year 2035 Projected traffic volumes ([Exhibit 4-2A](#)) to the Beta Terra 45 Residential Neighborhood new trips ([Exhibit 4-9](#)). The Year 2035 Background traffic volumes are shown in [Exhibit 4-2B](#).

### **B2. Initial Build Traffic**

The Year 2025 Initial Build (North and South Driveways on CTH H) traffic volumes assume the initial build-out of the Shoreland Lutheran High School under access scenario 1. Year 2025 Initial Build (North and South Driveways on CTH H) traffic volumes were determined by adding the Year 2025 Background traffic volumes ([Exhibit 4-1](#)) to the Initial Build (North and South Driveways on CTH H) new trips ([Exhibit 4-5A](#)) and the Redistributed (North and South Driveways on CTH H) trips ([Exhibit 4-10A](#)). The Year 2025 Initial Build (North and South Driveways on CTH H) traffic volumes are shown in [Exhibit 4-11A](#).

The Year 2025 Initial Build (North Driveway Only on CTH H) traffic volumes assume the initial build-out of the Shoreland Lutheran High School under access scenario 2. Year 2025 Initial Build (North Driveway Only on CTH H) traffic volumes were determined by adding the Year 2025 Background traffic volumes ([Exhibit 4-1](#)) to the Initial Build (North Driveway Only on CTH H) new trips ([Exhibit 4-5B](#)) and the Redistributed (North Driveway Only on CTH H) trips ([Exhibit 4-10B](#)). The Year 2025 Initial Build (North Driveway Only on CTH H) traffic volumes are shown in [Exhibit 4-11B](#).

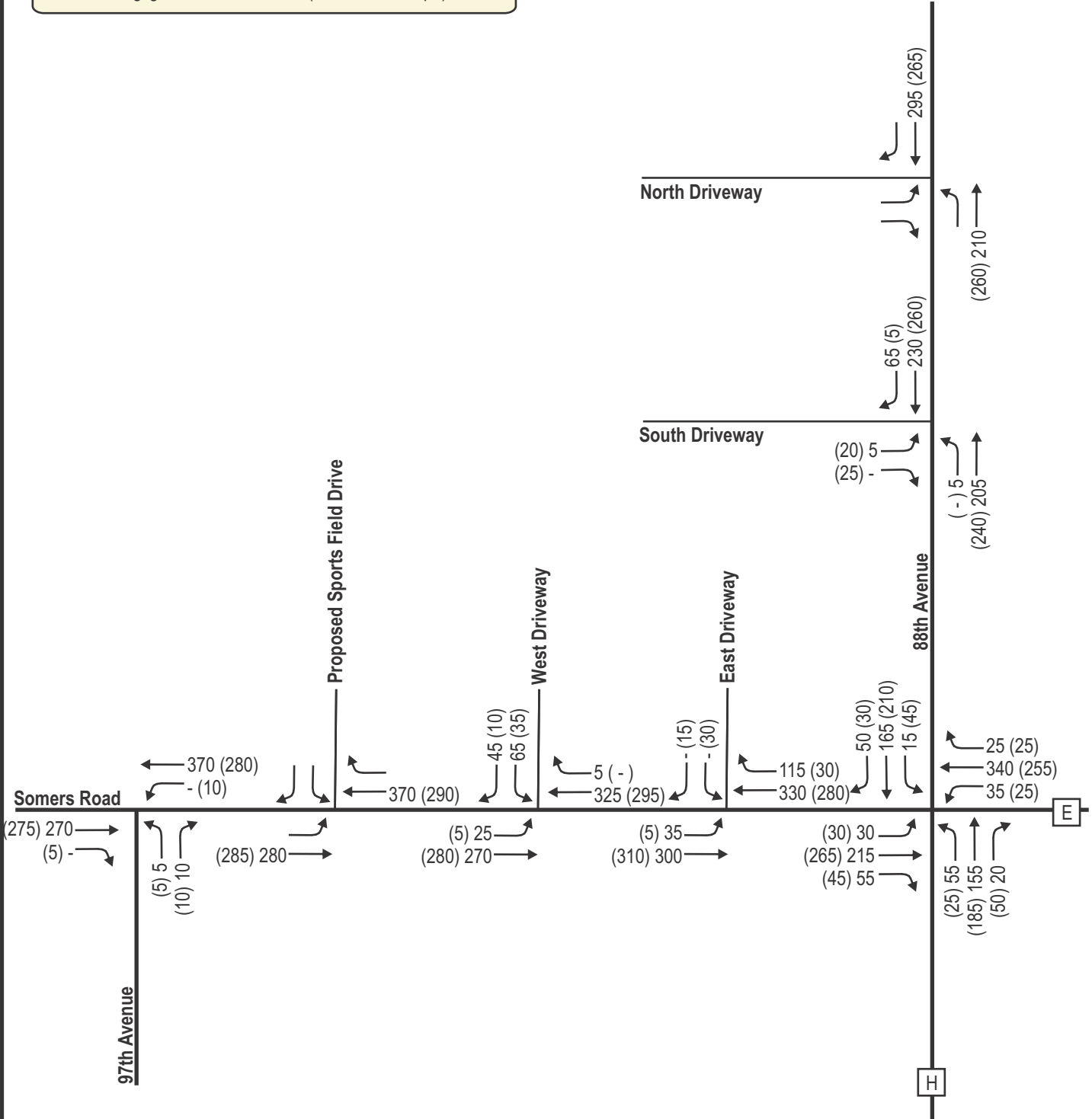
### **B3. Full Build Traffic**

The Year 2035 Full Build (North and South Driveways on CTH H) traffic volumes assume the full build-out of the Shoreland Lutheran High School under access scenario 1. Year 2035 Full Build (North and South Driveways on CTH H) traffic volumes were determined by adding the Year 2035 Background traffic volumes ([Exhibit 4-2B](#)) to the Full Build (North and South Driveways on CTH H) new trips ([Exhibit 4-8A](#)) and the Redistributed (North and South Driveways on CTH H) trips ([Exhibit 4-10A](#)). The Year 2035 Full Build (North and South Driveways on CTH H) traffic volumes are shown in [Exhibit 4-14A](#).

The Year 2035 Full Build (North Driveway Only on CTH H) traffic volumes assume the full build-out of the Shoreland Lutheran High School under access scenario 2. Year 2035 Full Build (North Driveway Only on CTH H) traffic volumes were determined by adding the Year 2035 Background traffic volumes ([Exhibit 4-2B](#)) to the Full Build (North Driveway Only on CTH H) new trips ([Exhibit 4-8B](#)) and the Redistributed (North Driveway Only on CTH H) trips ([Exhibit 4-10B](#)). The Year 2035 Full Build (North Driveway Only on CTH H) traffic volumes are shown in [Exhibit 4-14B](#).

**LEGEND**

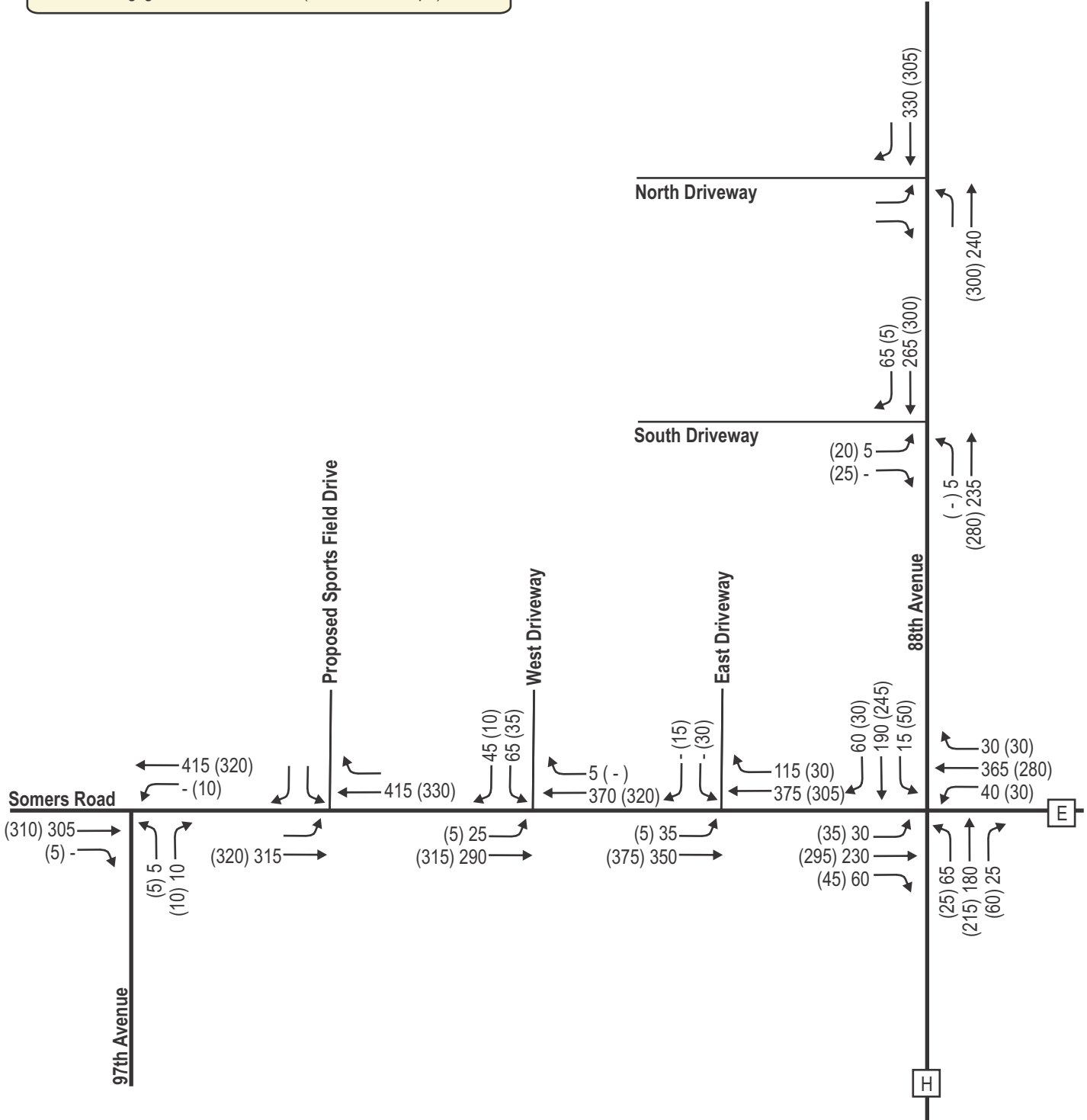
- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



NOT TO SCALE

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

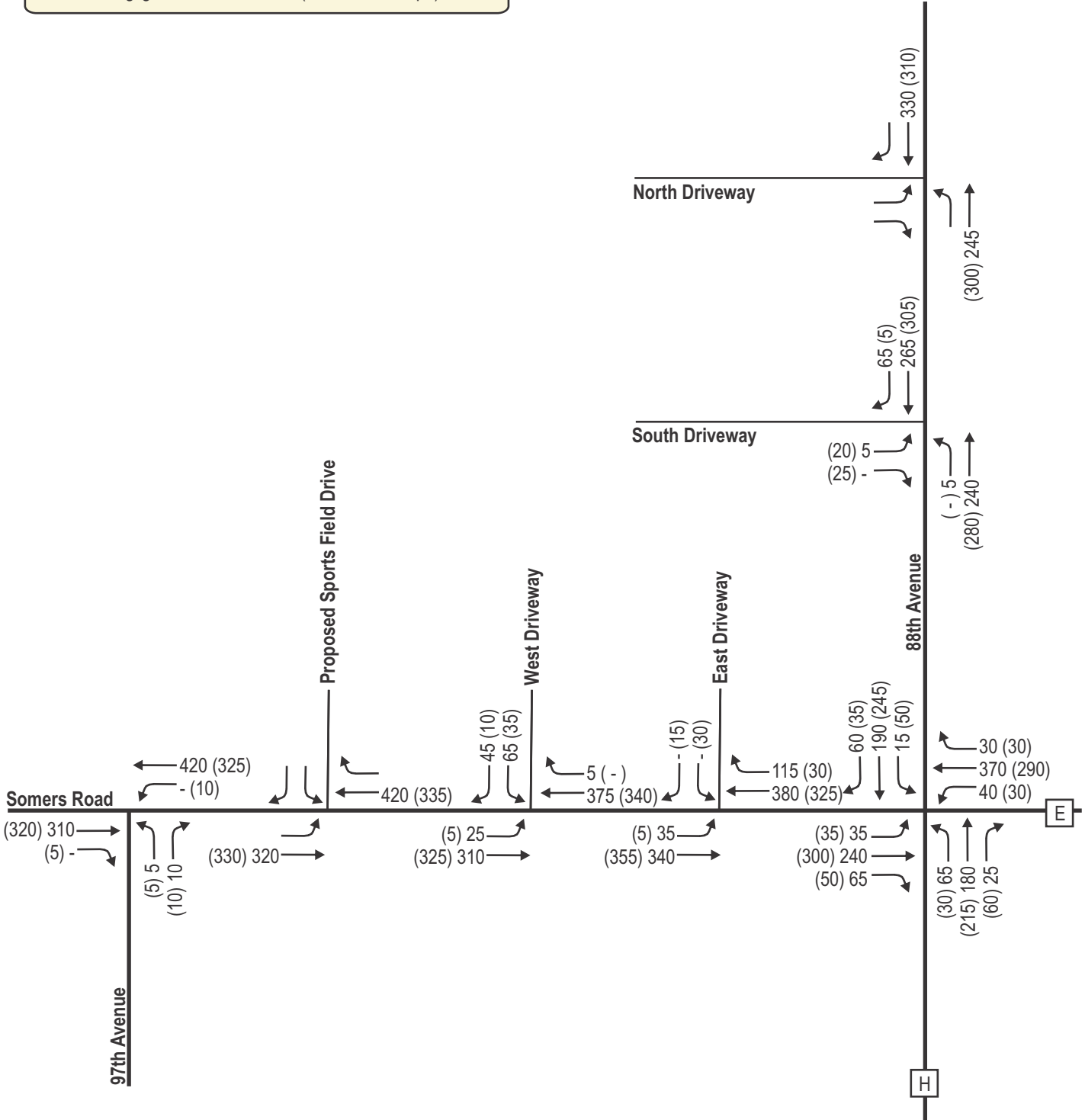


**EXHIBIT 4-2A  
YEAR 2035 PROJECTED TRAFFIC VOLUMES  
BACKGROUND GROWTH ONLY**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



**EXHIBIT 4-2B**  
**YEAR 2035 BACKGROUND TRAFFIC VOLUMES**  
**INCLUDES OFFSITE DEVELOPMENT**

**SOMERS, WISCONSIN**

**Exhibit 4-3A**  
**Opening Year (Initial Build) Trip Generation Table<sup>1</sup>**

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Private High School (Additional Students)	TADI	25 Students	50 (2.17)	15 (68%)	5 (32%)	20 (0.81)	5 (25%)	5 (75%)	10 (0.40)
<b>Total New Trips</b>			<b>50</b>	<b>15</b>	<b>5</b>	<b>20</b>	<b>5</b>	<b>5</b>	<b>10</b>

<sup>1</sup> Daily volumes calculated using ITE Trip Rates (X.XX) from the ITE Trip Generation Manual, 11th Edition. Peak hour volumes based on calculated rates using actual driveway counts conducted in early September of 2024.

**TRIP DISTRIBUTION (New Trips)**

East on CTH E	50%	25	10	5	5	5
West on CTH E	35%	15	5	0	0	0
North on CTH H	10%	10	0	0	0	0
South on CTH H	5%	0	0	0	0	0
	<b>100%</b>	<b>50</b>	<b>15</b>	<b>5</b>	<b>5</b>	<b>5</b>

**Exhibit 4-3B**  
**Design Year (Full Build) Trip Generation Table<sup>1</sup>**

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Private High School (Additional Students)	TADI	100 Students	220 (2.17)	55 (68%)	25 (32%)	80 (0.81)	10 (25%)	30 (75%)	40 (0.40)
<b>Total New Trips</b>			<b>220</b>	<b>55</b>	<b>25</b>	<b>80</b>	<b>10</b>	<b>30</b>	<b>40</b>

<sup>1</sup> Daily volumes calculated using ITE Trip Rates (X.XX) from the ITE Trip Generation Manual, 11th Edition. Peak hour volumes based on calculated rates using actual driveway counts conducted in early September of 2024.

**TRIP DISTRIBUTION (New Trips)**

East on CTH E	50%	110	25	10	5	15
West on CTH E	35%	80	20	10	5	10
North on CTH H	10%	20	5	5	0	5
South on CTH H	5%	10	5	0	0	0
	<b>100%</b>	<b>220</b>	<b>55</b>	<b>25</b>	<b>10</b>	<b>30</b>

**Exhibit 4-3C**

**Off-Site (Beta Terra 45 Residential) Trip Generation Table <sup>1</sup>**

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Single-Family Housing	210	45 Units	500 FCE	10 (25%)	25 (75%)	35 FCE	30 (63%)	15 (37%)	45 FCE
<b>Total New Trips</b>			<b>500</b>	<b>10</b>	<b>25</b>	<b>35</b>	<b>30</b>	<b>15</b>	<b>45</b>

<sup>1</sup> ITE Trip Rates (X.XX) and/or Fitted Curve Equations (FCE) are from the ITE Trip Generation Manual, 11th Edition.

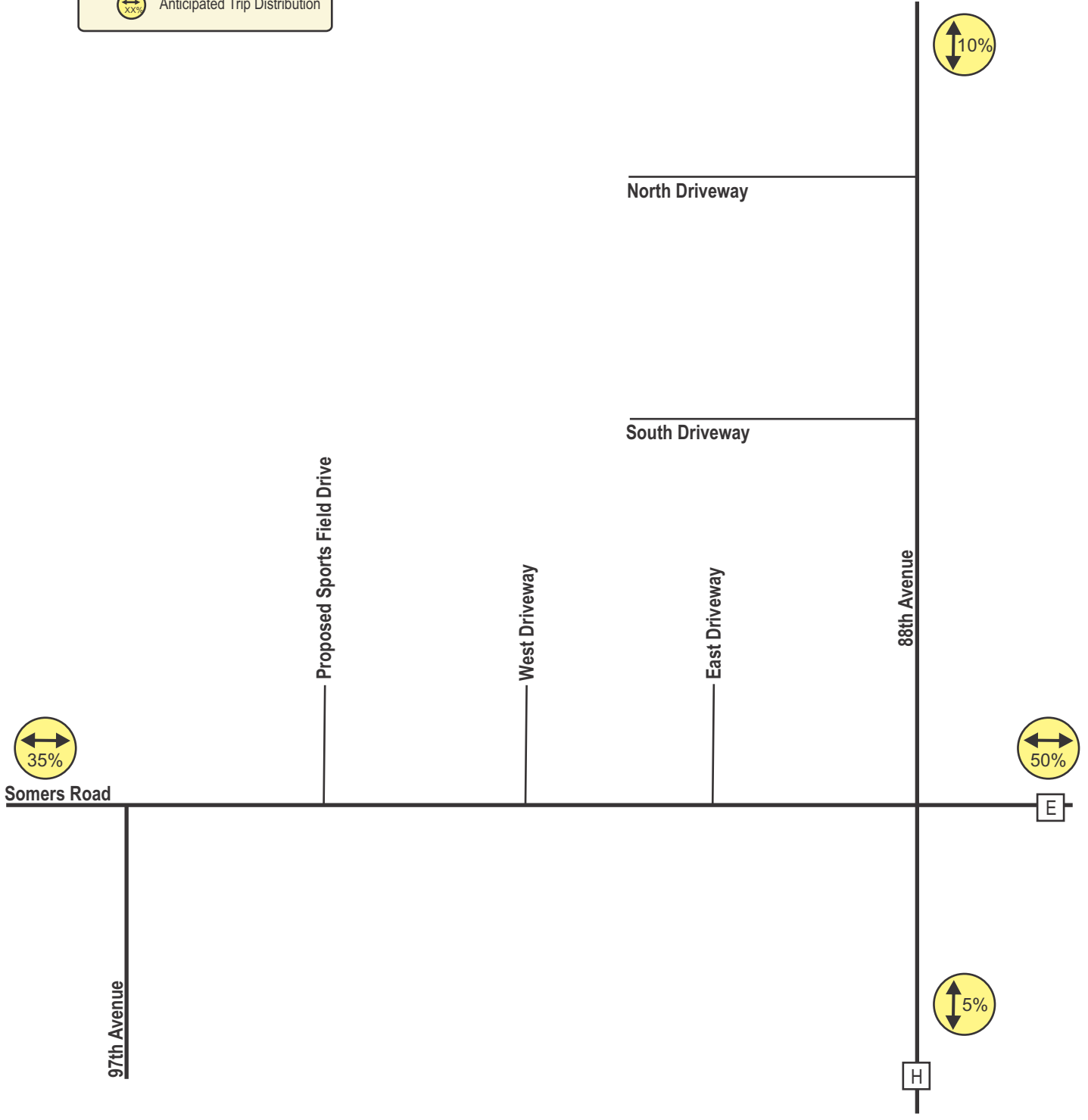
**TRIP DISTRIBUTION (New Trips)**

East on CTH E	30%	150	5	10	10	5
West on CTH E	30%	150	5	5	10	5
North on CTH H	15%	80	0	5	5	0
South on CTH H	25%	130	0	5	5	5
	<b>100%</b>	<b>510</b>	<b>10</b>	<b>25</b>	<b>30</b>	<b>15</b>

**LEGEND**

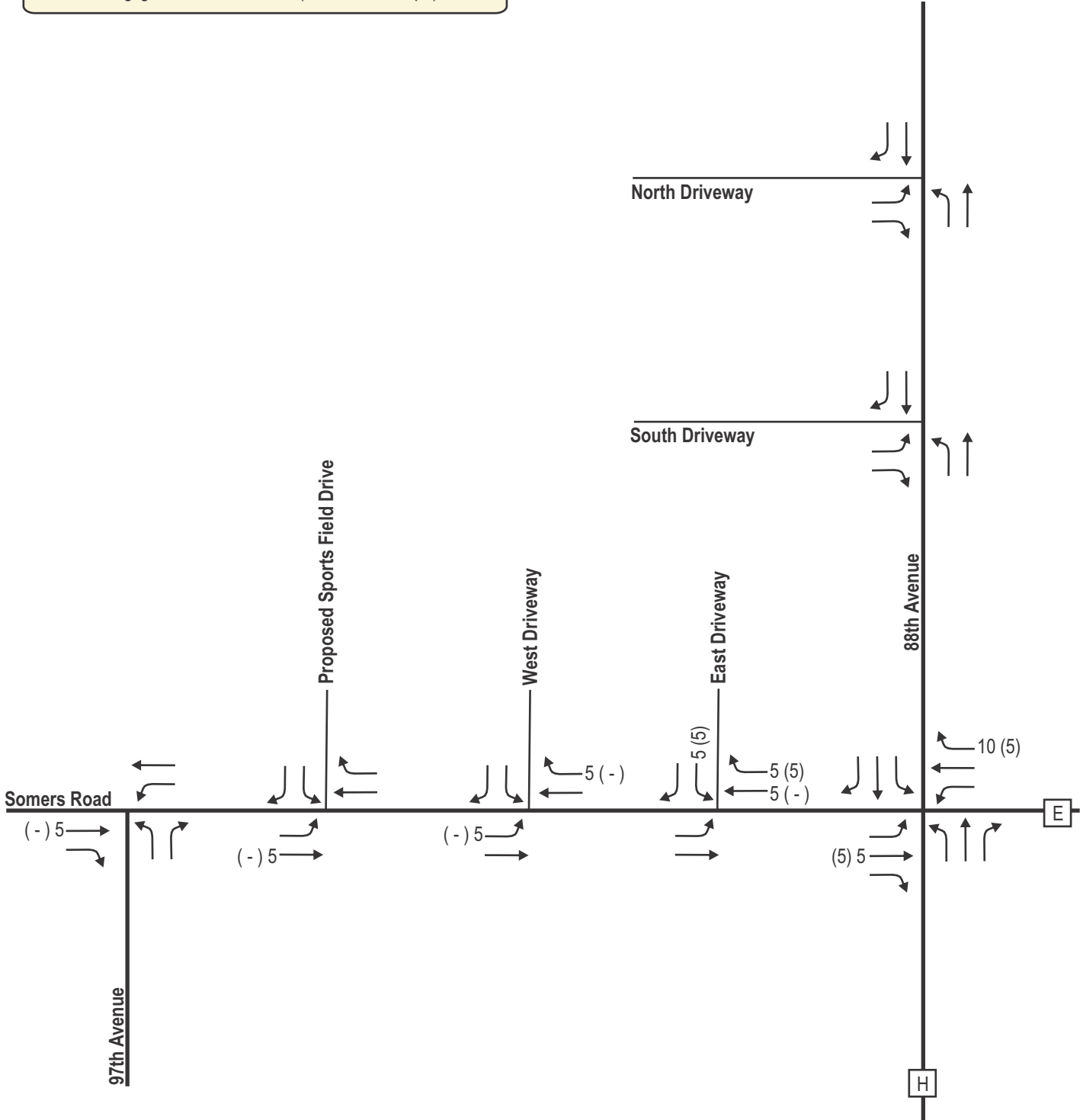


Anticipated Trip Distribution



**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

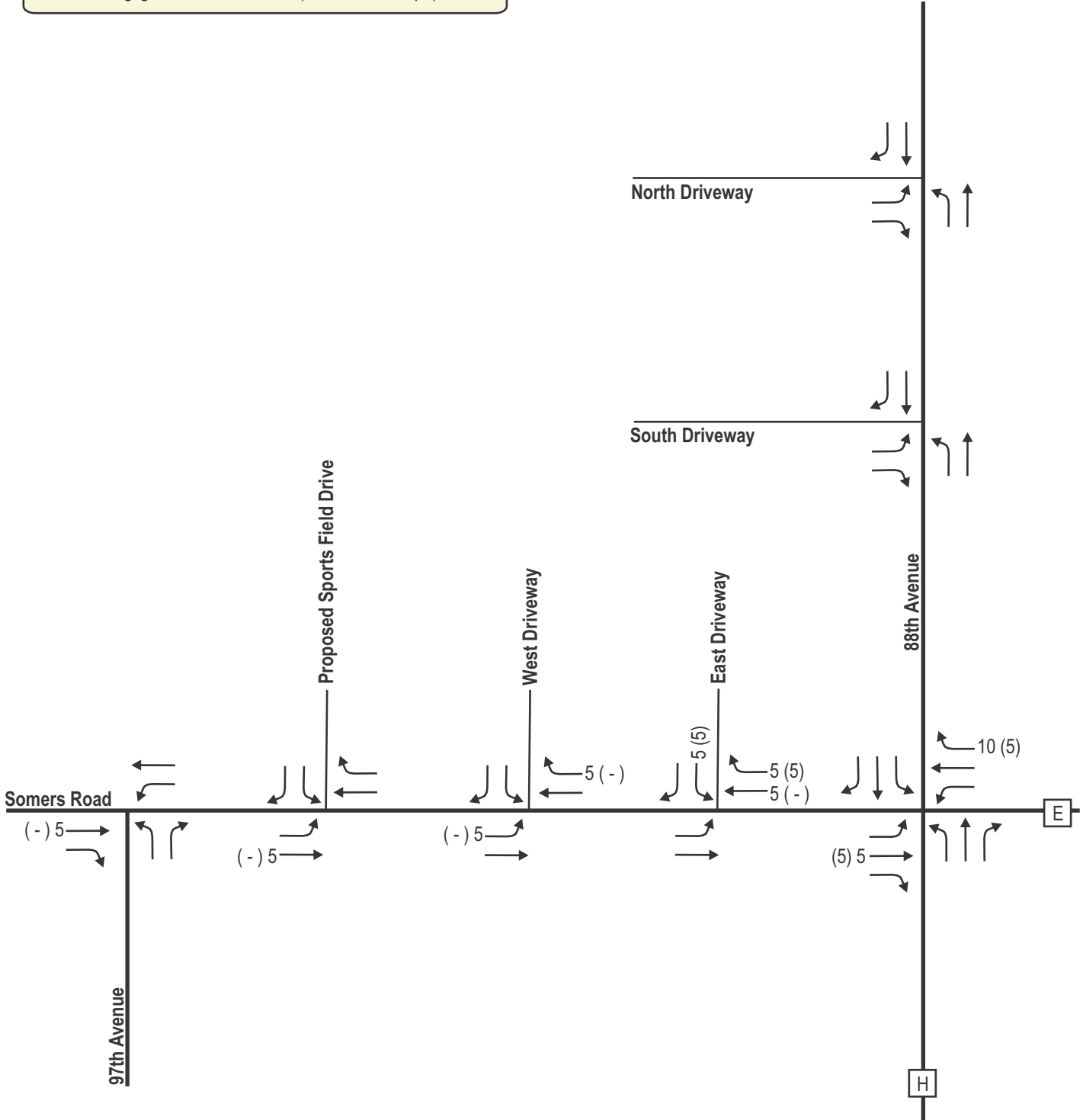


**EXHIBIT 4-5A  
INITIAL BUILD NEW TRIPS  
NORTH AND SOUTH DRIVEWAYS ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

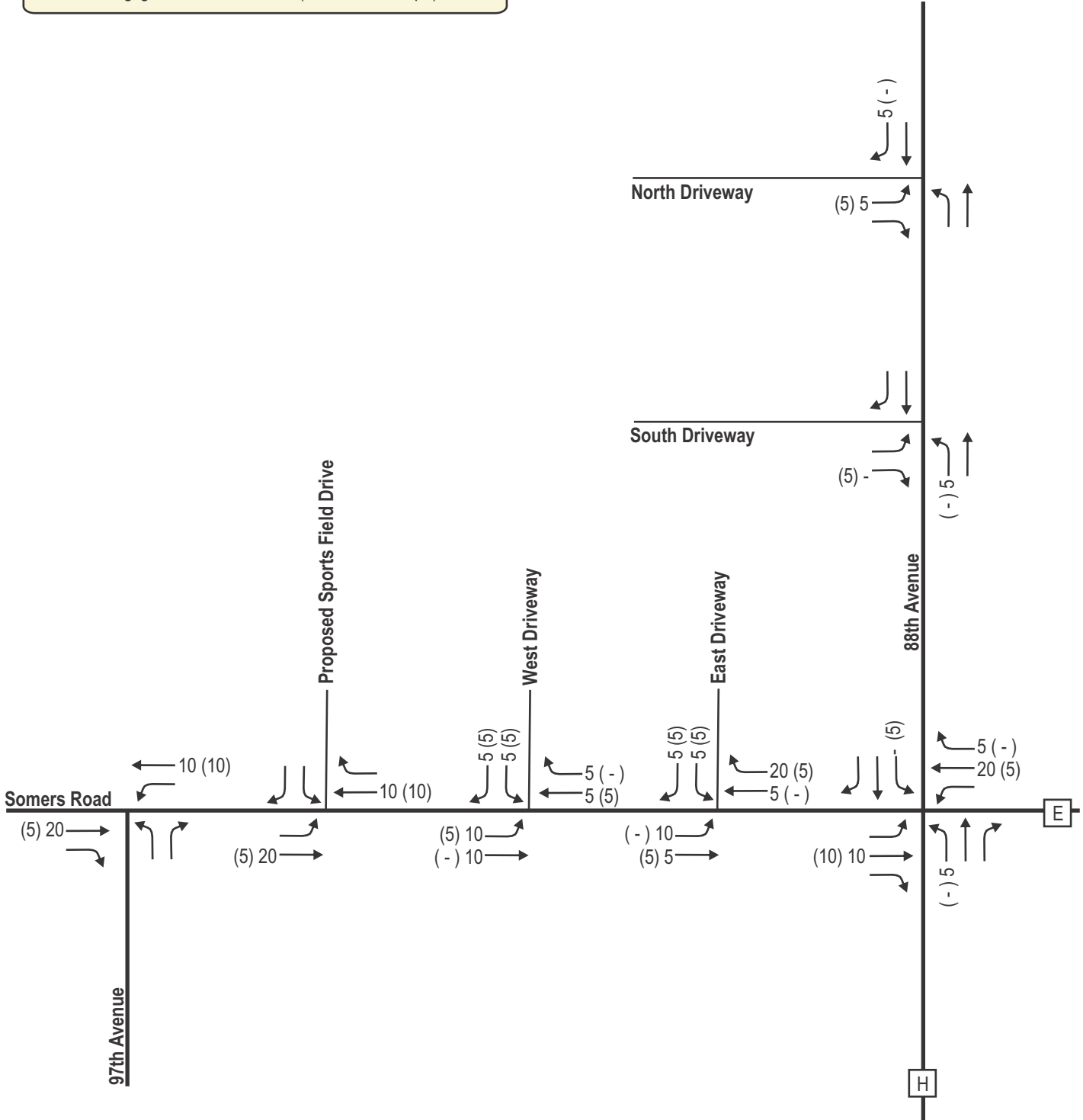


**EXHIBIT 4-5B  
INITIAL BUILD NEW TRIPS  
NORTH DRIVEWAY ONLY ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

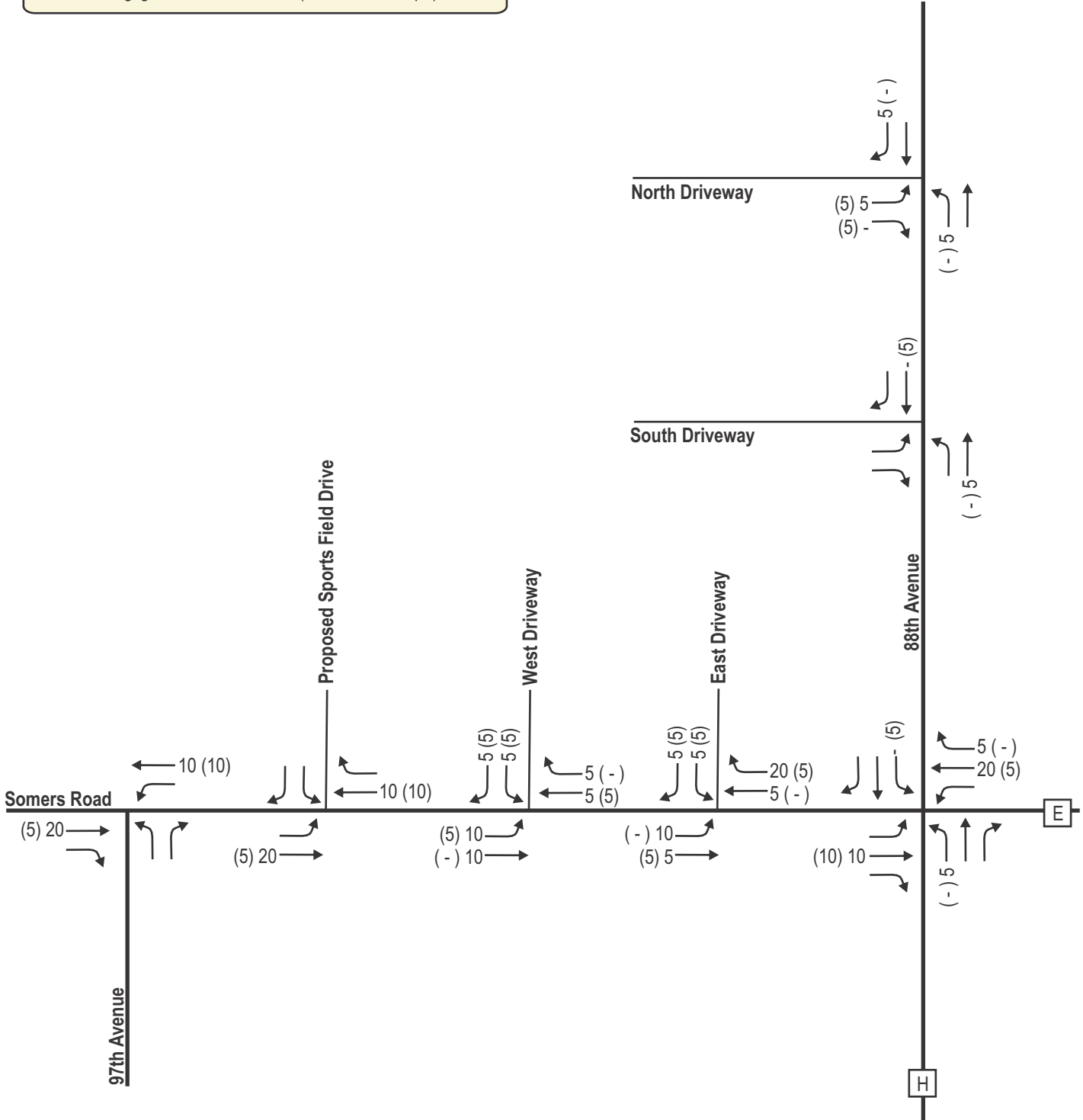


**EXHIBIT 4-8A  
FULL BUILD NEW TRIPS  
NORTH AND SOUTH DRIVEWAYS ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

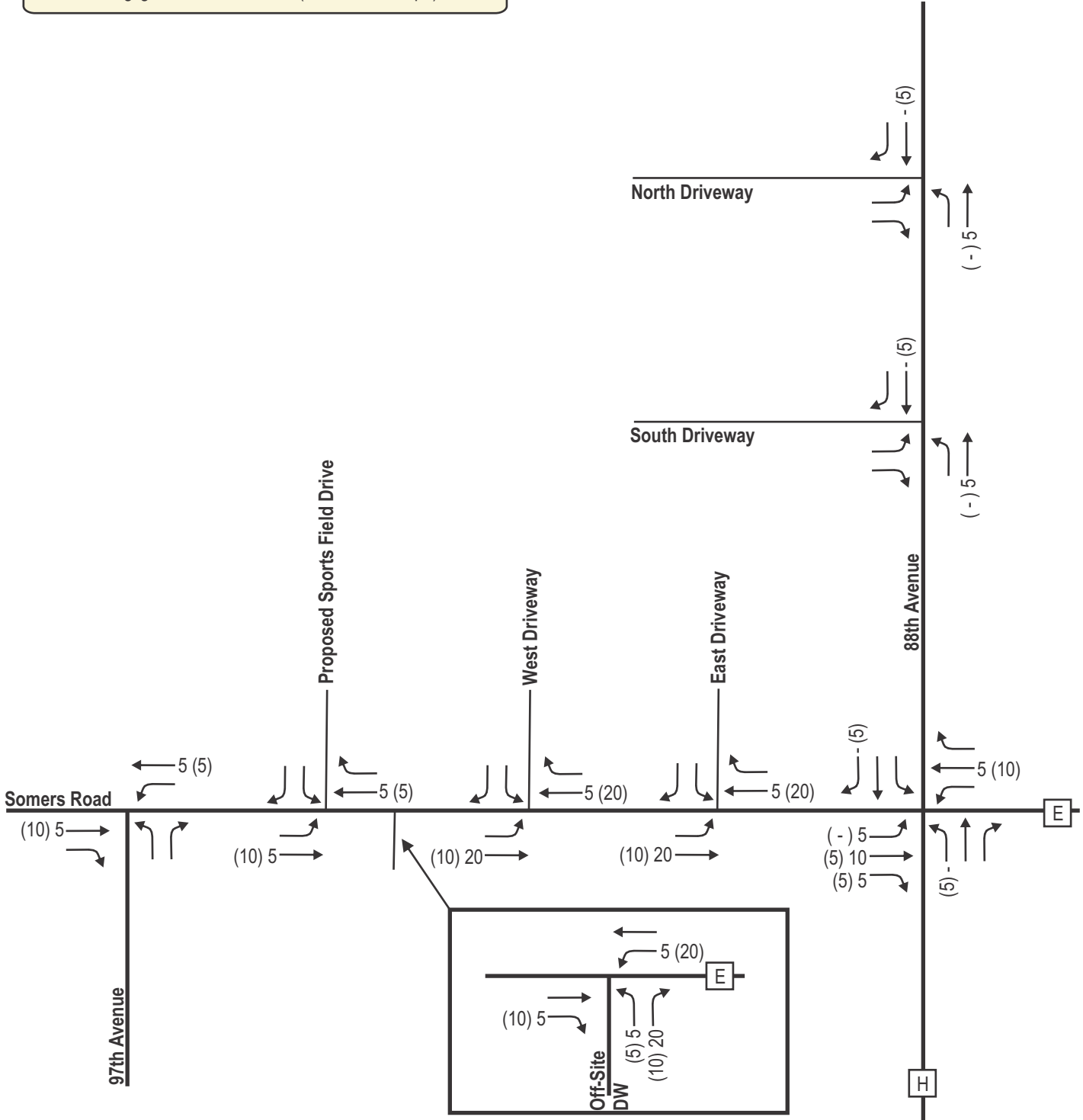


**EXHIBIT 4-8B  
FULL BUILD NEW TRIPS  
NORTH DRIVEWAY ONLY ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

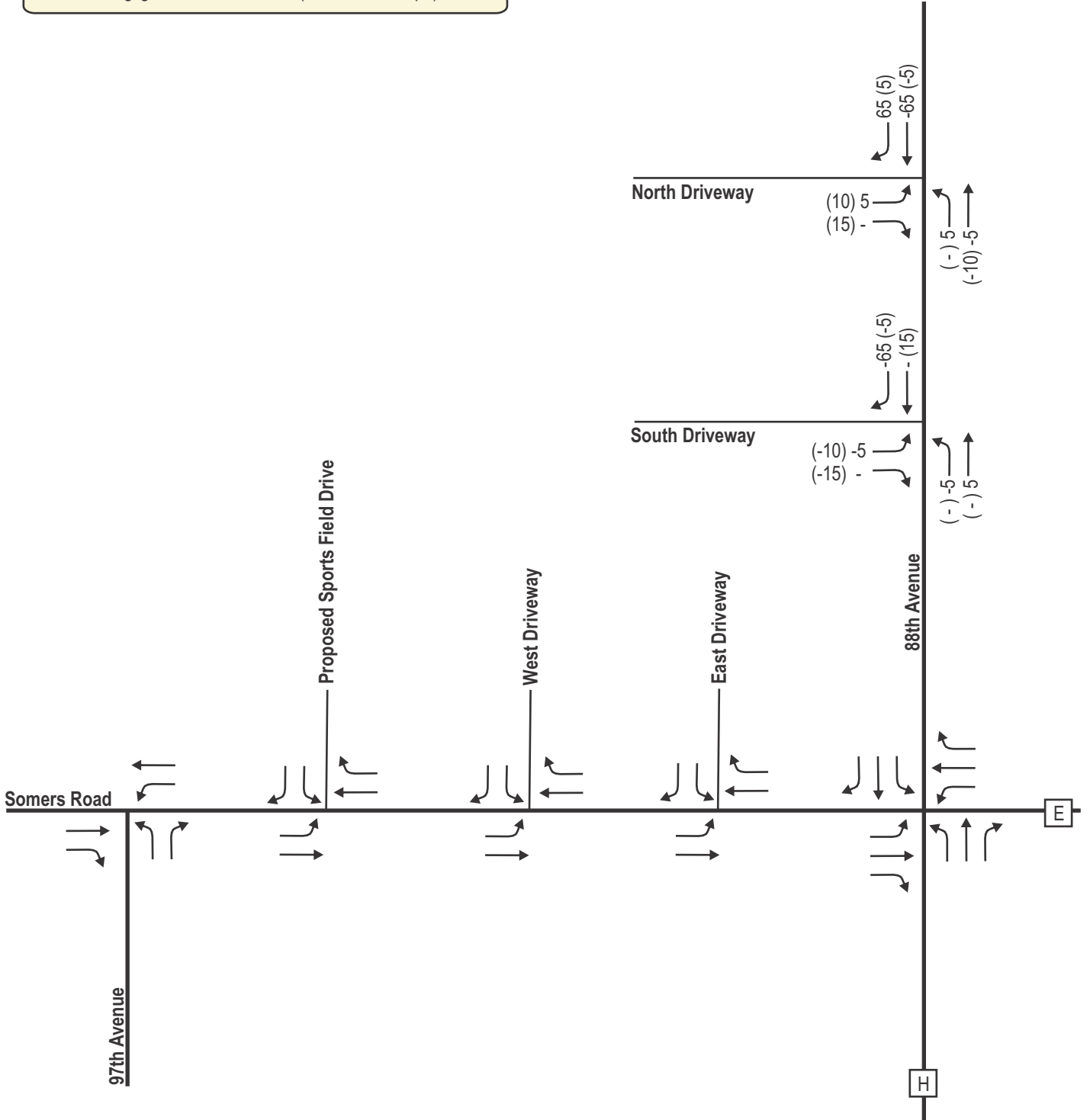


**EXHIBIT 4-9  
OFF-SITE NEW TRIPS**

**SOMERS, WISCONSIN**

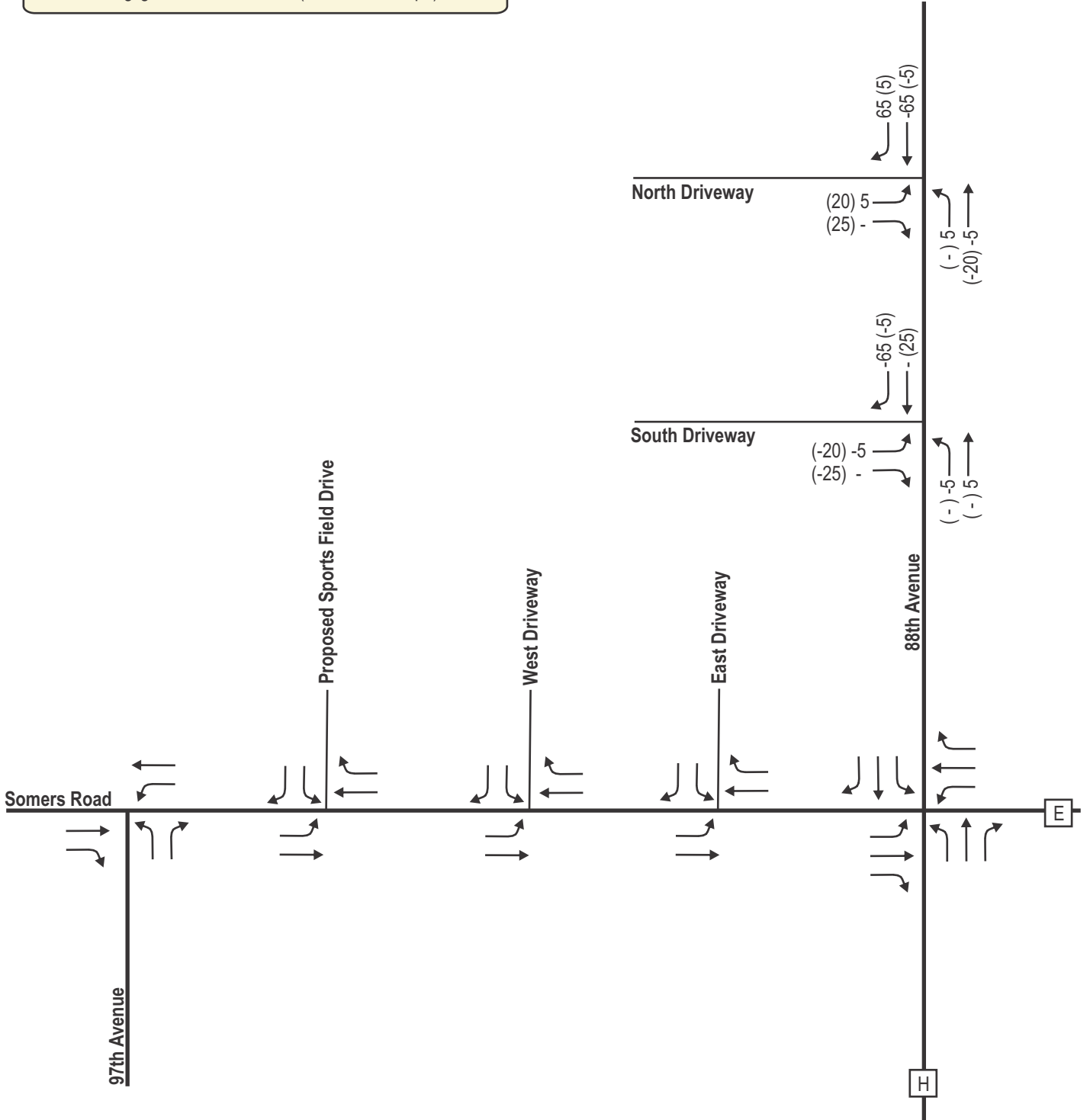
**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



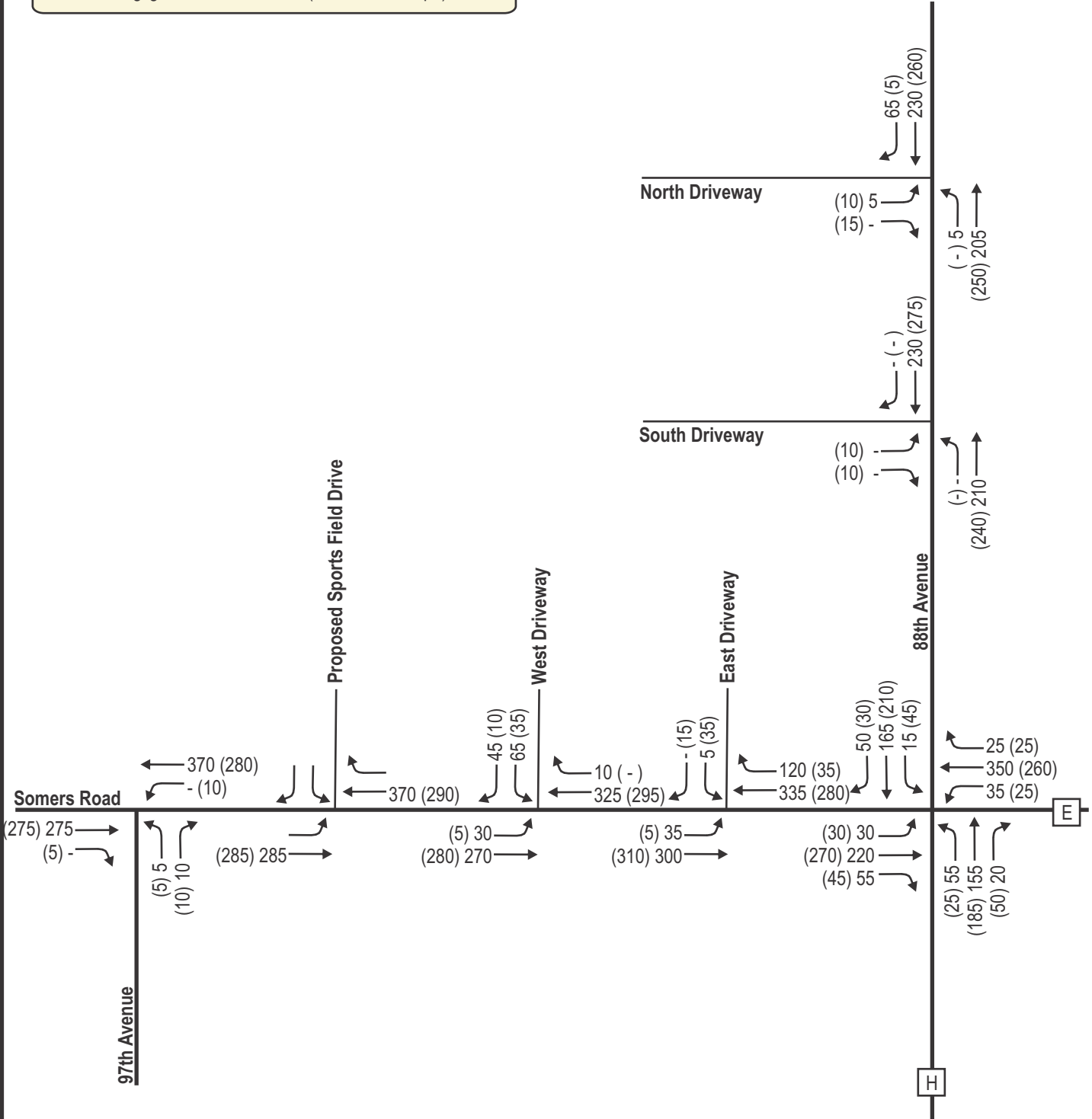
**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

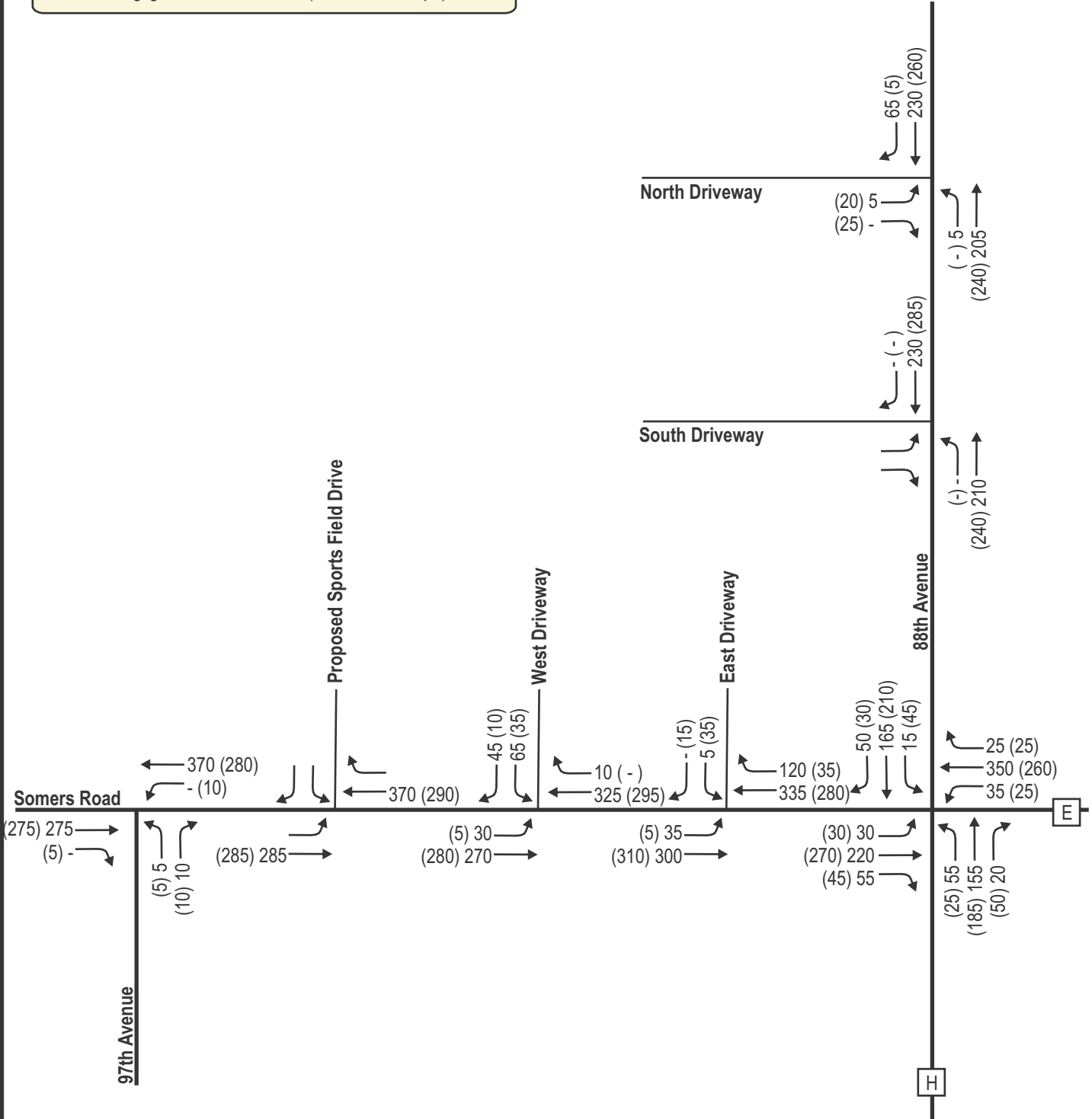


**EXHIBIT 4-11A  
YEAR 2025 INITIAL BUILD TRAFFIC VOLUMES  
NORTH AND SOUTH DRIVEWAYS ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



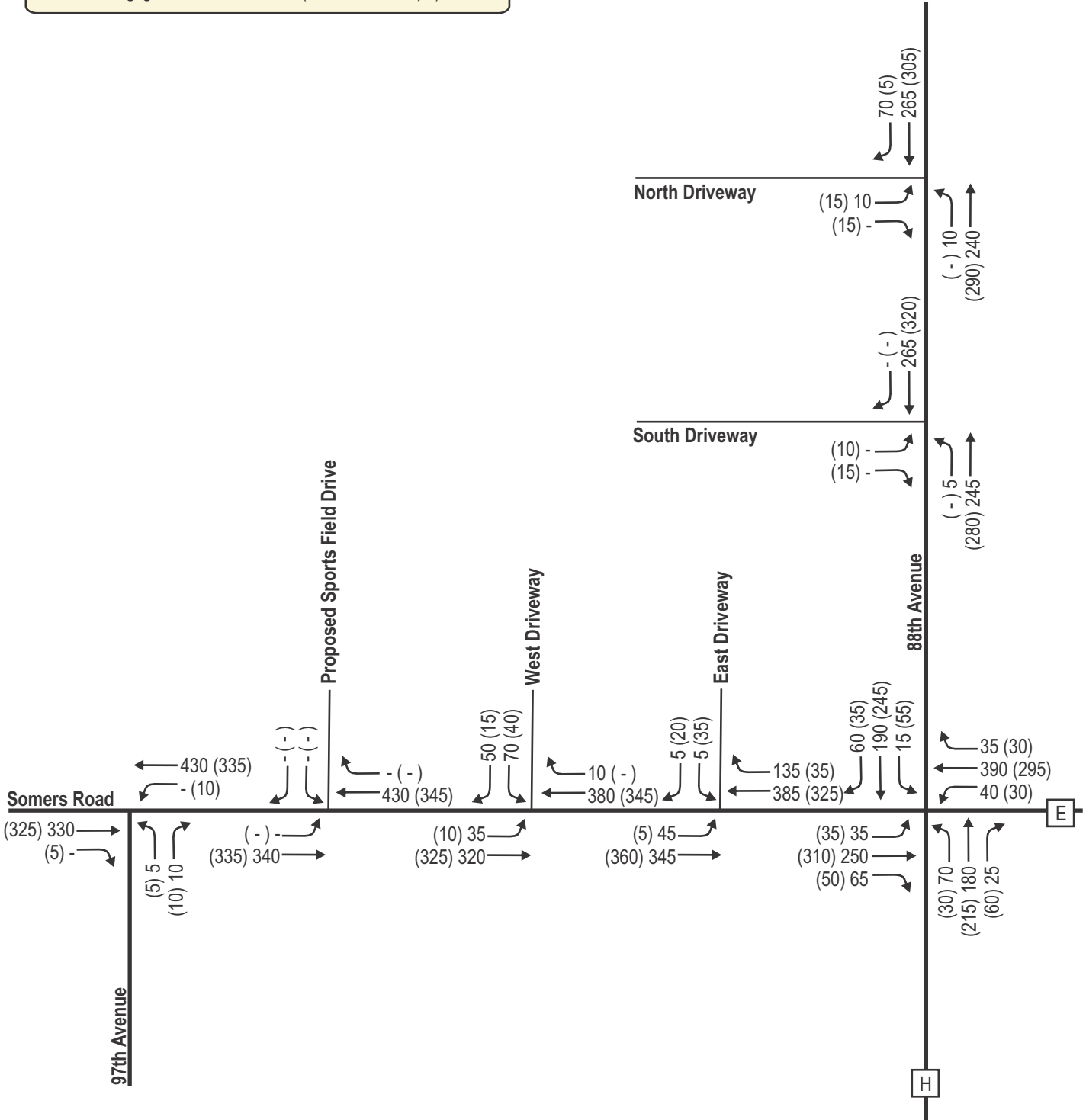
NOT TO SCALE

**EXHIBIT 4-11B  
YEAR 2025 INITIAL BUILD TRAFFIC VOLUMES  
NORTH DRIVEWAY ONLY ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)

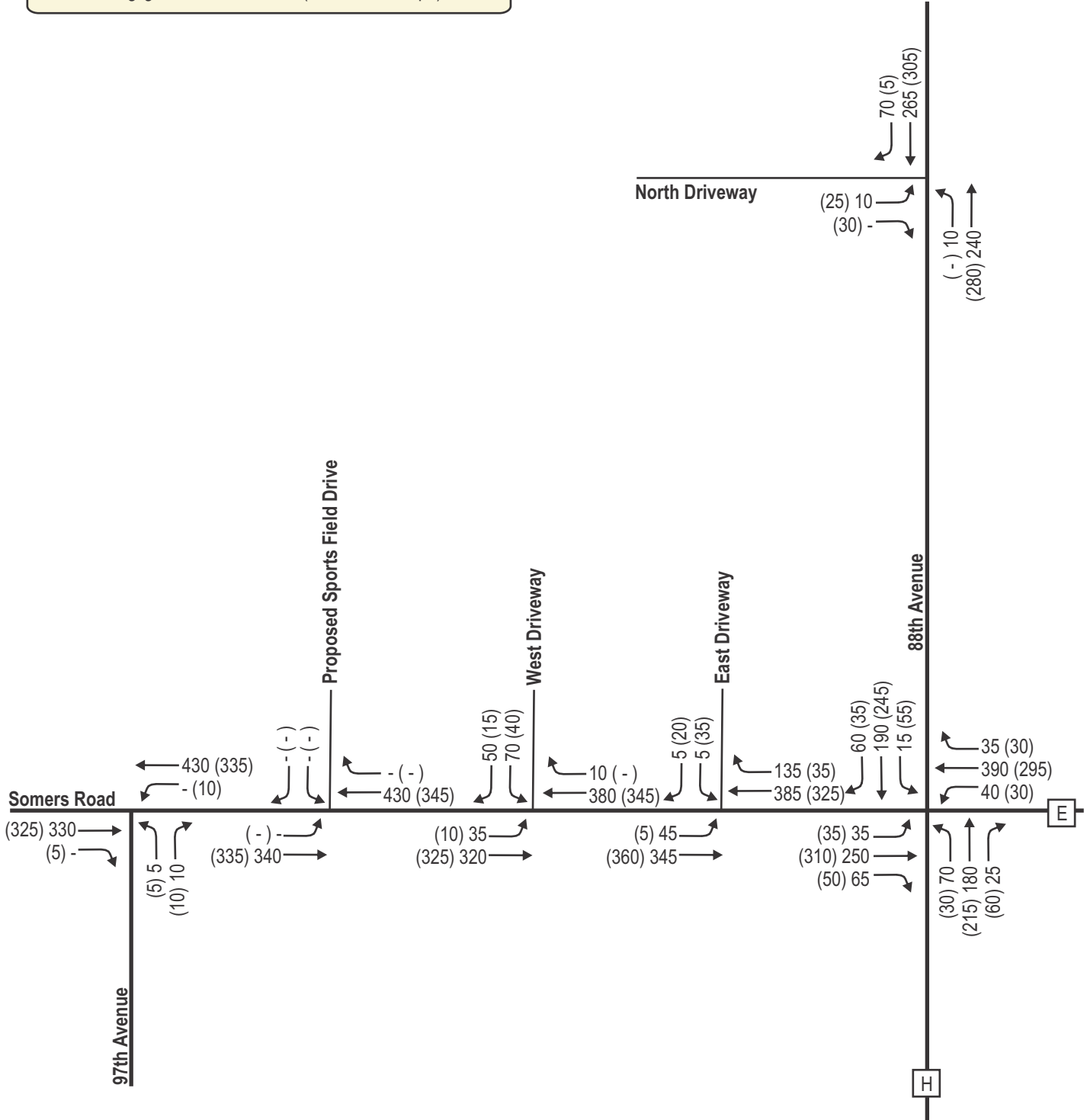


**EXHIBIT 4-14A  
YEAR 2035 FULL BUILD TRAFFIC VOLUMES  
NORTH AND SOUTH DRIVEWAYS ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**

- XX AM Weekday Peak Hour (7:00 - 8:00 AM)
- (XX) PM Weekday Peak Hour (3:00 - 4:00 PM)
- Negligible Traffic Volumes (Fewer than 3 vph)



**EXHIBIT 4-14B  
YEAR 2035 FULL BUILD TRAFFIC VOLUMES  
NORTH DRIVEWAY ONLY ON CTH H**

**SOMERS, WISCONSIN**

## CHAPTER V – TRAFFIC AND IMPROVEMENT ANALYSIS

### PART A – FUTURE CAPACITY LEVEL OF SERVICE

#### A1. Year 2025 Background Traffic Operating Conditions

[Exhibit 5-1](#) shows the Year 2025 Background traffic peak hour operating conditions at the study area intersections without modifications.

Based on the analysis, the study area intersections are expected to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2025 Background traffic peak hour operating conditions, which includes the identified off-site development, except the following:

- The westbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS F during the typical weekday morning arrival peak hour.
- The eastbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS E during the typical weekday afternoon discharge peak hour.

#### A2. Year 2035 Background Traffic Operating Conditions

[Exhibit 5-2](#) shows the Year 2035 Background traffic peak hour operating conditions at the study area intersections without modifications.

Based on the analysis, the study area intersections are expected to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2035 Background traffic peak hour operating conditions, which includes typical background growth as well as the identified off-site development, except the following:

- The eastbound, westbound, northbound, and southbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS F during the typical weekday morning and afternoon discharge peak hours.

#### A3. Year 2025 Initial Build Traffic Operating Conditions

As previously discussed, two access scenarios were evaluated as part of this study to analyze the operation of the two potential driveways along CTH H. [Exhibits 5-3A&B](#) show the Year 2025 Initial Build traffic peak hour operating conditions at the study area intersections for the two respective access scenarios along CTH H, without additional modifications. Based on the analysis, *regardless of the access scenario evaluated*, the study area intersections are expected to continue to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2025 Initial Build traffic peak hour operating conditions except:

- The westbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS F during the typical weekday morning arrival peak hour.
- The eastbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS E during the typical weekday afternoon discharge peak hour.

#### A4. Year 2035 Full Build Traffic Operating Conditions

[Exhibits 5-5A&B](#) show the Year 2035 Full Build traffic peak hour operating conditions at the study area intersections for the two respective access scenarios along CTH H, without additional

modifications. Based on the analysis, *regardless of the access scenario evaluated*, the study area intersections are expected to continue to operate acceptably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours under the Year 2035 Full Build traffic peak hour operating conditions except:

- The eastbound, westbound, northbound, and southbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS F during the typical weekday morning and afternoon discharge peak hours.

#### **A5. Traffic Operating Conditions – *With Recommended Modifications***

Modifications to accommodate the background and build traffic volumes are summarized in *Chapter VI – Recommendations and Conclusion*. The following outlines the location of the expected peak hour operating conditions with modifications at the study area intersections. The analysis outputs are included in the [Appendix](#) of this report.

- [Exhibit 5-9](#) – Year 2025 Background Traffic Operations – *With Modifications*
- [Exhibit 5-11A](#) – Year 2035 Background Traffic Operations – *With Modifications*
- [Exhibit 5-12A](#) – Year 2025 Initial Build (North and South Driveways on CTH H) Traffic Operations – *With Modifications*
- [Exhibit 5-12B](#) – Year 2025 Initial Build (North Driveway Only on CTH H) Traffic Operations – *With Modifications*
- [Exhibit 5-14A](#) – Year 2035 Full Build (North and South Driveways on CTH H) Traffic Operations – *With Modifications*
- [Exhibit 5-14B](#) – Year 2035 Full Build (North Driveway Only on CTH H) Traffic Operations – *With Modifications*

As shown, all movements are expected to operate desirably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours with recommended modifications with the modified geometry except the southbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS E (5 seconds over threshold) during the typical weekday afternoon discharge peak hours under background traffic conditions and the northbound and southbound movements at the Somers Road intersection with 88<sup>th</sup> Avenue which are expected to operate at LOS E (2 seconds and 8 seconds over threshold, respectively) during the typical weekday afternoon discharge peak hours under full build traffic conditions.

A sensitivity analysis was also completed at the Somers Road intersection with 88<sup>th</sup> Avenue looking at the operations if dual lanes were not only constructed along Somers Road (eastbound/westbound lanes) but also along 88<sup>th</sup> Avenue (northbound/southbound lanes) at the all-way stop-controlled intersection. With the modifications, all movements are expected to operate safely with moderate queue lengths as shown on the following exhibits.

- [Exhibit 5-11B](#) – Year 2035 Background Traffic Operations – *With Modifications, Sensitivity Analysis*
- [Exhibit 5-14C](#) – Year 2035 Full Build (North and South Driveways on CTH H) Traffic Operations – *With Modifications, Sensitivity Analysis*
- [Exhibit 5-14D](#) – Year 2035 Full Build (North Driveway Only on CTH H) Traffic Operations – *With Modifications, Sensitivity Analysis*

As shown, all movements are expected to operate desirably at LOS D or better conditions during the typical weekday morning arrival and weekday afternoon discharge peak hours with recommended modifications under future year background and full build traffic conditions.

## **PART B – QUEUEING ANALYSIS**

To estimate storage length requirements for turn bays at the study area intersections with modifications, a queuing analysis has been conducted. Note that the 95<sup>th</sup> percentile probable queue lengths were used for the design of turn bay storage at stop sign and traffic signal-controlled intersections. The following is a list of where the results of the queuing analysis can be found.

- Existing Traffic – [Exhibit 3-3](#)
- Year 2025 Background Traffic – [Exhibits 5-9 & 5-18](#)
- Year 2035 Background Traffic – [Exhibits 5-11A & 5-20A](#)
- Year 2035 Background Traffic, Sensitivity Analysis – [Exhibits 5-11B & 5-20B](#)
- Year 2025 Initial Build (North and South Driveways on CTH H) Traffic – [Exhibits 5-12A & 5-21A](#)
- Year 2025 Initial Build (North Driveway Only on CTH H) Traffic – [Exhibits 5-12B & 5-21B](#)
- Year 2035 Full Build (North and South Driveways on CTH H) Traffic – [Exhibits 5-14A & 5-23A](#)
- Year 2035 Full Build (North Driveway Only on CTH H) Traffic – [Exhibits 5-14B & 5-23B](#)
- Year 2035 Full Build (North and South Driveways on CTH H) Traffic, Sensitivity Analysis – [Exhibits 5-14C & 5-23C](#)
- Year 2035 Full Build (North Driveway Only on CTH H) Traffic, Sensitivity Analysis – [Exhibits 5-14D & 5-23D](#)

**Exhibit 5-1**  
**Year 2025 Background Traffic Peak Hour Operating Conditions**  
**With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	<i>Lanes-&gt;</i>		-	1		1	-		1					
	AM	LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.6					
		Queue	-	*		25'	-		25'					
	PM	LOS	-	*		A	-		B					
		Delay	-	*		7.9	-		11.3					
Queue		-	*		25'	-		25'						
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	<i>Lanes-&gt;</i>		1	-	-	1			-			1		
	AM	LOS	A	-	-	*			-			C		
		Delay	8.3	-	-	*			-			18.5		
		Queue	25'	-	-	*			-			40'		
	PM	LOS	A	-	-	*			-			B		
		Delay	8.0	-	-	*			-			13.7		
Queue		25'	-	-	*			-			25'			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	<i>Lanes-&gt;</i>		1	-	-	1			-			1		
	AM	LOS	A	-	-	*			-			C		
		Delay	8.9	-	-	*			-			15.4		
		Queue	25'	-	-	*			-			25'		
	PM	LOS	A	-	-	*			-			B		
		Delay	8.1	-	-	*			-			13.7		
Queue		25'	-	-	*			-			25'			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	<i>Lanes-&gt;</i>		1			1			1			1		
	AM	LOS	D			F			C			C		
		Delay	28.1			52.2			22.4			21.9		
		Queue	145'			285'			95'			95'		
	PM	LOS	E			D			C			D		
		Delay	36.6			30.7			24.9			28.1		
Queue		195'			155'			115'			140'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	<i>Lanes-&gt;</i>		1			-			1	-	-	1		
	AM	LOS	B			-			A	*	-	*		
		Delay	12.4			-			8.1	*	-	*		
		Queue	25'			-			25'	*	-	*		
	PM	LOS	B			-			A	*	-	*		
		Delay	12.3			-			8.0	*	-	*		
Queue		25'			-			25'	*	-	*			

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

**Exhibit 5-2**  
**Year 2035 Background Traffic Peak Hour Operating Conditions**  
**With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>		Lanes->	-	1		1	-		1		-			
	AM	LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.3		-			
		Queue	-	*		25'	-		25'		-			
	PM	LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		11.9		-			
Queue		-	*		25'	-		25'		-				
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	A	-	-	*	-	-	C		-			
		Delay	8.5	-	-	*	-	-	21.8		-			
		Queue	25'	-	-	*	-	-	45'		-			
	PM	LOS	A	-	-	*	-	-	C		-			
		Delay	8.1	-	-	*	-	-	15.1		-			
Queue		25'	-	-	*	-	-	25'		-				
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	A	-	-	*	-	-	C		-			
		Delay	9.1	-	-	*	-	-	17.0		-			
		Queue	25'	-	-	*	-	-	25'		-			
	PM	LOS	A	-	-	*	-	-	C		-			
		Delay	8.2	-	-	*	-	-	15.0		-			
Queue		25'	-	-	*	-	-	25'		-				
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	F	-	-	F	-	-	E	-	-	E		
		Delay	55.5	-	-	131.6	-	-	37.5	-	-	35.5		
		Queue	245'	-	-	495'	-	-	160'	-	-	150'		
	PM	LOS	F	-	-	F	-	-	F	-	-	F		
		Delay	120.4	-	-	88.4	-	-	61.6	-	-	74.5		
Queue		410'	-	-	315'	-	-	235'	-	-	275'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-	1	-	-	1	-	-	1		
	AM	LOS	B	-	-	A	*	-	*	-	-	*		
		Delay	13.2	-	-	8.2	*	-	*	-	-	*		
		Queue	25'	-	-	25'	*	-	*	-	-	*		
	PM	LOS	B	-	-	A	*	-	*	-	-	*		
		Delay	13.3	-	-	8.2	*	-	*	-	-	*		
Queue		25'	-	-	25'	*	-	*	-	-	*			

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

**Exhibit 5-3A**  
**Year 2025 Initial Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions**  
**With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.6		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.3		-			
Queue	-	*		25'	-		25'		-					
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	B		-			
		Delay	8.2	-	-	*	-	-	12.7		-			
	PM	Queue	25'	-	-	*	-	-	25'		-			
		LOS	A	-	-	*	-	-	B		-			
		Delay	7.9	-	-	*	-	-	11.5		-			
Queue	25'	-	-	*	-	-	25'		-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	C		-			
		Delay	8.3	-	-	*	-	-	19.0		-			
	PM	Queue	25'	-	-	*	-	-	40'		-			
		LOS	A	-	-	*	-	-	B		-			
		Delay	8.0	-	-	*	-	-	13.7		-			
Queue	25'	-	-	*	-	-	25'		-					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	C		-			
		Delay	9.0	-	-	*	-	-	18.5		-			
	PM	Queue	25'	-	-	*	-	-	25'		-			
		LOS	A	-	-	*	-	-	B		-			
		Delay	8.1	-	-	*	-	-	14.0		-			
Queue	25'	-	-	*	-	-	25'		-					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	AM	Lanes->	1			1		1		1		1		
		LOS	D			F		C		C				
		Delay	30.1			58.0		23.1		22.6				
	PM	Queue	160'			305'		100'		95'				
		LOS	E			D		D		D				
		Delay	39.1			32.5		25.6		28.9				
Queue	205'			165'		120'		140'						
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-		1		-		1		
		LOS	B			-		A		*		*		
		Delay	12.4			-		8.1		*		*		
	PM	Queue	25'			-		25'		*		*		
		LOS	B			-		A		*		*		
		Delay	11.8			-		8.0		*		*		
Queue	25'			-		25'		*		*				
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-		1		-		1		
		LOS	B			-		A		*		*		
		Delay	11.0			-		7.9		*		*		
	PM	Queue	25'			-		25'		*		*		
		LOS	B			-		A		*		*		
		Delay	12.2			-		8.1		*		*		
Queue	25'			-		25'		*		*				

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

**Exhibit 5-3B**  
**Year 2025 Initial Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions**  
**With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.6		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.3		-			
Queue	-	*		25'	-		25'		-					
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		B			
		Delay	8.2	-	-	*	-		-		12.7			
	PM	Queue	25'	-	-	*	-		-		25'			
		LOS	A	-	-	*	-		-		B			
		Delay	7.9	-	-	*	-		-		11.5			
Queue	25'	-	-	*	-		-		25'					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	8.3	-	-	*	-		-		19.0			
	PM	Queue	25'	-	-	*	-		-		40'			
		LOS	A	-	-	*	-		-		B			
		Delay	8.0	-	-	*	-		-		13.7			
Queue	25'	-	-	*	-		-		25'					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	9.0	-	-	*	-		-		18.5			
	PM	Queue	25'	-	-	*	-		-		25'			
		LOS	A	-	-	*	-		-		B			
		Delay	8.1	-	-	*	-		-		14.0			
Queue	25'	-	-	*	-		-		25'					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	AM	Lanes->	1			1			1			1		
		LOS	D			F			C			C		
		Delay	30.1			58.0			23.1			22.6		
	PM	Queue	160'			305'			100'			95'		
		LOS	E			D			D			D		
		Delay	39.1			32.5			25.6			28.9		
Queue	205'			165'			120'			140'				
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	1		
		LOS	B			-			A	*	-	*		
		Delay	12.4			-			8.1	*	-	*		
	PM	Queue	25'			-			25'	*	-	*		
		LOS	B			-			A	*	-	*		
		Delay	12.3			-			8.0	*	-	*		
Queue	25'			-			25'	*	-	*				

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

**Exhibit 5-5A**  
**Year 2035 Full Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions**  
**With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.5		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.0		-			
Queue	-	*		25'	-		25'		-					
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		1			
		LOS	A	-	-	*	-		B		-			
		Delay	8.4	-	-	*	-		13.8		-			
	PM	Queue	25'	-	-	*	-		25'		-			
		LOS	A	-	-	*	-		B		-			
		Delay	8.1	-	-	*	-		12.3		-			
Queue	25'	-	-	*	-		25'		-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		1			
		LOS	A	-	-	*	-		C		-			
		Delay	8.6	-	-	*	-		24.8		-			
	PM	Queue	25'	-	-	*	-		60'		-			
		LOS	A	-	-	*	-		C		-			
		Delay	8.2	-	-	*	-		15.3		-			
Queue	25'	-	-	*	-		25'		-					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		1			
		LOS	A	-	-	*	-		C		-			
		Delay	9.3	-	-	*	-		18.4		-			
	PM	Queue	25'	-	-	*	-		25'		-			
		LOS	A	-	-	*	-		C		-			
		Delay	8.2	-	-	*	-		15.2		-			
Queue	25'	-	-	*	-		25'		-					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>	AM	Lanes->	1			1			1			1		
		LOS	F			F			E			E		
		Delay	64.2			167.5			41.1			37.7		
	PM	Queue	265'			585'			170'			155'		
		LOS	F			F			F			F		
		Delay	130.1			95.4			61.4			77.0		
Queue	430'			330'			230'			280'				
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	1		
		LOS	B			-			A	*	-	*		
		Delay	13.6			-			8.2	*	-	*		
	PM	Queue	25'			-			25'	*	-	*		
		LOS	B			-			A	*	-	*		
		Delay	13.3			-			8.2	*	-	*		
Queue	25'			-			25'	*	-	*				
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	1		
		LOS	B			-			A	*	-	*		
		Delay	11.7			-			8.0	*	-	*		
	PM	Queue	25'			-			25'	*	-	*		
		LOS	B			-			A	*	-	*		
		Delay	12.8			-			8.2	*	-	*		
Queue	25'			-			25'	*	-	*				

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

**Exhibit 5-5B**  
**Year 2035 Full Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions**  
**With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>		Lanes->	-	1		1	-		1		-			
	AM	LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.5		-			
		Queue	-	*		25'	-		25'		-			
	PM	LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.0		-			
Queue		-	*		25'	-		25'		-				
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>		Lanes->	1	-	-		1		-		-		1	
	AM	LOS	A	-	-		*		-		-		B	
		Delay	8.4	-	-		*		-		-		13.8	
		Queue	25'	-	-		*		-		-		25'	
	PM	LOS	A	-	-		*		-		-		B	
		Delay	8.1	-	-		*		-		-		12.3	
Queue		25'	-	-		*		-		-		25'		
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-		1		-		-		1	
	AM	LOS	A	-	-		*		-		-		C	
		Delay	8.6	-	-		*		-		-		24.8	
		Queue	25'	-	-		*		-		-		60'	
	PM	LOS	A	-	-		*		-		-		C	
		Delay	8.2	-	-		*		-		-		15.3	
Queue		25'	-	-		*		-		-		25'		
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>		Lanes->	1	-	-		1		-		-		1	
	AM	LOS	A	-	-		*		-		-		C	
		Delay	9.3	-	-		*		-		-		18.4	
		Queue	25'	-	-		*		-		-		25'	
	PM	LOS	A	-	-		*		-		-		C	
		Delay	8.2	-	-		*		-		-		15.2	
Queue		25'	-	-		*		-		-		25'		
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <i>All-Way Stop Control</i>		Lanes->	1			1			1				1	
	AM	LOS	F			F			E				E	
		Delay	64.2			167.5			41.1				37.7	
		Queue	265'			585'			170'				155'	
	PM	LOS	F			F			F				F	
		Delay	130.1			95.4			61.4				77.0	
Queue		430'			330'			230'				280'		
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>		Lanes->	1			-			1	-	-		1	
	AM	LOS	B			-			A	*	-		*	
		Delay	13.9			-			8.3	*	-		*	
		Queue	25'			-			25'	*	-		*	
	PM	LOS	B			-			A	*	-		*	
		Delay	13.6			-			8.2	*	-		*	
Queue		25'			-			25'	*	-		*		

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

**Exhibit 5-9**  
**Year 2025 Background Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.6		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		7.9	-		11.3		-			
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	8.3	-	-	*	-		-		18.5			
	PM	Queue	25'	-	-	*	-		-		40'			
		LOS	A	-	-	*	-		-		B			
		Delay	8.0	-	-	*	-		-		13.7			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-		-		-	1		
		LOS	A	-	-	*	-		-		C			
		Delay	8.9	-	-	*	-		-		15.4			
	PM	Queue	25'	-	-	*	-		-		25'			
		LOS	A	-	-	*	-		-		B			
		Delay	8.1	-	-	*	-		-		13.7			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1	1	1	1		
		LOS	B		B	C		C	C	C	C	C		
		Delay	14.2		14.4	16.8		15.6	18.1	17.7				
	PM	Queue	40'		45'	65'		60'	80'	75'				
		LOS	C		C	C		B	C	C	C	C		
		Delay	15.4		15.4	15.2		14.7	19.7	21.6				
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-		1	-	-	1			
		LOS	B			-		A	*	-	*			
		Delay	12.4			-		8.1	*	-	*			
	PM	Queue	25'			-		25'	*	-	*			
		LOS	B			-		A	*	-	*			
		Delay	12.3			-		8.0	*	-	*			
Queue	25'			-		25'	*	-	*					

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes consist of a shared left/through lane and a shared right/through lane

**Exhibit 5-11A**  
**Year 2035 Background Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.3		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		11.9		-			
Queue	-	*		25'	-		25'		-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-	1	-	
		LOS	A	-	-	*	-	-	C	-	-	C	-	
		Delay	8.5	-	-	*	-	-	21.8	-	-	45'	-	
	PM	Queue	25'	-	-	*	-	-	25'	-	-	45'	-	
		LOS	A	-	-	*	-	-	C	-	-	C	-	
		Delay	8.1	-	-	*	-	-	15.1	-	-	25'	-	
Queue	25'	-	-	*	-	-	25'	-	-	25'	-			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-	1	-	
		LOS	A	-	-	*	-	-	C	-	-	C	-	
		Delay	9.1	-	-	*	-	-	17.0	-	-	25'	-	
	PM	Queue	25'	-	-	*	-	-	25'	-	-	25'	-	
		LOS	A	-	-	*	-	-	C	-	-	C	-	
		Delay	8.2	-	-	*	-	-	15.0	-	-	25'	-	
Queue	25'	-	-	*	-	-	25'	-	-	25'	-			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1	1	1	1	1	
		LOS	C		C	C		C	D	D	D	D	D	
		Delay	17.3		18.1	22.0		20.1	26.8	25.5	25.5	120'	120'	
	PM	Queue	55'		65'	95'		85'	130'	120'	120'	200'	200'	
		LOS	C		C	C		C	D	E	E	E	E	
		Delay	20.3		20.5	19.7		18.9	34.0	39.8	39.8	200'	200'	
Queue	70'		75'	65'		65'	170'	200'	200'	200'	200'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1	-	-	1		
		LOS	B			-			A	*	-	*		
		Delay	13.2			-			8.2	*	-	*		
	PM	Queue	25'			-			25'	*	-	*		
		LOS	B			-			A	*	-	*		
		Delay	13.3			-			8.2	*	-	*		
Queue	25'			-			25'	*	-	*				

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane

**Exhibit 5-11B**  
**Year 2035 Background Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control - Sensitivity Analysis**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1		1	-		1		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		12.3		-			
	PM	Queue	-	*		25'	-		25'		-			
		LOS	-	*		A	-		B		-			
		Delay	-	*		8.0	-		11.9		-			
Queue	-	*		25'	-		25'		-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1		-	-		1		-		1		
		LOS	A		-	-		*		-		C		
		Delay	8.5		-	-		*		-		21.8		
	PM	Queue	25'		-	-		*		-		45'		
		LOS	A		-	-		*		-		C		
		Delay	8.1		-	-		*		-		15.1		
Queue	25'		-	-		*		-		25'				
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1		-	-		1		-		1		
		LOS	A		-	-		*		-		C		
		Delay	9.1		-	-		*		-		17.0		
	PM	Queue	25'		-	-		*		-		25'		
		LOS	A		-	-		*		-		C		
		Delay	8.2		-	-		*		-		15.0		
Queue	25'		-	-		*		-		25'				
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1		1	1	1	
		LOS	C		C	C		C	C		B	B	C	
		Delay	16.2		16.9	20.1		18.4	17.3		14.3	14.6	16.0	
	PM	Queue	50'		60'	85'		75'	55'		30'	30'	50'	
		LOS	C		C	C		C	C		C	C	C	
		Delay	18.1		18.1	17.7		17.0	15.9		16.6	17.9	16.0	
Queue	65'		70'	60'		55'	40'		55'	60'	50'			
Node 700: 88th Avenue/CTH H & Proposed South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1			-			1		-	-	1	
		LOS	B			-			A		*	-	*	
		Delay	13.2			-			8.2		*	-	*	
	PM	Queue	25'			-			25'		*	-	*	
		LOS	B			-			A		*	-	*	
		Delay	13.3			-			8.2		*	-	*	
Queue	25'			-			25'		*	-	*			

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane

**Exhibit 5-12A**  
**Year 2025 Initial Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach										
			Eastbound			Westbound			Northbound		Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-
		LOS	-	*	A	-	B	-	-				
		Delay	-	*	7.9	-	11.6	-	-				
	PM	Queue	-	*	25'	-	25'	-	-				
		LOS	-	*	A	-	B	-	-				
		Delay	-	*	7.9	-	11.3	-	-				
Queue	-	*	25'	-	25'	-	-						
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1	
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.2	-	-	*	-	12.7	-	-			
	PM	Queue	25'	-	-	*	-	25'	-	-			
		LOS	A	-	-	*	-	B	-	-			
		Delay	7.9	-	-	*	-	11.5	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1	
		LOS	A	-	-	*	-	C	-	-			
		Delay	8.3	-	-	*	-	19.0	-	-			
	PM	Queue	25'	-	-	*	-	40'	-	-			
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.0	-	-	*	-	13.7	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	-	1	
		LOS	A	-	-	*	-	C	-	-			
		Delay	9.0	-	-	*	-	18.5	-	-			
	PM	Queue	25'	-	-	*	-	25'	-	-			
		LOS	A	-	-	*	-	B	-	-			
		Delay	8.1	-	-	*	-	14.0	-	-			
Queue	25'	-	-	*	-	25'	-	-					
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1	1	1	1	1	1	1	1	1		
		LOS	B	B	C	C	C	C	C				
		Delay	14.4	14.7	17.2	16.0	18.3	18.0	18.0				
	PM	Queue	40'	45'	65'	60'	80'	80'	80'				
		LOS	C	C	C	B	C	C	C				
		Delay	15.7	15.6	15.4	14.9	19.9	21.9	21.9				
Queue	50'	55'	45'	40'	95'	110'	110'						
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	1		
		LOS	B	-	-	A	*	-	*				
		Delay	12.4	-	-	8.1	*	-	*				
	PM	Queue	25'	-	-	25'	*	-	*				
		LOS	B	-	-	A	*	-	*				
		Delay	11.8	-	-	8.0	*	-	*				
Queue	25'	-	-	25'	*	-	*						
Node 700: 88th Avenue/CTH H & South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	1	-	-	1		
		LOS	B	-	-	A	*	-	*				
		Delay	11.0	-	-	7.9	*	-	*				
	PM	Queue	25'	-	-	25'	*	-	*				
		LOS	B	-	-	A	*	-	*				
		Delay	12.2	-	-	8.1	*	-	*				
Queue	25'	-	-	25'	*	-	*						

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes consist of a shared left/through lane and a shared right/through lane



**EXHIBIT 5-12A**  
**INITIAL BUILD TRAFFIC OPERATIONS**  
**WITH MODIFICATIONS**  
**NORTH AND SOUTH DRIVEWAYS ON CTH H**

**SOMERS, WISCONSIN**

**Exhibit 5-12B**  
**Year 2025 Initial Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-	-	-	-	-	
		Delay	-	*	7.9	-	11.6	-	-	-	-	-	-	
	PM	Queue	-	*	25'	-	25'	-	-	-	-	-	-	
		LOS	-	*	A	-	B	-	-	-	-	-	-	
		Delay	-	*	7.9	-	11.3	-	-	-	-	-	-	
Queue	-	*	25'	-	25'	-	-	-	-	-	-	-		
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	8.2	-	-	*	-	-	-	-	-	12.7		
	PM	Queue	25'	-	-	*	-	-	-	-	-	25'		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	7.9	-	-	*	-	-	-	-	-	11.5		
Queue	25'	-	-	*	-	-	-	-	-	25'	-			
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	-	C		
		Delay	8.3	-	-	*	-	-	-	-	-	19.0		
	PM	Queue	25'	-	-	*	-	-	-	-	-	40'		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	8.0	-	-	*	-	-	-	-	-	13.7		
Queue	25'	-	-	*	-	-	-	-	-	25'	-			
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	-	C		
		Delay	9.0	-	-	*	-	-	-	-	-	18.5		
	PM	Queue	25'	-	-	*	-	-	-	-	-	25'		
		LOS	A	-	-	*	-	-	-	-	-	B		
		Delay	8.1	-	-	*	-	-	-	-	-	14.0		
Queue	25'	-	-	*	-	-	-	-	-	25'	-			
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1	1	1	1		
		LOS	B		B	C		C	C	C	C	C		
		Delay	14.4		14.7	17.2		16.0	18.3	18.0	18.0	18.0		
	PM	Queue	40'		45'	65'		60'	80'	80'	80'	80'		
		LOS	C		C	C		B	C	C	C	C		
		Delay	15.7		15.6	15.4		14.9	19.9	21.9	21.9	21.9		
Queue	50'		55'	45'		40'	95'	110'	110'	110'				
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1					1	-	-	-	1		
		LOS	B					A	*	-	-	*		
		Delay	12.4					8.1	*	-	-	*		
	PM	Queue	25'					25'	*	-	-	*		
		LOS	B					A	*	-	-	*		
		Delay	12.3					8.0	*	-	-	*		
Queue	25'					25'	*	-	-	*				

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.

<sup>1</sup> EB/WB lanes consist of a shared left/through lane and a shared right/through lane

**Exhibit 5-14A**  
**Year 2035 Full Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.5	-	-					
	PM	Queue	-	*	25'	-	25'	-	-					
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.0	-	-					
Queue	-	*	25'	-	25'	-	-							
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-	-		
		LOS	A	-	-	*	-	B						
		Delay	8.4	-	-	*	-	13.8						
	PM	Queue	25'	-	-	*	-	25'						
		LOS	A	-	-	*	-	B						
		Delay	8.1	-	-	*	-	12.3						
Queue	25'	-	-	*	-	25'								
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C						
		Delay	8.6	-	-	*	-	24.8						
	PM	Queue	25'	-	-	*	-	60'						
		LOS	A	-	-	*	-	C						
		Delay	8.2	-	-	*	-	15.3						
Queue	25'	-	-	*	-	25'								
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C						
		Delay	9.3	-	-	*	-	18.4						
	PM	Queue	25'	-	-	*	-	25'						
		LOS	A	-	-	*	-	C						
		Delay	8.2	-	-	*	-	15.2						
Queue	25'	-	-	*	-	25'								
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1	-	1	-	1	-	1	-	1			
		LOS	C	-	C	-	C	-	D	-	D			
		Delay	18.0	-	19.1	23.9	22.2	29.1	26.9					
	PM	Queue	55'	-	70'	105'	95'	140'	105'					
		LOS	C	-	C	-	C	-	E	-	E			
		Delay	21.1	-	21.4	20.3	19.5	35.5	43.1					
Queue	75'	-	80'	70'	65'	175'	215'							
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	B	-	-	A	-	*	-	*				
		Delay	13.6	-	-	8.2	-	*	-	*				
	PM	Queue	25'	-	-	25'	-	*	-	*				
		LOS	B	-	-	A	-	*	-	*				
		Delay	13.3	-	-	8.2	-	*	-	*				
Queue	25'	-	-	25'	-	*	-	*						
Node 700: 88th Avenue/CTH H & South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	B	-	-	A	-	*	-	*				
		Delay	11.7	-	-	8.0	-	*	-	*				
	PM	Queue	25'	-	-	25'	-	*	-	*				
		LOS	B	-	-	A	-	*	-	*				
		Delay	12.8	-	-	8.2	-	*	-	*				
Queue	25'	-	-	25'	-	*	-	*						

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane



**EXHIBIT 5-14A**  
**FULL BUILD TRAFFIC OPERATIONS**  
**WITH MODIFICATIONS**  
**NORTH AND SOUTH DRIVEWAYS ON CTH H**

**SOMERS, WISCONSIN**

**Exhibit 5-14B**  
**Year 2035 Full Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.5	-	-					
	PM	Queue	-	*	25'	-	25'	-	-					
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.0	-	-					
Queue	-	*	25'	-	25'	-	-							
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-	-		
		LOS	A	-	-	*	-	B	-					
		Delay	8.4	-	-	*	-	13.8	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	B	-					
		Delay	8.1	-	-	*	-	12.3	-					
Queue	25'	-	-	*	-	25'	-							
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C	-					
		Delay	8.6	-	-	*	-	24.8	-					
	PM	Queue	25'	-	-	*	-	60'	-					
		LOS	A	-	-	*	-	C	-					
		Delay	8.2	-	-	*	-	15.3	-					
Queue	25'	-	-	*	-	25'	-							
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C	-					
		Delay	9.3	-	-	*	-	18.4	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	C	-					
		Delay	8.2	-	-	*	-	15.2	-					
Queue	25'	-	-	*	-	25'	-							
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1		1	1		1	1		1			
		LOS	C		C	C		C	D		D			
		Delay	18.0		19.1	23.9		22.2	29.1	26.9		26.9		
	PM	Queue	55'		70'	105'		95'	140'	105'		105'		
		LOS	C		C	C		C	E		E			
		Delay	21.1		21.4	20.3		19.5	35.5	43.1		43.1		
Queue	75'		80'	70'		65'	175'	215'		215'				
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1		-			1	-	-	1			
		LOS	B		-			A	*	-	*			
		Delay	13.9		-			8.3	*	-	*			
	PM	Queue	25'		-			25'	*	-	*			
		LOS	B		-			A	*	-	*			
		Delay	13.6		-			8.2	*	-	*			
Queue	25'		-			25'	*	-	*					

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane



**EXHIBIT 5-14B**  
**FULL BUILD TRAFFIC OPERATIONS**  
**WITH MODIFICATIONS**  
**NORTH DRIVEWAY ONLY ON CTH H**

**SOMERS, WISCONSIN**

**Exhibit 5-14C**  
**Year 2035 Full Build (North and South Driveways on CTH H) Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control - Sensitivity Analysis**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.5	-	-					
	PM	Queue	-	*	25'	-	25'	-	-					
		LOS	-	*	A	-	B	-	-					
		Delay	-	*	8.0	-	12.0	-	-					
Queue	-	*	25'	-	25'	-	-							
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-	-		
		LOS	A	-	-	*	-	B	-					
		Delay	8.4	-	-	*	-	13.8	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	B	-					
		Delay	8.1	-	-	*	-	12.3	-					
Queue	25'	-	-	*	-	25'	-							
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C	-					
		Delay	8.6	-	-	*	-	24.8	-					
	PM	Queue	25'	-	-	*	-	60'	-					
		LOS	A	-	-	*	-	C	-					
		Delay	8.2	-	-	*	-	15.3	-					
Queue	25'	-	-	*	-	25'	-							
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	A	-	-	*	-	C	-					
		Delay	9.3	-	-	*	-	18.4	-					
	PM	Queue	25'	-	-	*	-	25'	-					
		LOS	A	-	-	*	-	C	-					
		Delay	8.2	-	-	*	-	15.2	-					
Queue	25'	-	-	*	-	25'	-							
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1	1	1	1	1	1	1	1	1			
		LOS	C	C	C	C	C	B	B	C				
		Delay	16.8	17.6	21.5	20.0	18.1	14.6	14.9	16.5				
	PM	Queue	55'	65'	95'	90'	55'	35'	35'	50'				
		LOS	C	C	C	C	C	C	C	C				
		Delay	18.6	18.7	18.1	17.4	16.2	16.9	18.5	16.2				
Queue	65'	70'	60'	60'	45'	55'	65'	50'						
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	B	-	-	A	*	-	*					
		Delay	13.6	-	-	8.2	*	-	*					
	PM	Queue	25'	-	-	25'	*	-	*					
		LOS	B	-	-	A	*	-	*					
		Delay	13.3	-	-	8.2	*	-	*					
Queue	25'	-	-	25'	*	-	*							
Node 700: 88th Avenue/CTH H & South Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-			
		LOS	B	-	-	A	*	-	*					
		Delay	11.7	-	-	8.0	*	-	*					
	PM	Queue	25'	-	-	25'	*	-	*					
		LOS	B	-	-	A	*	-	*					
		Delay	12.8	-	-	8.2	*	-	*					
Queue	25'	-	-	25'	*	-	*							

(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane



**EXHIBIT 5-14C**  
**FULL BUILD TRAFFIC OPERATIONS**  
**WITH MODIFICATIONS - SENSITIVITY ANALYSIS**  
**NORTH AND SOUTH DRIVEWAYS ON CTH H**

**Exhibit 5-14D**  
**Year 2035 Full Build (North Driveway Only on CTH H) Traffic Peak Hour Operating Conditions**  
**With Modified Geometrics and Traffic Control - Sensitivity Analysis**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙
Node 100: Somer Road/CTH E & 97th Avenue <i>One-Way Stop Control</i>	AM	Lanes->	-	1	-	1	-	1	-	1	-	-	-	
		LOS	-	*	A	-	B	-	-	-	-	-	-	
		Delay	-	*	8.0	-	12.5	-	-	-	-	-	-	
	PM	Queue	-	*	25'	-	25'	-	-	-	-	-	-	
		LOS	-	*	A	-	B	-	-	-	-	-	-	
		Delay	-	*	8.0	-	12.0	-	-	-	-	-	-	
Node 200: Somer Road/CTH E & Proposed Sports Field Drive <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	B	-		
		Delay	8.4	-	-	*	-	-	-	-	13.8	-		
	PM	Queue	25'	-	-	*	-	-	-	-	25'	-		
		LOS	A	-	-	*	-	-	-	-	B	-		
		Delay	8.1	-	-	*	-	-	-	-	12.3	-		
Node 300: Somer Road/CTH E & West Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	C	-		
		Delay	8.6	-	-	*	-	-	-	-	24.8	-		
	PM	Queue	25'	-	-	*	-	-	-	-	60'	-		
		LOS	A	-	-	*	-	-	-	-	C	-		
		Delay	8.2	-	-	*	-	-	-	-	15.3	-		
Node 400: Somer Road/CTH E & East Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	-	-	-	1		
		LOS	A	-	-	*	-	-	-	-	C	-		
		Delay	9.3	-	-	*	-	-	-	-	18.4	-		
	PM	Queue	25'	-	-	*	-	-	-	-	25'	-		
		LOS	A	-	-	*	-	-	-	-	C	-		
		Delay	8.2	-	-	*	-	-	-	-	15.2	-		
Node 500: Somer Road/CTH E & 88th Avenue/CTH H <sup>1</sup> <i>All-Way Stop Control</i>	AM	Lanes->	1	1	1	1	1	1	1	1	1	1		
		LOS	C	C	C	C	C	C	B	B	C	C		
		Delay	16.8	17.6	21.5	20.0	18.1	14.6	14.9	16.5	16.5	16.5		
	PM	Queue	55'	65'	95'	90'	55'	35'	35'	50'	50'	50'		
		LOS	C	C	C	C	C	C	C	C	C	C		
		Delay	18.6	18.7	18.1	17.4	16.2	16.9	18.5	16.2	16.2	16.2		
Node 600: 88th Avenue/CTH H & North Driveway <i>One-Way Stop Control</i>	AM	Lanes->	1	-	-	1	-	-	1	-	-	1		
		LOS	B	-	-	A	-	-	*	-	*			
		Delay	13.9	-	-	8.3	-	-	*	-	*			
	PM	Queue	25'	-	-	25'	-	-	*	-	*			
		LOS	B	-	-	A	-	-	*	-	*			
		Delay	13.6	-	-	8.2	-	-	*	-	*			
Queue	25'	-	-	25'	-	-	*	-	*					



(-) indicates a movement that is prohibited or does not exist; (\*) indicates a freeflow movement.  
 Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.  
<sup>1</sup> EB/WB lanes and NB/SB lanes each consist of a shared left/through lane and a shared right/through lane

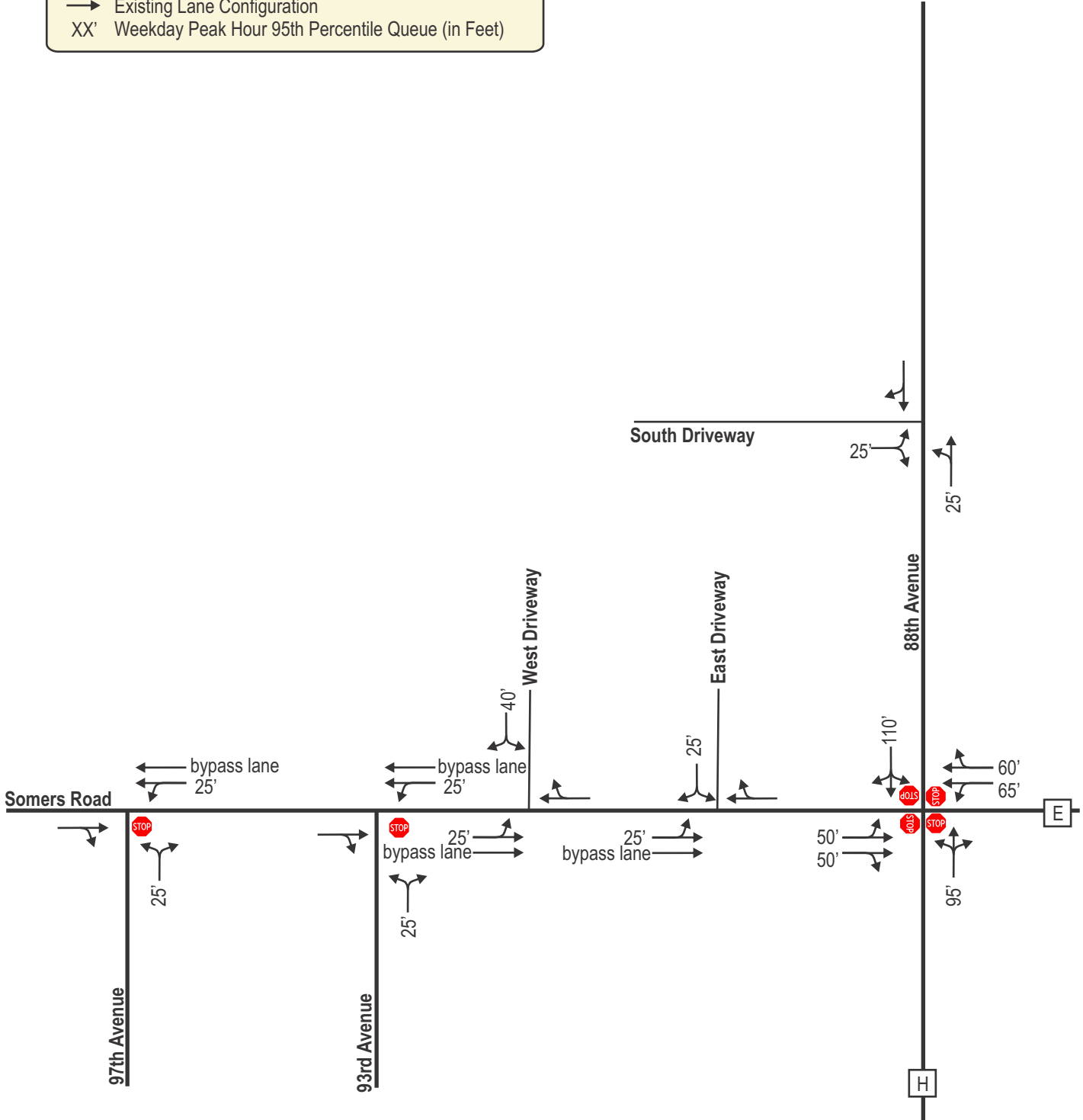


**EXHIBIT 5-14D**  
**FULL BUILD TRAFFIC OPERATIONS**  
**WITH MODIFICATIONS - SENSITIVITY ANALYSIS**  
**NORTH DRIVEWAY ONLY ON CTH H**

**SOMERS, WISCONSIN**

**LEGEND**



-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)

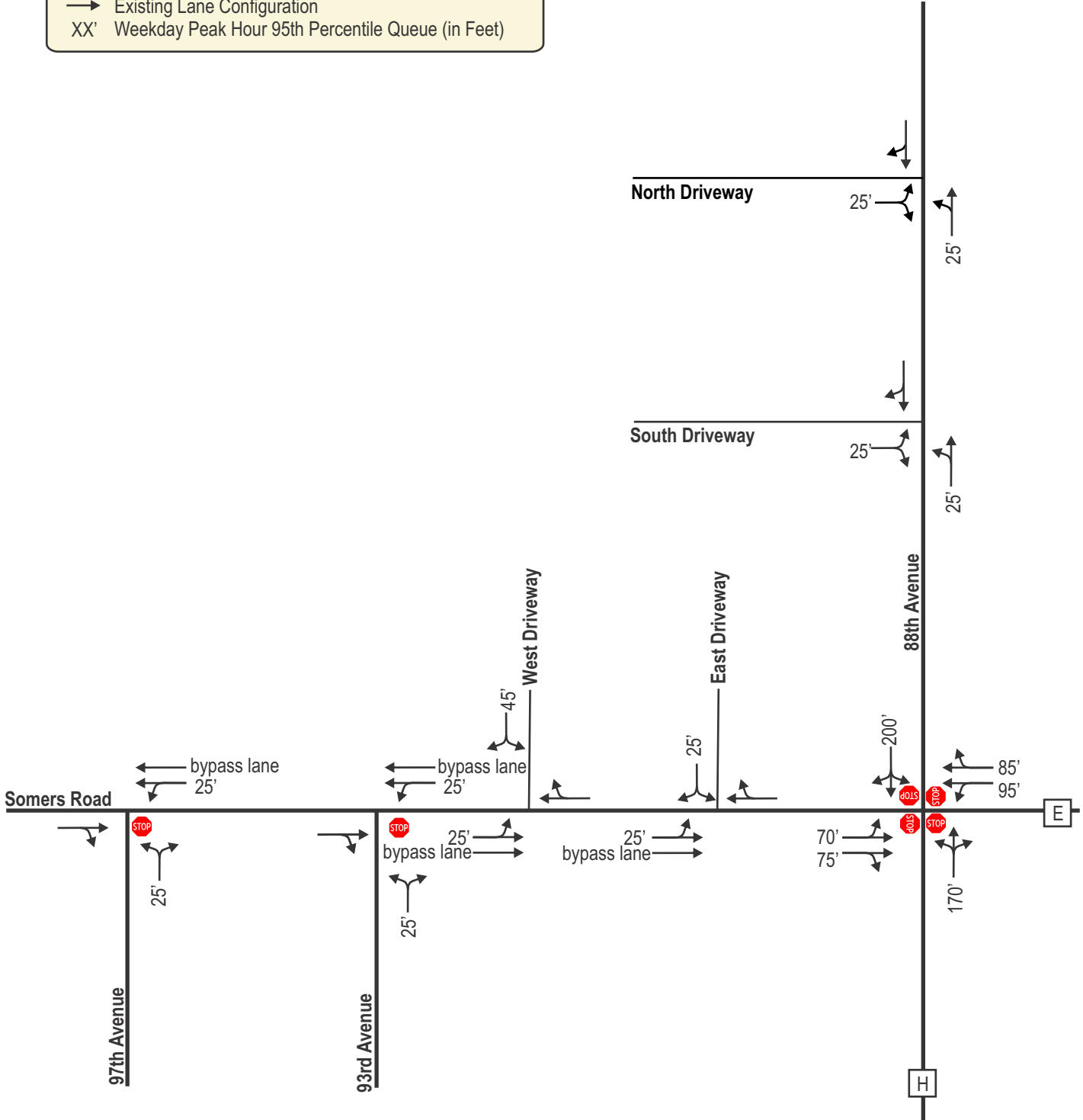


**EXHIBIT 5-18  
YEAR 2025 BACKGROUND TRAFFIC  
MAXIMUM QUEUE LENGTHS**

**SOMERS, WISCONSIN**

**LEGEND**



-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)

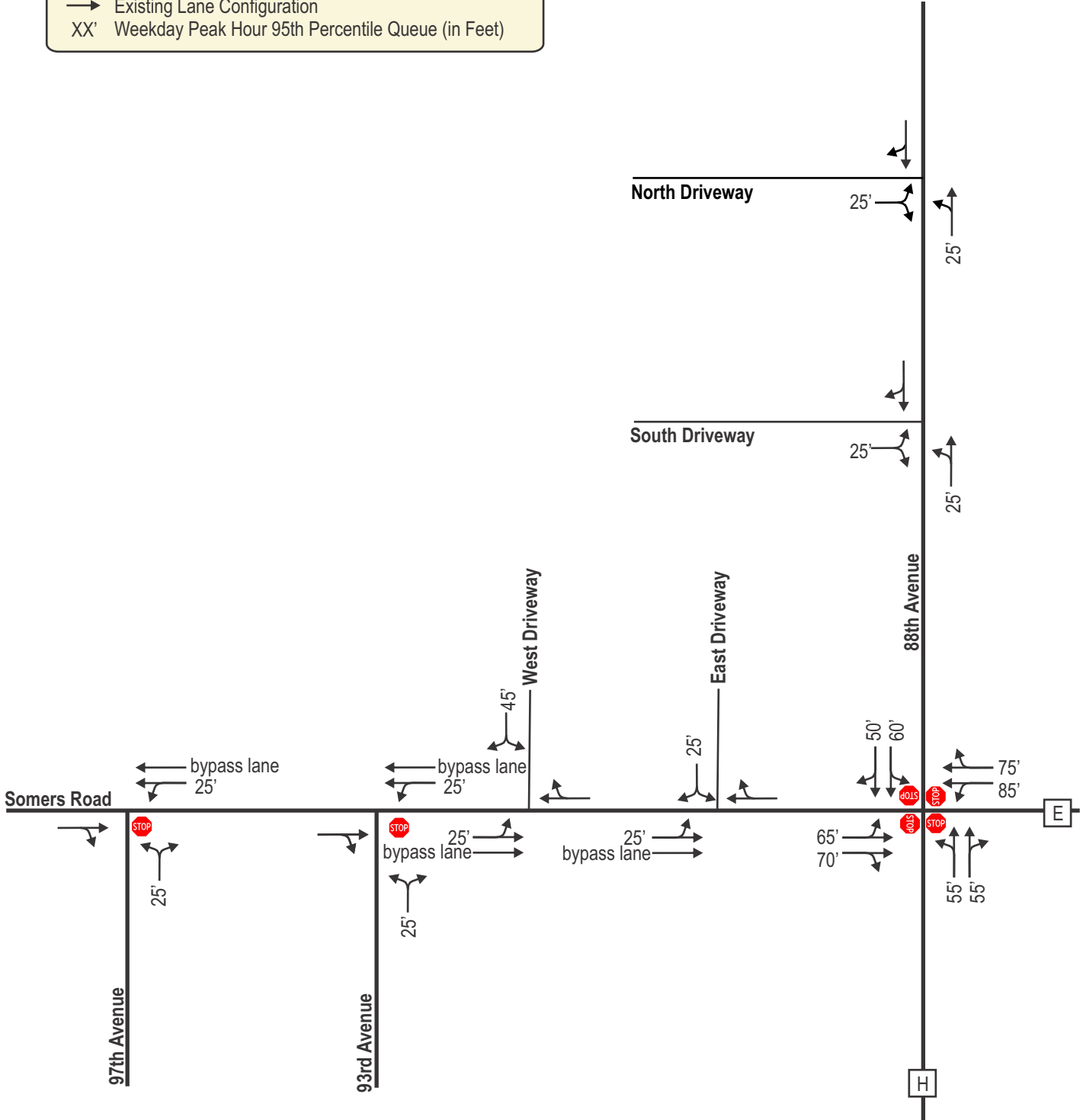


**EXHIBIT 5-20A  
YEAR 2035 BACKGROUND TRAFFIC  
MAXIMUM QUEUE LENGTHS**



**SOMERS, WISCONSIN**

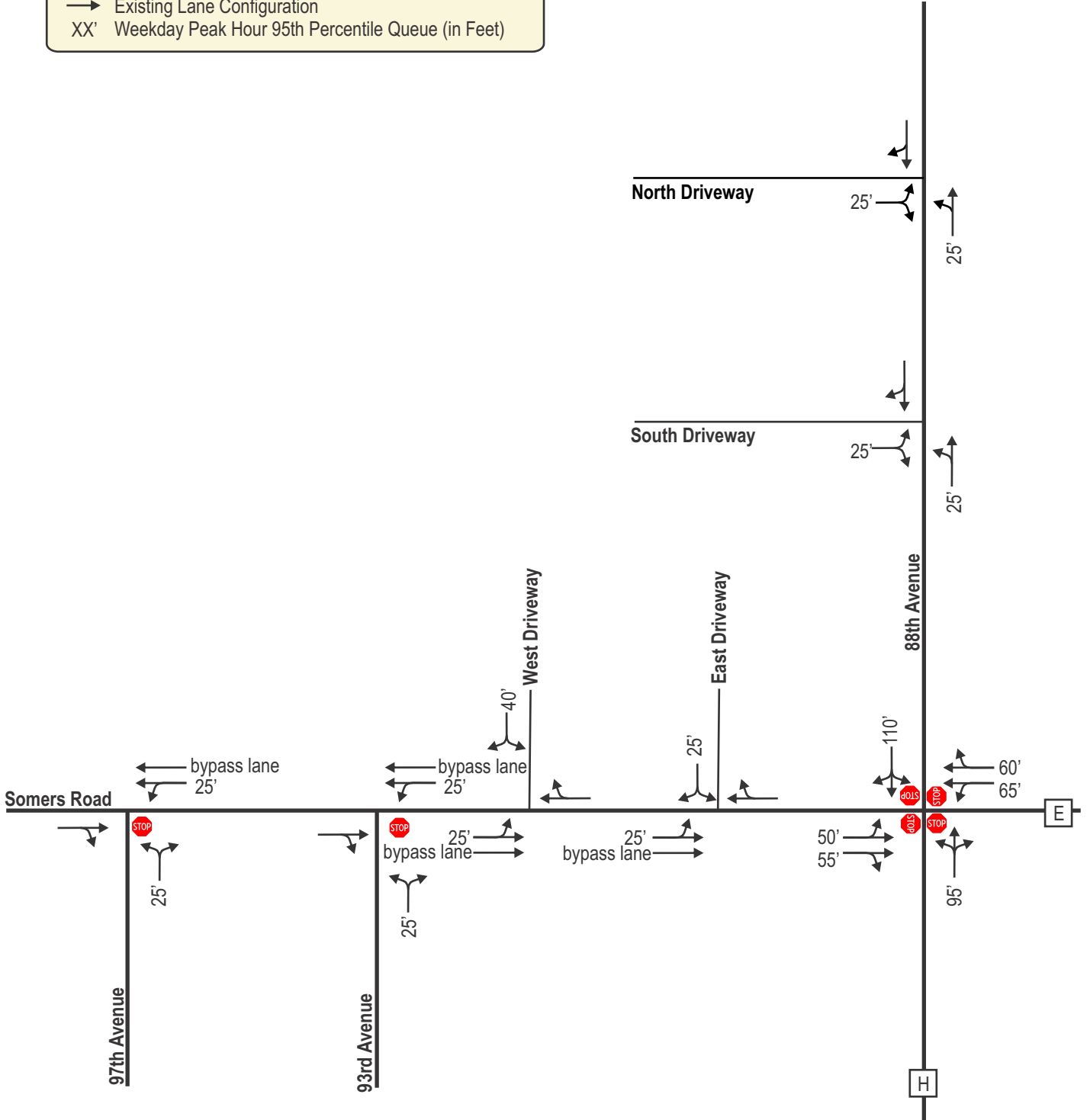
**LEGEND**

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)





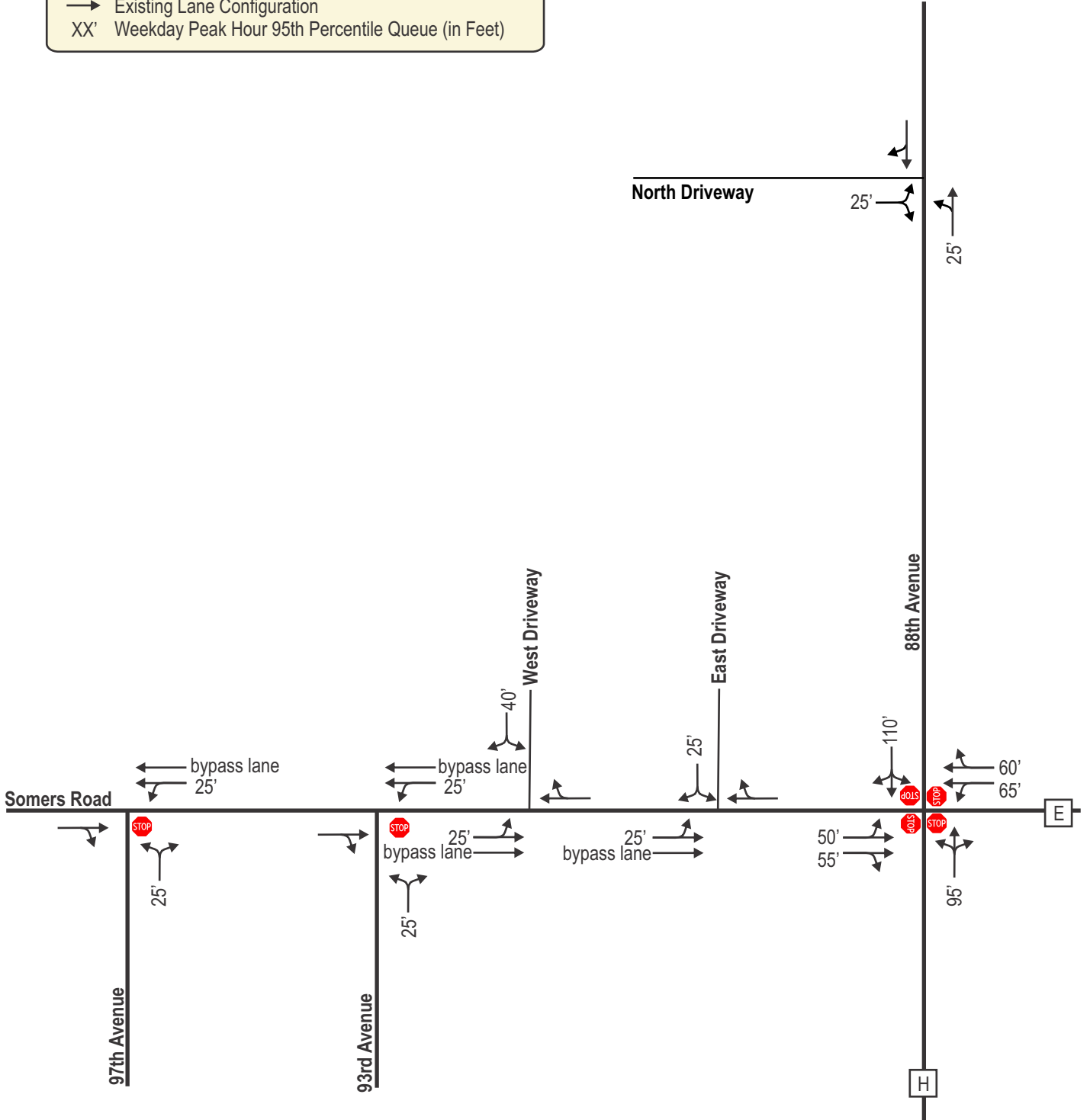
**LEGEND**

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)





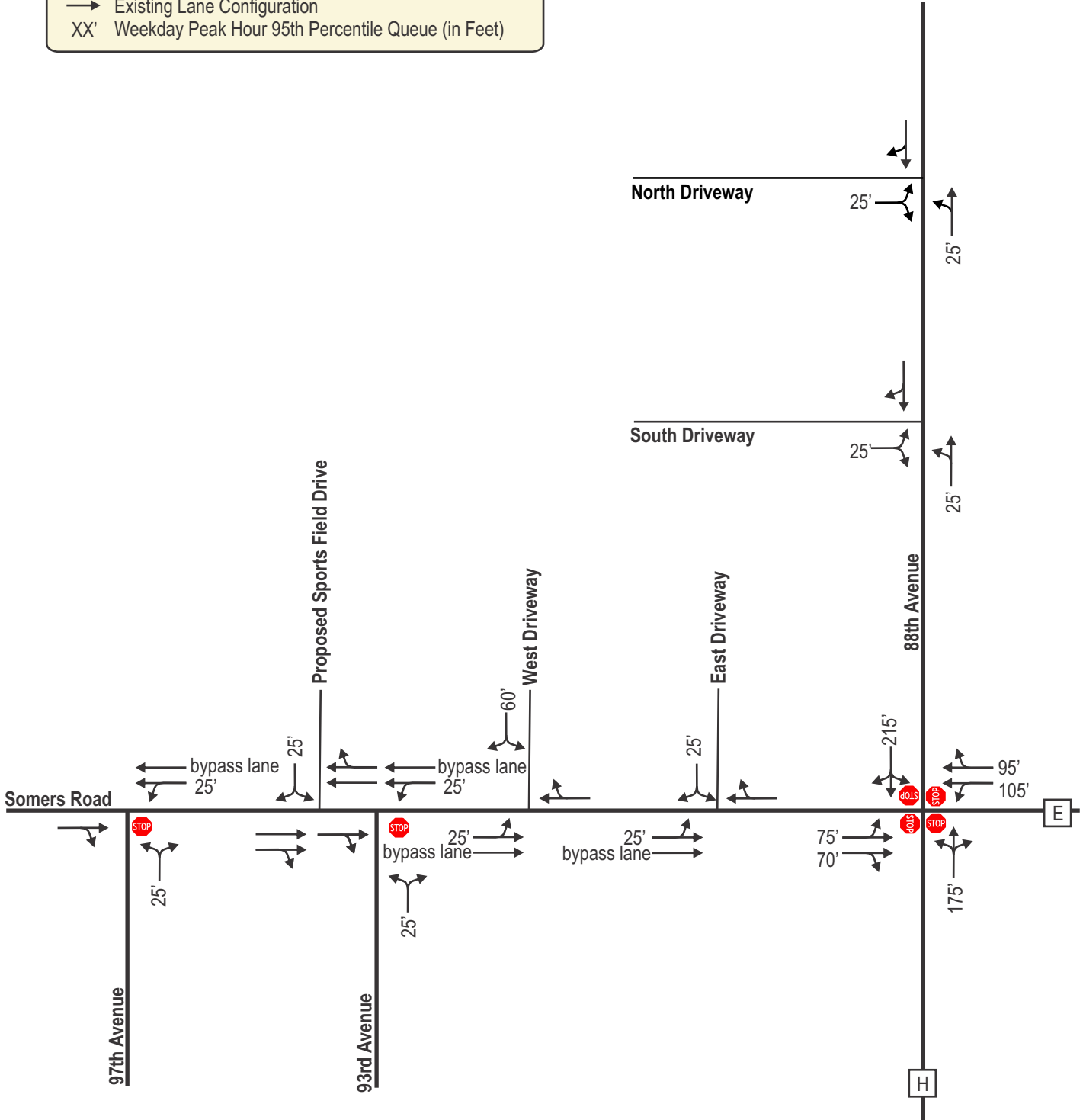
**LEGEND**

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)





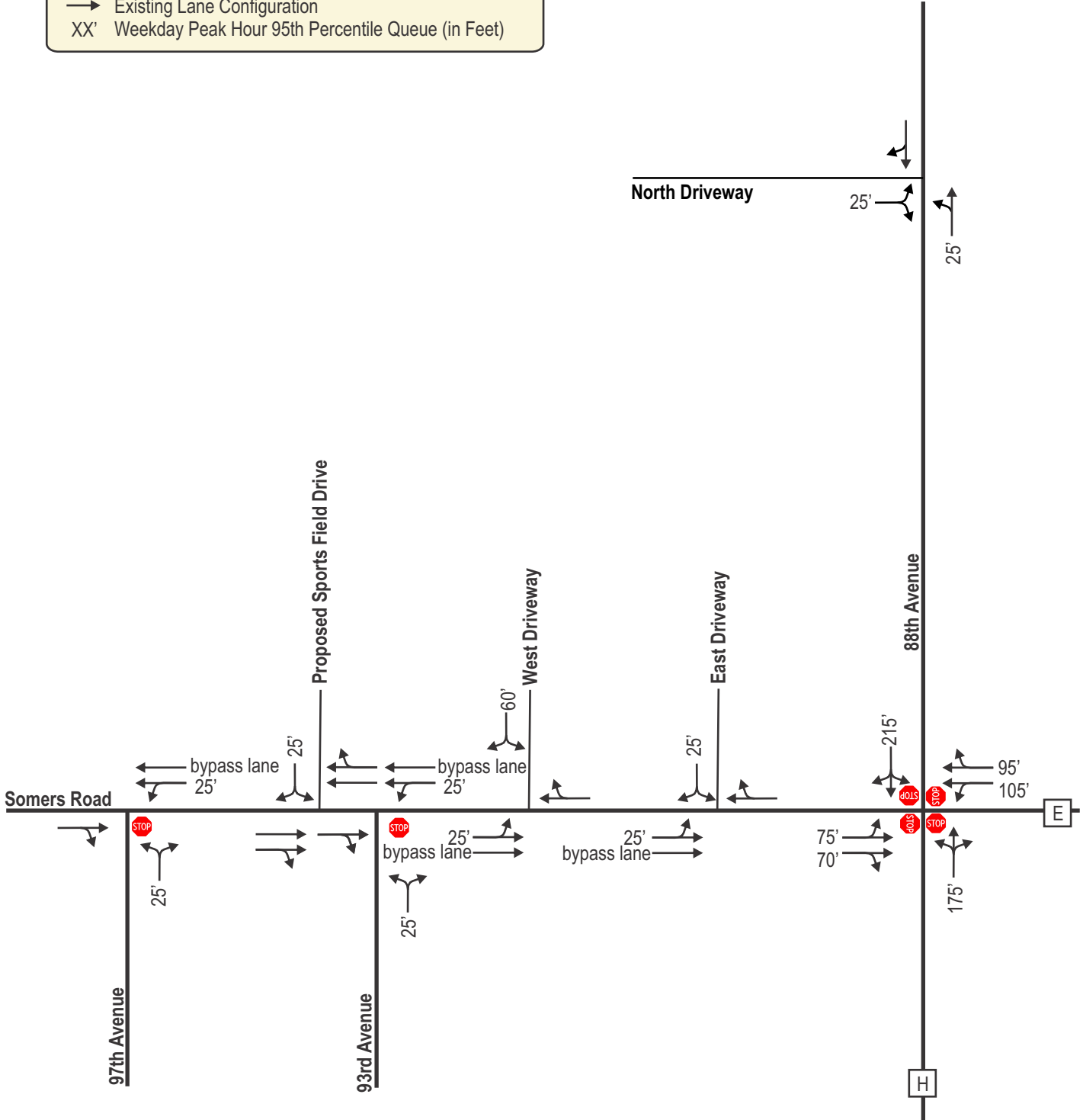
**LEGEND**

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)





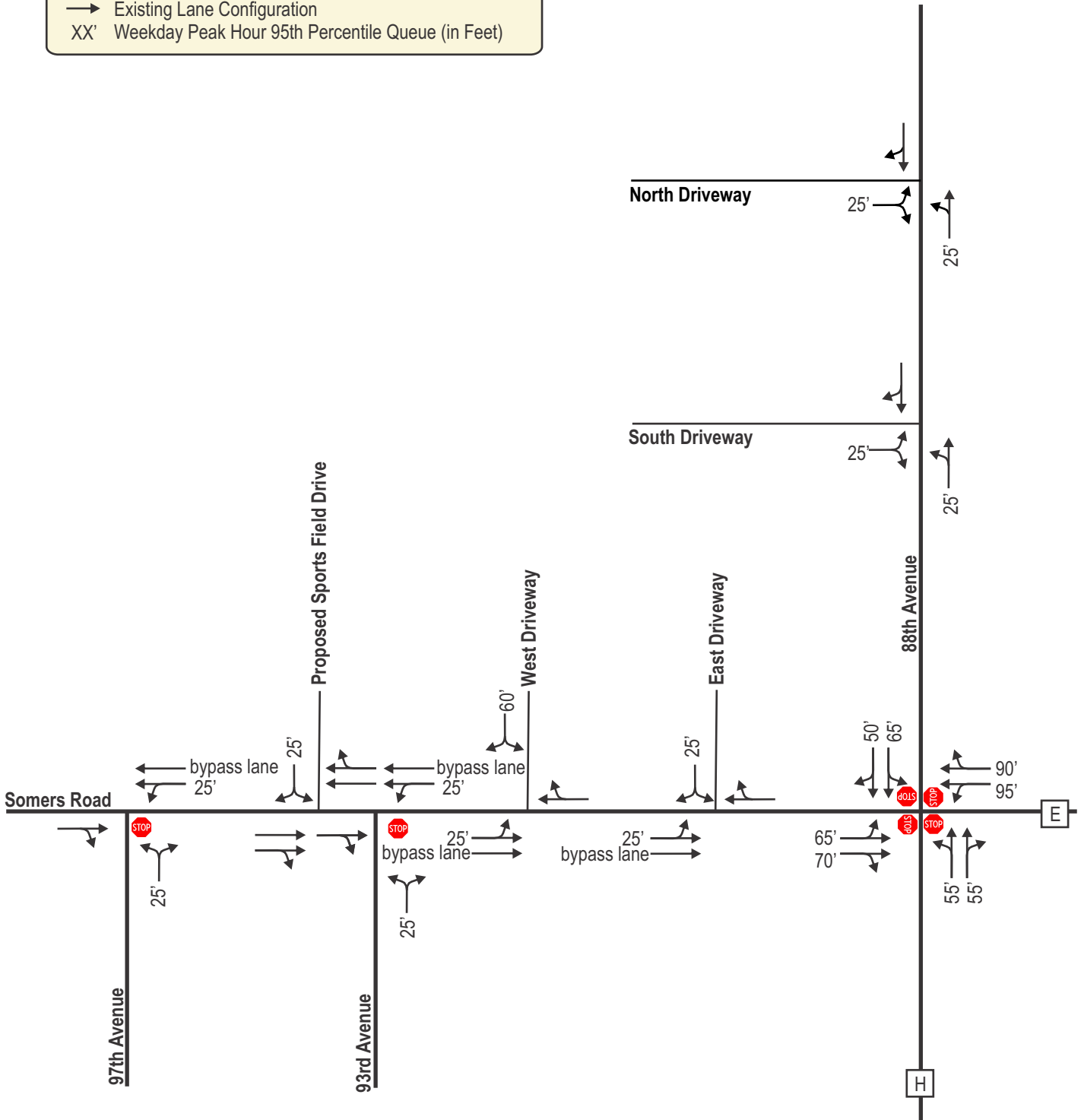
**LEGEND**

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)





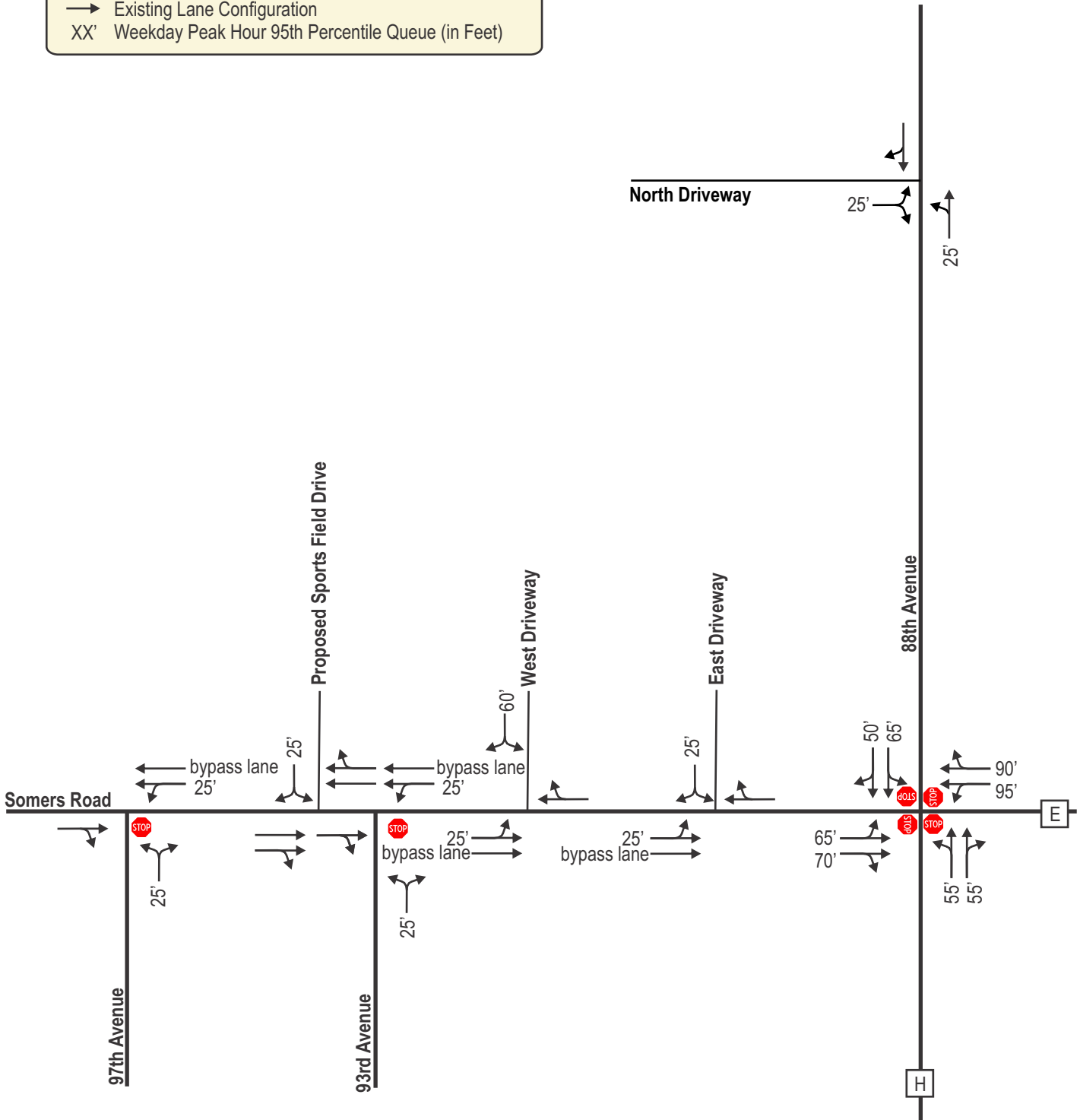
**LEGEND**

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)



**LEGEND**

-  Stop Control
-  Existing Lane Configuration
- XX' Weekday Peak Hour 95th Percentile Queue (in Feet)



## CHAPTER VI – RECOMMENDATIONS AND CONCLUSION

### PART A – RECOMMENDATIONS

The study area intersections were analyzed based on the procedures set forth in the *Highway Capacity Manual (HCM) 6<sup>th</sup> Edition*. Intersection operation is defined by “level of service.” Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A,’ to very poor, represented by LOS ‘F.’ For the purpose of this study, LOS D or better was used to define acceptable peak hour operating conditions.

Modifications to address traffic impacts are included for the Existing, Background, and Build traffic conditions and have been shown for the following five scenarios:

- “Existing Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions, without the proposed school.
- “Year 2025 Background Traffic” – These modifications are expected to be necessary to accommodate the Existing traffic volumes under current conditions plus full build out of the identified off-site development, without the proposed school.
- “Year 2025 Initial Build Traffic” – These modifications are expected to be necessary to accommodate the Initial Build traffic volumes, which includes the initial build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development.
- “Year 2035 Background Traffic” – These modifications are expected to be necessary to accommodate the future background traffic volume conditions which include typical background growth along the highways plus full build out of the identified off-site development, without the proposed school.
- “Year 2035 Full Build Traffic” – These modifications are expected to be necessary to accommodate the Full Build traffic volumes, which includes the full build-out of Shoreland Lutheran High School as well as full buildout of the identified off-site development, including typical background growth along the highways.

The analysis was conducted using existing intersection geometrics and traffic control. The following modifications, as shown in [Exhibits 1-3 & 1-4](#), are recommended to accommodate the respective Background and Build traffic volume scenarios for the years 2025 and 2035, respectively. *Modifications are for jurisdictional consideration and are not legally binding. Kenosha County and the Village of Somers reserve the right to determine alternative solutions.*

#### Node 100: Somers Road/CTH E intersection with 97<sup>th</sup> Avenue

- *Existing Traffic*: No modifications.
- *Year 2025 Background Traffic*: No modifications.
- *Year 2025 Initial Build Traffic*: No modifications.
- *Year 2035 Background Traffic*: No modifications.
- *Year 2035 Full Build Traffic*: No modifications.

#### Node 200: Somers Road/CTH E intersection with :Proposed Sports Field Drive

- *Existing Traffic*: No modifications.

- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:*
  - Provide a new full access drive on the north approach as shown on the site plan.
  - Provide stop sign control on the north approach.
  - Provide an eastbound bypass lane on the south side of CTH E at the new intersection.
  - Connect the existing westbound bypass lanes on the north side of CTH E between 93<sup>rd</sup> Avenue and 97<sup>th</sup> Avenue.

*Node 300: Somers Road/CTH E intersection with West School Driveway*

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

*Node 400: Somers Road/CTH E intersection with East School Driveway*

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

*Node 500: Somers Road/CTH E intersection with 88<sup>th</sup> Avenue/CTH H*

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:*
  - Consider providing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2025 Initial Build Traffic:* No additional modifications.
- *Year 2035 Background Traffic:*
  - If not previously constructed, consider constructing an additional shared eastbound and westbound lane on the west and east approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.

- Consider providing an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection (see sensitivity analysis discussion below). If implemented, the additional lane would taper down to a single lane again once vehicles traverse the intersection.

- *Year 2035 Full Build Traffic:* No additional modifications.

Node 600: 88<sup>th</sup> Avenue/CTH H intersection with North Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:*
  - Provide a new full access drive on the west approach as shown on the site plan.
  - Provide stop sign control on the west approach.
- *Year 2035 Background Traffic:* No additional modifications.
- *Year 2035 Full Build Traffic:* No additional modifications.

Node 700: Somers Road intersection with South School Driveway

- *Existing Traffic:* No modifications.
- *Year 2025 Background Traffic:* No modifications.
- *Year 2025 Initial Build Traffic:* No modifications.
- *Year 2035 Background Traffic:* No modifications.
- *Year 2035 Full Build Traffic:* No modifications.

With a new north access driveway onto CTH H proposed, two access scenarios were evaluated as part of this study to analyze the operation of the two driveways along CTH H with and without the existing south school driveway operational. Regardless of the access scenario, both the north and south driveways are expected to operate acceptably at LOS B or better under both weekday peak periods; therefore, since providing the south driveway is not expected to cause an unsafe condition, allowing the south access driveway is recommended to allow for additional access options to/from the site. However, if it is decided to remove the driveway, the north driveway is expected to operate acceptably as the sole access onto CTH H.

All intersections are expected to operate acceptably with the proposed initial build out of the Shoreland Lutheran High School campus with the recommended modifications. Without the recommended modifications under the current year 2025 background traffic conditions, longer delays and queues are expected for the eastbound and westbound movements at the all-way stop-controlled Somers Road intersection with 88<sup>th</sup> Avenue during the weekday morning arrival and weekday afternoon discharge peak periods, with school in session. The higher delays and queues increase under the initial build conditions. However, the increases in delay under the background and initial build conditions are expected to be slightly higher (15 seconds for westbound vehicles and 7 seconds for eastbound vehicle) during the typical weekday morning (for westbound) and afternoon (for eastbound) discharge peak period under the initial build traffic conditions with the existing lane configurations on all approaches, that is, with no additional eastbound/westbound lanes along Somers Road. The increases in queues under the existing lane configurations are

expected to be 2 to 3 vehicles. It is noted that these delays and increases in queue lengths would only be expected for less than 30 minutes during the typical weekday morning and afternoon school “surge” peak periods with lower delays and queues all other hours of the day.

In addition, with the recommended modifications under future year 2035 background traffic conditions; that is, with additional lanes on the eastbound and westbound approaches but without additional lanes on the northbound and southbound approaches at the Somers Road intersection with 88<sup>th</sup> Avenue, slightly higher delays (2 seconds for northbound vehicles and 8 seconds for southbound vehicle) would be expected on the north and south approaches during the typical weekday afternoon discharge peak period under the full build traffic conditions with single lanes on the north and south approaches. Queue lengths on the north and south approaches would be expected to be 7 to 9 vehicles; however, these queue lengths would only be expected for less than 30 minutes. To alleviate the expected longer delays and queue lengths, additional northbound and southbound lanes would be required; therefore, a sensitivity analysis was completed to document this geometric condition, as described below.

Because modifications are recommended at the all-way stop-controlled Somers Road intersection with 88<sup>th</sup> Avenue under current and background conditions and because the modifications are further driven by projected future year volumes during peak “surge” traffic periods during the typical weekday morning school arrival and afternoon school discharge peak periods, a sensitivity analysis was completed for this project to document the operations if dual lanes were not only constructed along Somers Road (eastbound/westbound lanes) but also along 88<sup>th</sup> Avenue (northbound/southbound lanes) at the all-way stop controlled intersection. With the modifications, all movements are expected to operate safely with moderate queue lengths. Therefore, the following modifications could be considered as optional under the design year background and full build conditions.

Node 500: Somers Road/CTH E intersection with 88<sup>th</sup> Avenue/CTH H

- *Year 2035 Background Traffic:*
  - Provide an additional shared northbound and southbound lane on the south and north approaches of this all-way stop-controlled intersection. The additional lane would taper down to a single lane again once vehicles traverse the intersection.
- *Year 2035 Full Build Traffic:* No additional modifications.

## **PART B – CONCLUSION**

All movements at the study area intersections are expected to operate safely and efficiently with the development assumptions outlined in this TIA and with the identified recommended modifications if properly designed and implemented through the design year of the development.

**VILLAGE OF SOMERS  
VILLAGE BOARD  
WORK SESSION ITEM MEMORANDUM**

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**WORK SESSION:** November 6<sup>th</sup>, 2024

**TO:** Village President Stoner and Board of Trustees

**PREPARED BY:** Jason J. Peters, Administrator

**AGENDA ITEM:** #7 Continued discussion on 2025 Budget

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**BACKGROUND:**

Below will summarize what additional items that have been discussed since August 6<sup>th</sup>:

- At our August 20<sup>th</sup> Work Session, the Board reviewed the Village's allowable/potential tax levy based on net new construction. Net new construction in the Village rose by 13.36%. The Board was presented with the effects of levy increases between 1% and 13.36%. Direction from the Board at time was to proceed with a 13.36% increase. Ehlers also presented on the benefits of taking the full 13.36% on the Village's Levy Limit Work Sheet. The Board also discussed in closed session, possible changes to the Assistant to the Administrator Position.
- At our September 3<sup>rd</sup> Work Session, McMahon Group, presented their findings on the Station evaluation portion of our Fire Study.
- Annual Joint Village/Town Budget Work Session was held on September 21<sup>st</sup>.
- At our September 24<sup>th</sup> Board Meeting, discussed possible changes to health care insurance program.
- At our October 1<sup>st</sup> Work Session, in closed session held discussion on possible salary changes and staffing levels for Administration, Fire, and Public Works and how they would affect the proposed budget.
- At our October 8<sup>th</sup> Board Meeting, in closed session continued discussion on possible salary changes and staffing levels for Administration, Fire, and Public Works how they would affect the proposed budget.

- At our October 15<sup>th</sup> Work Session, the Board reviewed levy limits with Ehlers and gave direction to reduce our Line E in the Levy Limit Work Sheet by \$100,000. Also reviewed CIP requests and proposed CIP Budget.
- At our October 22<sup>nd</sup> meeting this budget was presented to the Board.
- Public has been made for action on this budget on November 26<sup>th</sup>.

COMMENTS:

There have been no changes to the budget since the October 22<sup>nd</sup> Presentation. Staff is still waiting on our certified assessment numbers for the assessor.

**2025 GENERAL FUND BUDGET OVERVIEW**

Staff has gone to great lengths to present a balanced budget. The budget presented today is balanced.

Key 2025 Updates

- Uses full allowable levy limit increase of 13.36% based on net new construction
- Below full-time positions are being requested by Departments
  - Fire
    - 1 Fire/EMS
  - Public Works
    - 1 Public Works I
    - This additional Public Works I position allows for the creation of a Utility Manager Position.
  - Administration
    - 1 Deputy Clerk/Treasurer
  - Village health insurance premium cost to increase by 16% or \$110,834 over all funds. This number is higher due to proposed additions to Staff discussed below. The increase in premium if no additions to Staff would be 8% per EFT for health.
  - 3% percent pay increase for non-contract employees.

- Fire/EMS contract calls for a 3% increase in 2025. This is the last year of this contract. Over the contract the increases were as follows:
  - 2% in year 1
  - 4% in year 2
  - 3% in year 3
- Increase in Sheriff's Contract by \$80,375
- Changes to Garbage Contract with an overall savings of \$176,992

Key changes in revenue from 2024 budget to 2025:

- Department Taxes increased by \$230,852
  - Contributing factors:
    - Village levied property taxes increased by \$283,964
      - (101-41000-41111)
    - Town levied property taxes decreased by \$23,310
      - (101-41000-41112)
    - Continued use of Ravine Park's voluntary donation \$77,000
      - (101-42000-421800)
      - Note: The Development Agreement with Ravine Park requires a voluntary payment to the Village. The annual payment to the Village equates to half of the property tax incremental revenues from the residential component of the property for the previous year.
      - In the 2024 Budget the Board allowed the utilization of \$80,000 in the general fund budget based on the amount that the Village would receive through property taxes upon the conclusion of the voluntary payments.
      - If the Board allows this to continue the estimated calculation is as follows:

Assesed Value Parcel 1	\$ 37,800,000.00	Parcel #82-4-222-104-0331
Assessed Value Parcel 2	\$ 5,580,000.00	Parcel #82-4-222-104-0335
Total	\$ 43,380,000.00	
Less Base Value	\$ 131,000.00	
Total Taxable Assessed Value	\$ 43,249,000.00	
Total Estimated Taxes	\$ 432,490.00	Divide by 1000 * Proposed Mill rate
Voluntary Payment to Village 1/2 of Estimated Taxes	\$ 216,245.00	
		3.62 Village Mill rate divided by Total Estimated Mill Rate of 10 = Village Portion of Taxes is 36%
Village Portion to Budget	\$ 77,848.20	Then Take our 1/2 times percentage

- Administration is comfortable recommending the utilization of \$77,000 (101-42000-42180) in the general fund budget based on the amount that the Village would receive through property taxes upon the conclusion of the voluntary payments. This number has been reduced due to the Village mill rate being lower and an estimate of other jurisdictions mill rate lowering.
- Intergovernmental revenues increased by \$104,511
  - Contributing factors:
    - Payment for Municipal Services increased by \$28,671
      - (101-43000-43691)
      - Note: Village pay out 90% to City of Kenosha (accounted for in expenditures)
    - State Shared Revenue increased by \$2,066
      - (101-43000-43410)
    - Personal Property Aid increased by \$24,091
      - (101-43000-43411)
    - Fire Insurance Due increased by \$22,000
      - (101-43000-43420)
    - Transportation Aid increased by \$26,838
      - (101-43000-43531)
- Licenses and Permits revenue decreased by \$1,939
  - Contributing factors:

- Other Licenses and Permits decreased by \$5,000
  - (101-44000-44900)
  - Note: Activity was for 2024 was down compared to our original budget. Decreased fees as a more conservative estimate.
- Building permits increased by \$8,860
  - (101-44000-44300)
  - Note: Staff suggests being more conservative with the projected revenue in this line. Budgeted amount only accounts for normal activity. We did not include possible permits for larger approved projects.
- Fines and Forfeitures increased by \$64,975
  - Contributing factors:
    - Court Penalties increased by \$65,000
      - (101-45000-45100)
      - Note: Activity in 2024 was higher than estimated and Court is comfortable with their projected revenue of \$175,000
- Public Charges for Services increased by \$47,500
  - Contributing factors
    - Fire and Rescue Service increased by \$100,000
      - (101-46000-46220)
      - While this revenue is estimated at \$950,000 the Village accounts for an uncollected allowance for doubtful accounts (101-46000-46290). This is represented at \$325,000 with a net positive to the Village of \$625,000.
    - Fire Inspection Fees decreased by \$7,500
      - (101-46000-46221)
      - Note: This is being conservative by not taking into account possible inspections for larger projects that have been approved but have not broken ground.
- Miscellaneous Revenue decreased by \$18,910

- Contributing factors
  - Interest income remained unchanged
    - (101-48000-48110)
    - Note: This number was originally higher, but with discussions with Board they felt more comfortable with backing our estimate off by \$100,000.
  - Total Revenues in the 2025 budget totals \$7,170,685. This represents an increase over 2024 by \$418,989.

Key changes in expenditures from 2024 budget to 2025:

- Municipal Court expenditures increased by \$10,219
  - Contributing factors
    - Salary increased by \$8,146
      - (101-51210-50101)
- Village/Office Expenditures increased by \$45,182
  - Contributing factors
    - Driven by proposed increase in salaries.
    - Equipment Lease & Rental increased by \$17,500
      - (101-51410-50503)
      - Note: This account will know reference the ongoing licensing fees for the 3 Flock cameras that were installed in 2024 and will account for adding 3 additional Flock cameras in 2025.
    - Conferences and Training increased by \$18,551
      - (101-51410-50701)
      - Note: Increased to account for additional training for current staff and proposed new staff. This increased to account for tuition reimbursement for staff that are currently using this program.

- Newsletter has been removed from the Budget that was shown on September 21<sup>st</sup>.
      - (101-51410-50801)
      - Note: Board requested that Staff review bringing back printing and mailing our Quarterly Newsletter for residents. Staff estimates that at a minimum this would have been a cost of \$25,000. Given our strategy of backing off the \$100,000 per year in the Levy Limit Work Sheet, Administration believes this \$25,000 would not be sustainable.
- Clerk/Treasurer expenditures increased by \$35,735
  - Contributing factors
    - Salaries increased by \$21,450
      - (101-51420-50101)
      - NOTE: This increase is due to Administration request to add a full-time Deputy Clerk/Treasurer position as an authorized position. There has been turnover in the Clerk/Treasurer position. One of the main reasons given was due to the lack of back up for this position. The Village has seen fit in the last few years to add back up positions for the Administrator, Accounting Manager, and Fire Chief. Given the importance of the Clerk/Treasurers duties, Administration feels it is imperative to have a dedicated back for this position. This would also allow, the Clerk I to increase her work with Building Inspector Seymour in his property maintenance program.
    - Health Insurance increased by \$8,583
      - (101-51420-50203)
      - NOTE: Due to request to a Deputy Clerk/Treasurer position
- Village/Town Hall decreased by \$1,227
  - Contributing Factors
    - Other supplies and expense decreased by \$2,000
      - (101-51610-50309)

- Telephone increased by \$2,856
      - (101-51610-50604)
      - NOTE: Village telephone contract will be ending and new contract will show an increase.
- Other General Government expenditures increased by \$126,604
  - Contributing Factors
    - Consulting and financial advisors increased by \$5,000
      - (101-51910-50402)
      - NOTE: Staff is increasing this account as the line has been over in the past and we foresee the possible use of consultants in the next year due to results of the fire study.
    - Collection expense increased by \$8,000
      - (101-51910-50803)
      - NOTE: Increased is due to projected increase in revenues collected by Life Quest, which in turn increases the amount billed to the Village.
    - Prior year expense increased \$10,000
      - (101-51910-50808)
      - NOTE: Staff is budgeting for any possible refunds due to assessment refunds that are awarded through possible challenges valuations.
    - Other miscellaneous expenses increased by \$89,968
      - (101-51910-50809)
      - Increase is due to the Village recognizing payment due to the Town of Paris under our IGA. The Village is required to pay 1/2 the tax revenues on development in the Paris/Somers growth area. The revenues below represent the value added by the Flint 94 development.



was added to last year's contract as an amendment. The contract was approved by Board at our October 8<sup>th</sup> Board meeting.

- Fire Department expenditures increased by \$326,836
  - Contributing Factors
    - Driven by salary increases and changes to health insurance.
      - Multiple Accounts
      - NOTE: Adds one new full-time Firefighter/EMT
- Building Inspection expenditures decreased by \$13,871
  - Contributing factors
    - Driven by salary increases and changes to health insurance.
      - Multiple Accounts
- Public Works expenditures increased by \$56,329
  - Contributing factors
    - Driven by salary increases and health insurance increases.
      - Multiple Accounts
      - NOTE: This adds one new full time Public Works I position. With this position added, Public Works will add a Utility Manager position. The majority of this salary is allocated to the Utility Funds.
    - Road maintenance increased by \$15,000
      - (101-53100-50505)
      - NOTE: This increase is due to estimating higher rental fees for certain equipment and materials for crack sealing. Also estimating an increase in the cost of salt.
    - Water and sewer decreased by \$5,880
      - (101-53100-50606)
      - NOTE: Staff feels that this account was overbudgeted in the past.

- Solid Waste expenditures decreased by \$264,515
  - Contributing Factors
    - Garbage Collection decreased by \$176,992
      - (101-53620-50408)
      - NOTE: This represents the new contract numbers from Johns Disposal that requires multi-family housing with over four-units to privately contract for their garbage handling. Letters went out to property owners/managers in July and September to inform them of the proposed change. Under the proposed contract the per unit price goes from \$10.40 to \$11.10 per unit. This contract was approved by the Board at our October 8<sup>th</sup> Board Meeting.
    - Recycling decreased by \$88,673
      - (101-53620-50608)
      - NOTE: This represents the new contract numbers from Johns Disposal that would require multi-family housing with over four-units to privately contract for their garbage handling. Letters went out to property owners/managers in July and September to inform them of the proposed change. Under the proposed contract the per unit price goes from \$5.40 to \$5.85 per unit. This contract was approved by the Board at our October 8<sup>th</sup> Board Meeting.
- Total Expenditures in the 2025 budget totals \$7,170,685. This represents an increase over 2024 by \$418,989.

### 2025 Proposed Levy

- General Fund Levy
  - The Village portion of the General Fund levy is proposed at \$3,854,330. This is an increase of \$283,964 from 2024 or an 8% increase.
  - The Town portion of the General Fund levy is proposed at \$302,047. This is a decrease of \$23,310 from 2024 or a .07% decrease.
  - Total General Fund Levy is proposed at \$4,156,377. This is an increase of \$260,654 from 2024 or a 6.7% increase.

- Debt Levy
  - The Village portion of the Debt Levy is proposed at \$959,274. This is an increase of \$26,453 from 2024 or a 2.8% increase.
  - The Town portion of the Debt Levy is proposed at \$69,678. This is a decrease of \$9,726 from 2024 or 12.2% decrease.
  - Total Debt Levy is proposed at \$1,028,951. This is an increase of \$16,726 from 2024 or 1.7%.
- The total proposed tax levy is \$5,185,328. This is an increase of \$277,381 from 2024. The increase equates to an overall increase of 5.7%

2024 Proposed Village Tax Rate- Preliminary

All properties in the Village and the Town were revalued this year. The average values for residential properties went up by 42%. The charts below show what effect a total levy of \$4,813,603 would have on an average homeowner with our new valuations and employing the strategy of lowering our Line E Levy Limit Worksheet adjustment by \$100,000. Also note these rates have been calculated before our final certified assessment numbers have been provided due to the Board of Review meeting in late October.

Village Tax Rate based on 13.36% increase in Levy				
2024		3.62355		
2023		5.3422		
<b>Assuming no Increase in Valuation</b>		<b>Assume Increase in Valuation of 45%</b>		
<b>Cost for Village Taxes for \$200,000 Home</b>		<b>Cost for Village Taxes for \$290,000 Home</b>		
2024	\$	724.71	2024	\$ 1,050.83
2023	\$	1,068.44	Increase Over No Increase in Valuation \$ 326.12	
Difference	\$	(343.73)	Increase over 2023 \$ (17.61)	
				-1.65%
<b>Cost for Village Taxes for \$300,000 Home</b>		<b>Cost for Village Taxes for \$435,000 Home</b>		
2024	\$	1,087.07	2024	\$ 1,576.25
2023	\$	1,602.66	Increase Over No Increase in Valuation \$ 489.18	
Difference	\$	(515.59)	Increase over 2023 \$ (26.41)	
				-1.65%
<b>Cost for Village Taxes for \$400,000 Home</b>		<b>Cost for Village Taxes for \$580,000 Home</b>		
2024	\$	1,449.42	2024	\$ 2,101.66
2023	\$	2,136.88	Increase Over No Increase in Valuation \$ 652.24	
Difference	\$	(687.46)	Increase over 2023 \$ (35.22)	
				-1.65%
<b>Cost for Village Taxes for \$500,000 Home</b>		<b>Cost for Village Taxes for \$725,000 Home</b>		
2024	\$	1,811.78	2024	\$ 2,627.08
2023	\$	2,671.10	Increase Over No Increase in Valuation \$ 815.30	
Difference	\$	(859.32)	Increase over 2023 \$ (44.02)	
				-1.65%
<b>Cost for Village Taxes for \$600,000 Home</b>		<b>Cost for Village Taxes for \$870,000 Home</b>		
2024	\$	2,174.13	2024	\$ 3,152.49
2023	\$	3,205.32	Increase Over No Increase in Valuation \$ 978.36	
Difference	\$	(1,031.19)	Increase over 2023 \$ (52.83)	
				-1.65%

2024 Proposed Town Tax Rate-Preliminary

<b>Town Tax Rate based on 13.36% increase in Levy</b>					
2024	2.98657				
2023	4.54792				
<b>Assuming no Increase in Valuation</b>			<b>Assume Increase in Valuation of 45%</b>		
<b>Cost for Town Taxes for \$200,000 Home</b>			<b>Cost for Village Taxes for \$290,000 Home</b>		
2024	\$ 597.31	2024	\$ 866.10		
2023	\$ 909.58		Increase Over No Increase in Valuation	\$ 268.79	
Difference	\$ (312.27)		Increase over 2023	\$ (43.48)	
					-4.78%
<b>Cost for Village Taxes for \$300,000 Home</b>			<b>Cost for Village Taxes for \$435,000 Home</b>		
2024	\$ 895.97	2024	\$ 1,299.16		
2023	\$ 1,364.38		Increase Over No Increase in Valuation	\$ 403.19	
Difference	\$ (468.41)		Increase over 2023	\$ (65.22)	
					-4.78%
<b>Cost for Town Taxes for \$400,000 Home</b>			<b>Cost for Village Taxes for \$580,000 Home</b>		
2024	\$ 1,194.63	2024	\$ 1,732.21		
2023	\$ 1,819.17		Increase Over No Increase in Valuation	\$ 537.58	
Difference	\$ (624.54)		Increase over 2023	\$ (86.96)	
					-4.78%
<b>Cost for Town Taxes for \$500,000 Home</b>			<b>Cost for Village Taxes for \$725,000 Home</b>		
2024	\$ 1,493.28	2024	\$ 2,165.26		
2023	\$ 2,273.96		Increase Over No Increase in Valuation	\$ 671.98	
Difference	\$ (780.68)		Increase over 2023	\$ (108.70)	
					-4.78%
<b>Cost for Village Taxes for \$600,000 Home</b>			<b>Cost for Village Taxes for \$870,000 Home</b>		
2024	\$ 1,791.94	2024	\$ 2,598.31		
2023	\$ 2,728.75		Increase Over No Increase in Valuation	\$ 806.37	
Difference	\$ (936.81)		Increase over 2023	\$ (130.44)	
					-4.78%

Other Key Points

- Net new construction in the Village caused a \$175,203,100 increase in equalized value or a 13.36% increase.
- The overall increase in equalized value for Tax Increment Value in 2024 is \$166,172,200.00
- The Village Tax Levy consists of General Fund (Fund 101) operating costs and debt Service payments (Fund 301).
- Staff has created a Budget Booklet containing detailed information used to determine both costs and revenues. A hard copy is available by request.
- The electronic version of our Budget Spreadsheet includes notes for each line that define how each number is generated. Staff hopes that this will serve to prevent the loss of “institutional knowledge” on what each budget line entails.

**CAPITAL IMPROVEMENT PLANS:**

Total proposed requests for General Fund CIP are \$820,134. Based on our October 15<sup>th</sup> Work Session, the CIP has removed the requested Public Works Bucket Truck and added \$3,500 for additional Somers banners. Superintendent Kreye will be at our meeting to further express his desire to put the Bucket Truck back in as CIP item. Project details are listed below by departments:

### **General Fund**

Total requested for Paving is \$457,134. Item requested is as follows:

- 37th Ave. (CTH A to 6th Pl.) Pets Sub. \$71,634
- 6th Place (37th Ave. to 39th Ave.) Pets Sub. \$31,000
- 39th Ave. (6th Pl. to Cul-De-Sac) Pets Sub. \$57,400
- 39th Ave. (6th Pl. to 5th Pl.) Pets Sub. \$85,400
- 5th Place (39th Ave. to Cul-De-Sac) Pets Sub. \$61,700
- 5th Place (39th Ave. to 4204, 6th St.) Pets Sub. \$120,000
- 6th Street (6th Street to Termination) Pets Sub. \$30,000

Total requested from Public Works is \$30,000. Item requested is as follows:

- Purchase Trench Box \$15,000
- GIS Program \$15,000

Total requested by Public Safety is \$254,500. Items requested are as follows:

- Replace Turnout Gear \$150,000
- Replace Fire Hoses \$15,000
- Replace CO Meters \$12,000
- Purchase Dispatch Software \$12,500
- Replace Gym Equipment \$15,000
- Station #2 Architecture \$50,000

Total requested by Administration is \$78,500. Items requested are as follows:

- Website Upgrade \$5,000
- Fire Department Lobby Rehabilitation \$25,000

- Replacement Computer Plan \$20,000
- Electronic Speed Signs \$15,000
- Village Buildings HVAC repairs \$10,000
- Somers Banners \$3,500

**Sewer Fund CIP:**

Total proposed requests for Sewer Fund CIP are \$250,000. Items requested are as follows:

- I&I Replacement/Lining Program \$250,000
  - This is listed each year and it is up to the Board if they would like to leave it in 2025 or not.

**Water Fund CIP:**

No projects are listed at this time.

**Stormwater Fund CIP:**

Total proposed requests for Stormwater Fund CIP are \$213,311. Strand Associates will be doing a presentation to the Storm Water Utility Board on October 22<sup>nd</sup> several options for possible projects will be presented. If the Board wishes to move forward with any of the suggestions, they will be added to the list of projects. Project details are listed below:

- Green Bay Road Mueller Parcel (carried over from 2024) \$40,000
- Gitzlaff Phase II (carried over from 2024) \$31,711
- Neumiller Phase II (carried over from 2024) \$30,000
- Davis Culvert (carried over from 2024) \$40,000
- 4<sup>th</sup> Street Culver Replacement (carried over from 2024) \$71,600

**KR Sewer District:**

- Administration does not foresee any capital items needed for the KR Sewer District in 2025.

**TID CIPs:**

- Baxter Woodman has been working on a possible water tower and the Village is awaiting information the PSC.

The General Fund budget as presented is balanced. It includes the necessary basic services for the citizens of Somers. The Board can still suggest and make changes to the proposed budget. The Public Hearing for the 2025 Budget is scheduled to take place at 5:30 p.m. on November 26<sup>th</sup>, 2024.

**ATTACHMENTS:**

Proposed 2025 Budget

2025 Consolidated CIP

General Fund CIP Request Forms (including Bucket Truck)

Authorized Positions

BUDGET REPORT FOR SOMERS VILLAGE AND TOWN

2025 Budget estimate Calculations

GL NUMBER	DESCRIPTION	2021	2022	2023	2024	2024	2024	2025
		ACTIVITY	ACTIVITY	ACTIVITY	AMENDED BUDGET	THRU 12/31/24 ACTIVITY	PROJECTED ACTIVITY	REQUESTED BUDGET
<b>ESTIMATED REVENUES</b>								
Dept 41000 - TAXES								
101-41000-41111	VILLAGE PROPERTY TAXES	3,120,878	3,239,424	3,315,695	3,570,366	3,570,366	3,570,366	3,854,330
(2025) \$77,000 OF THE VILLAGE'S PROPERTY TAXES TO GO INTO VOLUNTARY DONATIONS PER RAVINE PARK AGREEMENT								
101-41000-41112	TOWN PROPERTY TAXES	334,374	375,784	325,166	325,357	325,424	325,424	302,047
101-41000-41140	MOBILE HOME TAXES	13,379	13,718	79,626	47,000	43,142	12,059	12,200
101-41000-41150	PRIVATE AND MANAGED FOREST, WOODLAND TA	200	200	200	202	11	11	200
101-41000-41320	TAXES FROM OTHER TAX EXEMPT ENTITIES			9,065		5,774	5,774	5,000
101-41000-41800	INTEREST AND PENALTY ON TAXES							
101-41000-41910	OMITTED TAXES							
101-41000-41920	AG-USE PENALTY	362	7,659	72,625	1,000			1,000
101-41000-41930	BOUNDARY AGREEMENT							
Totals for dept 41000 - TAXES		3,469,193	3,636,785	3,802,377	3,943,925	3,944,717	3,913,634	4,174,777
Dept 42000 - SPECIAL ASSESSMENTS								
101-42000-42110	STREET IMPROVEMENT ASSESSMENTS							
101-42000-42180	VOLUNTARY DONATION	174,837	1,068,663	1,007,214	80,000	4,590	4,590	77,000
101-42000-42190	OTHER SPECIAL ASSESSMENTS		8					
Totals for dept 42000 - SPECIAL ASSESSMENTS		174,837	1,068,671	1,007,214	80,000	4,590	4,590	77,000
Dept 43000 - INTERGOVERNMENTAL								
101-43000-43212	FEMA GRANT	9,928	6,484	66,239				
101-43000-43219	SAFER GRANT							
101-43000-43300	OTHER FEDERAL PAYMENTS							
101-43000-43410	STATE SHARED REVENUE	187,605	188,766	189,442	451,762	67,764	67,764	453,828
101-43000-43411	PERSONAL PROPERTY AID	18,081	16,851	16,851	16,851	16,851	16,851	40,942
101-43000-43420	FIRE INSURANCE DUES	39,965	43,074	48,546	48,000	54,599	54,599	70,000
101-43000-43431	EXEMPT COMPUTER AID	3,797	3,797	3,797	3,797	3,797	3,797	3,797
101-43000-43432	EXPENDITURE RESTRAINT AID							
101-43000-43433	VIDEO SERVICE PROVIDER AID	24,485	24,485	24,485	24,485	24,485	24,485	24,485
101-43000-43529	AMBULANCE FUNDING		62,834	46,688	6,600	7,256	7,256	7,500
101-43000-43531	TRANSPORTATION AID	129,282	146,055	165,101	188,371	188,374	141,280	215,209
101-43000-43532	DISASTER DAMAGE AIDS							
101-43000-43534	LRIP FUNDING							
101-43000-43545	RECYCLING GRANT	29,872	29,795	29,810	29,810	29,854	29,854	29,855
101-43000-43690	OTHER STATE PAYMENTS							
101-43000-43691	PAYMENT FOR MUNICIPAL SERVICES	194,432	178,709	201,239	200,000	203,235	203,235	228,671
101-43000-43692	ENVIRONMENTAL IMPROVEMENT FUND	108,180	108,180	108,180	108,180	195,276	195,276	108,180
101-43000-43693	DNR GRANT - PW							
101-43000-43694	DNR GRANT - PS			1,006	1,500	3,213	3,213	1,400
101-43000-43791	OTHER LOCAL GOVERNMENT GRANTS - PS							
Totals for dept 43000 - INTERGOVERNMENTAL		745,627	809,030	901,384	1,079,356	794,704	747,610	1,183,867
Dept 44000 - LICENSES & PERMITS								
101-44000-44110	CLASS A LIQUOR LICENSES	5,670	6,600	5,410	6,000	5,400	5,400	5,400
101-44000-44111	CLASS B LIQUOR LICENSES	5,838	6,808	8,490	8,000	8,389	8,431	7,701
101-44000-44112	OPERATORS LICENSES	8,975	8,715	7,130	7,000	5,910	5,775	6,000
101-44000-44113	BUSINESS AND OCCUPATIONAL LICENSES	2,285	2,450	2,425	3,000	3,650	3,500	3,200
101-44000-44116	CIGARETTE LICENSES	1,108	1,500	1,400	1,300	1,000	1,000	1,200
101-44000-44121	CABLE FRANCHISE FEES	78,253	102,168	121,445	99,000	69,622	69,622	95,000
101-44000-44122	MOBILE HOME PARK LICENSES	720	700	700	700	200	200	700
101-44000-44130	DOG PARK LICENSES	198	170	(23)	300	637	630	300
101-44000-44131	DOG LICENSES	3,276	2,980	2,675	3,000	2,073	2,048	3,000
101-44000-44135	KENNEL LICENSES							
101-44000-44300	BUILDING PERMITS	119,607	903,597	997,762	241,140	220,360	204,587	250,000
101-44000-44900	OTHER LICENSES AND PERMITS	6,501	6,453	5,086	10,000	5,961	5,141	5,000
Totals for dept 44000 - LICENSES & PERMITS		232,431	1,042,141	1,152,500	379,440	323,202	306,334	377,501
Dept 45000 - FINES AND FORFEITURES								
101-45000-45100	COURT PENALTIES	109,543	109,864	124,590	110,000	161,515	145,622	175,000
101-45000-45102	ORDINANCE VIOLATION PENALTIES			37,500				

101-45000-45105	DOG PENALTIES	40	25	25	50	5	5	25
101-45000-45210	OTHER COURT PENALTIES							
Totals for dept 45000 - FINES AND FORFEITURES		109,583	109,889	162,115	110,050	161,520	145,627	175,025
Dept 46000 - PUBLIC CHARGES FOR SERVICES								
101-46000-46100	LICENSE PUBLICATION FEES	900	1,000	1,090	1,000	1,010	1,010	1,010
101-46000-46103	PREQUALIFICATION BID FEES	600	1,500	1,425	150	2,625	2,625	1,125
101-46000-46105	CHARGES FOR SERVICES	2,800	2,799	2,401	4,500			4,500
101-46000-46106	COURT SERVICE FEES	600			150			
101-46000-46220	FIRE AND RESCUE SERVICE FEES	577,567	629,385	820,213	850,000	367,684	336,739	950,000
101-46000-46221	FIRE INSPECTION FEES	9,407	38,156	2,741	40,000	23,295	22,840	32,500
101-46000-46222	BURN PERMITS	2,745	3,105	3,320	4,500	4,565	4,490	4,725
101-46000-46290	FIRE AND RESCUE ALLOWANCE	(231,027)	(251,754)	55,620	(280,000)	(134,573)	(124,110)	(325,000)
101-46000-46310	PUBLIC WORKS SERVICE FEES	4,777	2,188	2,687	4,500	7,324	7,324	3,500
101-46000-46431	LANDFILL PERMITS	150	150	50	175	130	130	150
101-46000-46440	WEED CUTTING FEES	5,440	5,978	4,982	5,000	5,077	5,077	5,000
101-46000-46445	POND MAINTENANCE FEES	6,390	5,005	5,910	6,390	6,330	6,330	6,330
101-46000-46590	OTHER HEALTH SERVICES	240						
101-46000-46742	AUDITORIUM RENTAL		375	375	550	(125)	(125)	375
101-46000-46743	FIELD RENTAL	280		150	2,500	200	200	2,700
101-46000-46750	RECREATION FEES - BASKETBALL			20				
101-46000-46751	RECREATION FEES - SOCCER							
101-46000-46752	RECREATION FEES - SOFTBALL							
101-46000-46753	RECREATION FEES - GOLF							
101-46000-46754	RECREATION FEES - FLAG FOOTBALL							
101-46000-46765	CONCESSION RENTAL							
101-46000-46851	DEVELOPER FILING FEES	7,220	26,605	3,600	4,000	2,400	2,400	4,000
101-46000-46852	DEVELOPER FEES	87,932	268,363	219,826	30,000	416,322	116,322	30,000
101-46000-46853	PLAN COMMISSION FEES							
Totals for dept 46000 - PUBLIC CHARGES FOR SERVICES		476,021	732,855	1,124,410	673,415	702,264	381,252	720,915
Dept 47000 - INTERGOVERNMENTAL CHARGES FOR SERVICES								
101-47000-47222	STATE HIGHWAY FIRES							
Totals for dept 47000 - INTERGOVERNMENTAL CHARGES FOR SERVICES								
Dept 48000 - MISCELLANEOUS REVENUES								
101-48000-48110	INTEREST INCOME	6,756	193,577	544,750	400,000	506,054	428,349	400,000
101-48000-48111	UNREALIZED GAIN/LOSS IN INVESTMENTS					27,029	21,058	
101-48000-48115	PENALTIES AND INTEREST INCOME	7,328	6,363	3,293	6,000	5,756	5,030	6,000
101-48000-48120	INTERFUND LOAN INTEREST INCOME							
101-48000-48130	SPECIAL ASSESSMENT INCOME							
101-48000-48200	RENTAL INCOME	62,165	62,710	60,291	59,510	2,160	2,160	27,600
101-48000-48303	SALE OF HIGHWAY PROPERTY							
101-48000-48306	SELL OF VILLAGE PROPERTY					496,634	496,634	10,000
101-48000-48307	SALE OF RECYCLABLES	18,251	12,471	23,835	2,000	14,335	14,335	2,000
101-48000-48500	DONATIONS - OTHER	3,000	3,000	3,000		3,300	3,300	3,000
101-48000-48510	DONATIONS - PARADE	7,850	10,475	10,553	10,000	9,100	9,100	10,000
101-48000-48520	DONATIONS - RECREATION							
101-48000-48900	MISCELLANEOUS REVENUE	3,828	4,433	76,358	3,000	20,355	19,839	3,000
Totals for dept 48000 - MISCELLANEOUS REVENUES		109,178	293,029	722,080	480,510	1,084,723	999,805	461,600
Dept 49000 - FINANCING SOURCES								
101-49000-49200	TRANSFER FROM OTHER FUNDS		50,750					
101-49000-49900	CONTINUING APPROPRIATIONS				5,000	(4,918)	(4,918)	
Totals for dept 49000 - FINANCING SOURCES			50,750		5,000	(4,918)	(4,918)	
TOTAL ESTIMATED REVENUES		5,316,870	7,743,150	8,872,080	6,751,696	7,010,802	6,493,934	7,170,685
APPROPRIATIONS								
Dept 51110 - VILLAGE BOARD								
101-51110-50101	SALARIES	48,181	47,996	47,996	48,000	37,843	33,920	48,000
101-51110-50201	FICA	3,686	3,650	3,671	3,672	2,895	2,595	3,672
101-51110-50202	RETIREMENT	237						
101-51110-50204	DENTAL INSURANCE							
101-51110-50301	OFFICE SUPPLIES			25		41	43	30
101-51110-50404	LEGAL							
101-51110-50701	CONFERENCES & TRAINING	80	110	228	500	90	95	500
101-51110-50702	TRAVEL	2,400	2,400	2,549	2,400	2,062	1,955	2,400

Totals for dept 51110 - VILLAGE BOARD		54,584	54,156	54,469	54,572	42,931	38,608	54,602
Dept 51120 - TOWN BOARD								
101-51120-50101	SALARIES	24,090	23,998	23,998	24,000	18,922	16,961	24,000
101-51120-50201	FICA	1,843	1,836	1,836	1,836	1,447	1,298	1,836
101-51120-50202	RETIREMENT	355						
101-51120-50206	POST RETIREMENT BENEFITS	418						
101-51120-50404	LEGAL							
101-51120-50405	COMPUTER AND WEBSITE							
101-51120-50604	TELEPHONE							
101-51120-50801	NEWSLETTER							
Totals for dept 51120 - TOWN BOARD		26,706	25,834	25,834	25,836	20,369	18,259	25,836
Dept 51130 - CIVIC COMMITTEE								
101-51130-50309	OTHER SUPPLIES AND EXPENSE	8,850	5,930	11,120	7,000	9,610	10,091	10,000
Totals for dept 51130 - CIVIC COMMITTEE		8,850	5,930	11,120	7,000	9,610	10,091	10,000
Dept 51210 - MUNICIPAL COURT								
101-51210-50101	SALARIES	55,460	57,833	59,161	60,936	47,722	42,773	69,082
101-51210-50102	WAGES							
101-51210-50201	FICA	4,243	4,233	4,509	4,662	3,651	3,272	5,285
101-51210-50202	RETIREMENT	2,465	3,365	3,129	3,272	2,579	2,311	3,625
101-51210-50203	HEALTH INSURANCE	21,269	8,593	9,176	10,085	8,404	7,942	10,992
101-51210-50204	DENTAL INSURANCE	1,477	1,745	819	556	463	438	556
101-51210-50205	LIFE INSURANCE	76	134	142	158	103	98	158
101-51210-50301	OFFICE SUPPLIES	620	1,616	616	600	381	400	630
101-51210-50303	POSTAGE							
101-51210-50309	OTHER SUPPLIES AND EXPENSE	4,870	5,330	6,031	5,700	5,627	5,908	5,985
101-51210-50404	LEGAL							
101-51210-50701	CONFERENCES & TRAINING	12,869	1,964	959	2,500	1,402	1,222	2,625
101-51210-50702	TRAVEL				250			
101-51210-50806	CODE ENFORCEMENT							
101-51210-50902	EQUIPMENT CAPITAL OUTLAY			160				
Totals for dept 51210 - MUNICIPAL COURT		103,349	84,813	84,702	88,719	70,332	64,364	98,938
Dept 51410 - VILLAGE/TOWN OFFICE								
101-51410-50101	SALARIES	184,247	220,171	159,825	164,588	129,616	116,027	172,453
101-51410-50102	WAGES	48,356	49,214	57,415	52,141	41,196	36,939	53,705
101-51410-50104	OVERTIME	95	212		1,500			1,500
101-51410-50201	FICA	17,811	19,866	16,294	16,695	12,947	11,595	17,416
101-51410-50202	RETIREMENT	15,092	18,126	14,120	14,954	11,786	10,555	15,718
101-51410-50203	HEALTH INSURANCE	70,787	47,876	54,301	70,925	41,040	38,794	55,763
101-51410-50204	DENTAL INSURANCE	4,134	3,329	4,082	4,522	2,550	2,410	3,731
101-51410-50205	LIFE INSURANCE	471	504	419	538	314	294	538
101-51410-50206	POST RETIREMENT BENEFITS	1,752	2,268	2,423	2,547	2,049	1,935	2,581
101-51410-50207	UNEMPLOYMENT							
101-51410-50301	OFFICE SUPPLIES	4,008	4,118	5,562	8,182	5,094	4,792	8,000
101-51410-50302	COPIER	2,608	2,530	2,240	2,331	2,181	2,290	2,332
101-51410-50303	POSTAGE	2,029	3,930	4,236	3,500	3,540	2,862	3,500
101-51410-50309	OTHER SUPPLIES AND EXPENSE	9,909	9,441	12,112	15,000	12,938	12,953	15,000
101-51410-50404	LEGAL	22,373	23,296	20,419	30,000	19,994	14,879	30,000
101-51410-50405	COMPUTER AND WEBSITE	40,721	44,600	34,293	42,615	20,250	21,066	52,779
101-51410-50503	EQUIPMENT LEASE AND RENTAL	1,645	1,560	1,560	1,600	12,294	12,726	19,100
101-51410-50701	CONFERENCES & TRAINING	80	110	2,580	2,500	2,484	1,852	21,051
101-51410-50702	TRAVEL	300	1,620	2,539	1,800	1,769	1,700	1,800
101-51410-50801	NEWSLETTER							
101-51410-50805	ADMINISTRATIVE CHARGE	(109,410)	(93,260)	(111,692)	(118,161)	(86,909)	(81,333)	(117,408)
101-51410-50902	EQUIPMENT CAPITAL OUTLAY	119		359				400
Totals for dept 51410 - VILLAGE/TOWN OFFICE		317,127	359,511	283,087	317,777	235,133	212,336	359,959
Dept 51420 - CLERK/TREASURER								
101-51420-50101	SALARIES	87,947	59,543	47,125	48,750	40,606	37,911	70,200
101-51420-50201	FICA	6,756	3,557	3,573	3,729	3,095	2,889	5,370
101-51420-50202	RETIREMENT	4,818	2,413	3,203	3,364	2,771	2,583	4,879
101-51420-50203	HEALTH INSURANCE	17,310	10,298	14,683	16,133	7,311	7,103	24,716
101-51420-50204	DENTAL INSURANCE	1,249	785	1,047	1,047	442	433	1,408
101-51420-50205	LIFE INSURANCE	192	37	30	45	100	89	305
101-51420-50206	POST RETIREMENT BENEFITS	185	246	291	266	73	77	291
101-51420-50301	OFFICE SUPPLIES		250	460				500
101-51420-50309	OTHER SUPPLIES AND EXPENSE	39	250			(9)	(9)	500

101-51420-50701	CONFERENCES & TRAINING	1,612	2,518	1,620	2,000	1,409	1,479	2,000
101-51420-50702	TRAVEL	1,334	1,640	1,800	1,800	450	473	2,000
101-51420-50902	EQUIPMENT CAPITAL OUTLAY							700
Totals for dept 51420 - CLERK/TREASURER		121,442	81,537	73,832	77,134	56,248	53,028	112,869
Dept 51421 - LICENSE PUBLICATION FEES								
101-51421-50305	PRINTING AND PUBLISHING	958	948	153	1,000	591	621	800
Totals for dept 51421 - LICENSE PUBLICATION FEES		958	948	153	1,000	591	621	800
Dept 51430 - ELECTIONS								
101-51430-50101	SALARIES	6,330	19,763	7,420	20,000	15,176	15,935	10,000
101-51430-50104	OVERTIME							
101-51430-50201	FICA	484	1,512	568	1,530	1,161	1,219	765
101-51430-50202	RETIREMENT	8						
101-51430-50203	HEALTH INSURANCE							
101-51430-50204	DENTAL INSURANCE							
101-51430-50303	POSTAGE	2,108	2,534	510	3,990	3,606	2,111	2,960
101-51430-50309	OTHER SUPPLIES AND EXPENSE	115	2,369	1,822	4,000	4,530	4,454	3,000
101-51430-50409	OTHER CONTRACTED SERVICES							
101-51430-50701	CONFERENCES & TRAINING		169					
101-51430-50902	EQUIPMENT CAPITAL OUTLAY	1,298	1,362	2,538	1,400	1,404	1,474	1,000
Totals for dept 51430 - ELECTIONS		10,343	27,709	12,858	30,920	25,877	25,193	17,725
Dept 51510 - ASSESSOR								
101-51510-50301	OFFICE SUPPLIES	941	696	977	1,000	4,175	3,794	1,000
101-51510-50407	ASSESSMENT	32,000	32,005	190,600	59,500	49,583	46,856	59,500
Totals for dept 51510 - ASSESSOR		32,941	32,701	191,577	60,500	53,758	50,650	60,500
Dept 51520 - BOARD OF REVIEW								
101-51520-50101	SALARIES				400			400
101-51520-50201	FICA				31			31
101-51520-50202	RETIREMENT							
101-51520-50309	OTHER SUPPLIES AND EXPENSE	45			100	77	81	100
101-51520-50404	LEGAL				500			500
101-51520-50701	CONFERENCES & TRAINING		45		50	56	59	60
Totals for dept 51520 - BOARD OF REVIEW		45	45		1,081	133	140	1,091
Dept 51610 - VILLAGE/TOWN HALL								
101-51610-50309	OTHER SUPPLIES AND EXPENSE	2,024	4,008	3,621	4,000	1,501	1,157	2,000
101-51610-50409	OTHER CONTRACTED SERVICES	5,292	5,405	5,244	4,800	4,000	3,675	5,200
101-51610-50501	BUILDINGS AND GROUNDS	15,224	18,456	25,035	28,500	15,193	13,718	25,000
101-51610-50602	ELECTRICITY	8,772	12,617	12,733	12,823	8,796	7,991	13,465
101-51610-50603	GAS	4,121	6,330	4,446	5,500	2,880	3,004	5,775
101-51610-50604	TELEPHONE	3,935	4,757	4,088	3,900	3,511	3,327	6,756
101-51610-50606	WATER AND SEWER	8,152	3,243	7,836	6,300	3,562	2,314	6,400
101-51610-50902	EQUIPMENT CAPITAL OUTLAY							
Totals for dept 51610 - VILLAGE/TOWN HALL		47,520	54,816	63,003	65,823	39,443	35,186	64,596
Dept 51910 - OTHER GENERAL GOVERNMENT								
101-51910-50304	MEMBERSHIPS	7,490	4,496	4,954	4,289	650	683	5,543
101-51910-50305	PRINTING AND PUBLISHING	811	978	2,053	1,500	482	506	1,000
101-51910-50401	AUDITING AND ACCOUNTING	19,872	23,213	25,163	24,051	18,745	19,682	24,000
101-51910-50402	CONSULTING AND FINANCIAL ADVISOR	468	945	4,721	5,000	42,712	39,760	10,000
101-51910-50403	ENGINEERING AND INSPECTION	48,079	233,698	190,015	11,000	41,722	39,810	11,000
101-51910-50404	LEGAL	64,477	115,691	87,843	67,625	73,321	67,977	75,000
101-51910-50409	OTHER CONTRACTED SERVICES		20,000		5,000	5,074	5,024	5,000
101-51910-50802	ILLEGAL AND UNCOLLECTIBLE TAXES	1,386			5,000			
101-51910-50803	COLLECTION EXPENSE	44,668	42,076	54,006	47,000	27,097	17,400	55,000
101-51910-50804	UNCOLLECTIBLE ACCOUNTS		2,615					
101-51910-50808	PRIOR YEAR EXPENSES		23,761			10,536	11,063	10,000
101-51910-50809	OTHER MISCELLANEOUS EXPENSES	2,604	2,784	5,637	58,800	4,580	4,809	148,768
101-51910-50811	LIABILITY INSURANCE	147,073	191,695	197,039	222,982	151,932	134,206	233,540
101-51910-50812	WORKER'S COMPENSATION INSURANCE							
101-51910-50813	PROPERTY INSURANCE							
101-51910-50913	PAYING AGENT FEES							
Totals for dept 51910 - OTHER GENERAL GOVERNMENT		336,928	661,952	571,431	452,247	376,851	340,920	578,851
Dept 51911 - UNION								
101-51911-50404	LEGAL	585	2,553		500			500
Totals for dept 51911 - UNION		585	2,553		500			500

Dept 51912 - INTERGOVERNMENTAL AGREEMENTS								
101-51912-50404	LEGAL							
Totals for dept 51912 - INTERGOVERNMENTAL AGREEMENTS								
Dept 51913 - PAYMENTS FOR MUNICIPAL SERVICES								
101-51913-50809	OTHER MISCELLANEOUS EXPENSES	174,989	160,838	176,389	180,000	182,912	192,058	205,804
Totals for dept 51913 - PAYMENTS FOR MUNICIPAL SERVICES		174,989	160,838	176,389	180,000	182,912	192,058	205,804
Dept 51999 - CONTINGENCY								
101-51999-50809	OTHER MISCELLANEOUS EXPENSES							
Totals for dept 51999 - CONTINGENCY								
Dept 52100 - LAW ENFORCEMENT								
101-52100-50406	LAW ENFORCEMENT	521,916	649,635	744,709	778,000	528,267	554,680	858,375
Totals for dept 52100 - LAW ENFORCEMENT		521,916	649,635	744,709	778,000	528,267	554,680	858,375
Dept 52210 - FIRE DEPARTMENT								
101-52210-50101	SALARIES	768,691	811,702	909,146	1,082,165	792,541	702,375	1,186,447
101-52210-50103	PART-TIME	110,362	121,769	204,636	200,000	137,021	125,314	243,500
101-52210-50104	OVERTIME	145,143	109,749	171,357	120,000	136,060	124,547	135,000
101-52210-50107	OFFICERS	16,395	15,613	17,755	18,500	13,643	12,168	18,000
101-52210-50108	PAID ON CALL	103,911	119,032	110,303	120,000	94,863	87,332	130,000
101-52210-50109	PAID ON PREMISES			1,338	110,000	86,510	76,722	115,000
101-52210-50201	FICA	87,798	86,701	107,825	126,276	96,024	85,969	139,838
101-52210-50202	RETIREMENT	120,279	121,768	158,960	194,093	147,372	131,611	233,186
101-52210-50203	HEALTH INSURANCE	232,728	231,169	248,603	345,079	261,017	244,760	418,944
101-52210-50204	DENTAL INSURANCE	15,281	15,518	16,138	20,443	15,264	14,309	23,110
101-52210-50205	LIFE INSURANCE	1,909	1,205	1,071	2,055	1,104	1,032	2,302
101-52210-50206	POST RETIREMENT BENEFITS	668	697	892	761	2,561	2,420	3,232
101-52210-50207	UNEMPLOYMENT	38						
101-52210-50208	LENGTH OF SERVICE							
101-52210-50302	COPIER	1,182	447	609	500	59	62	525
101-52210-50303	POSTAGE	47	73	114	150	126	132	200
101-52210-50306	MEDICAL SUPPLIES	36,390	35,724	43,278	45,000	36,159	34,707	47,000
101-52210-50307	GEAR AND CLOTHING	29,293	30,697	34,148	37,000	34,678	35,837	37,500
101-52210-50309	OTHER SUPPLIES AND EXPENSE	9,524	9,708	11,393	11,500	10,027	10,525	12,075
101-52210-50405	COMPUTER AND WEBSITE	17,872	17,381	16,445	22,082	10,090	9,614	26,595
101-52210-50502	EQUIPMENT MAINTENANCE	9,025	8,241	11,833	14,000	9,685	13,834	16,000
101-52210-50503	EQUIPMENT LEASE AND RENTAL	445	348	348	348	319	274	349
101-52210-50504	VEHICLE MAINTENANCE	34,397	28,191	36,588	42,500	37,937	35,348	44,500
101-52210-50601	FUEL - GASOLINE AND DIESEL	20,586	32,064	30,353	32,000	24,430	22,195	35,000
101-52210-50602	ELECTRICITY	17,363	17,164	17,978	19,000	13,205	12,124	19,950
101-52210-50603	GAS	7,789	9,051	6,342	10,000	3,770	3,906	10,000
101-52210-50604	TELEPHONE	4,699	10,025	6,684	12,000	4,637	3,508	12,000
101-52210-50606	WATER AND SEWER	9,644	8,648	9,159	10,750	8,642	5,687	11,035
101-52210-50609	PUBLIC FIRE PROTECTION							
101-52210-50701	CONFERENCES & TRAINING	22,189	21,480	22,967	28,000	19,062	19,611	29,000
101-52210-50703	FIRE PREVENTION	3,499	3,113	5,292	5,000	3,697	2,703	5,500
101-52210-50809	OTHER MISCELLANEOUS EXPENSES	15,226	16,659	18,171	20,000	11,833	9,887	20,000
101-52210-50812	WORKER'S COMPENSATION INSURANCE							
101-52210-50902	EQUIPMENT CAPITAL OUTLAY	32,059	69,637	75,973	35,750	24,025	24,428	36,000
Totals for dept 52210 - FIRE DEPARTMENT		1,874,432	1,953,574	2,295,699	2,684,952	2,036,361	1,852,941	3,011,788
Dept 52220 - FIRE COMMISSION								
101-52220-50101	SALARIES							
101-52220-50201	FICA							
101-52220-50701	CONFERENCES & TRAINING		140		500	160	168	300
Totals for dept 52220 - FIRE COMMISSION			140		500	160	168	300
Dept 52230 - PUBLIC FIRE PROTECTION								
101-52230-50609	PUBLIC FIRE PROTECTION	164,171	190,624	217,077	217,000	162,808	113,966	217,077
Totals for dept 52230 - PUBLIC FIRE PROTECTION		164,171	190,624	217,077	217,000	162,808	113,966	217,077
Dept 52300 - AMBULANCE								
101-52300-50804	UNCOLLECTIBLE ACCOUNTS							
Totals for dept 52300 - AMBULANCE								
Dept 52400 - BUILDING INSPECTIONS								
101-52400-50101	SALARIES	86,068	88,302	110,905	77,250	60,887	54,573	85,000

101-52400-50102	WAGES	5,867	6,470	7,418	6,863	5,421	4,860	7,069
101-52400-50201	FICA	7,126	7,048	9,058	6,435	5,068	4,542	7,044
101-52400-50202	RETIREMENT	6,208	6,115	6,384	5,804	4,575	4,101	6,399
101-52400-50203	HEALTH INSURANCE	26,412	25,579	10,478	27,874	1,813	1,721	2,526
101-52400-50204	DENTAL INSURANCE	1,707	1,736	699	1,800	107	102	137
101-52400-50205	LIFE INSURANCE	783	826	369	288	186	176	286
101-52400-50206	POST RETIREMENT BENEFITS				1,431			1,325
101-52400-50207	UNEMPLOYMENT							
101-52400-50309	OTHER SUPPLIES AND EXPENSE	370	570	2,298	1,500	1,872	1,913	2,211
101-52400-50403	ENGINEERING AND INSPECTION	1,188	1,179	3,711	1,500	4,253	3,684	4,500
101-52400-50405	COMPUTER AND WEBSITE	598	613	1,067	1,067	1,101	1,156	1,144
101-52400-50504	VEHICLE MAINTENANCE					58	61	200
101-52400-50601	FUEL - GASOLINE AND DIESEL							1,000
101-52400-50701	CONFERENCES & TRAINING			1,275	3,000	2,093	1,973	3,000
101-52400-50702	TRAVEL	1,225	793	2,570	2,000	1,113	947	2,100
101-52400-50806	CODE ENFORCEMENT			200	1,500	20		500
101-52400-50902	EQUIPMENT CAPITAL OUTLAY							
Totals for dept 52400 - BUILDING INSPECTIONS		137,552	139,231	156,432	138,312	88,567	79,809	124,441
Dept 53100 - PUBLIC WORKS								
101-53100-50101	SALARIES	33,820	33,030	32,849	34,030	26,820	24,038	39,548
101-53100-50102	WAGES	180,844	128,456	178,179	189,598	144,136	129,728	196,910
101-53100-50104	OVERTIME	1,998	1,420	3,607	3,500	2,890	2,139	3,500
101-53100-50105	SNOW REMOVAL	7,443	4,321	4,320	7,000	5,234	5,496	10,000
101-53100-50106	SEASONAL	27,114	22,558	21,293	40,000	24,639	22,308	40,000
101-53100-50201	FICA	19,228	14,424	18,296	20,971	15,421	13,882	22,182
101-53100-50202	RETIREMENT	14,034	11,555	14,839	15,817	12,394	11,149	16,955
101-53100-50203	HEALTH INSURANCE	69,896	48,928	66,399	69,691	54,464	51,232	93,449
101-53100-50204	DENTAL INSURANCE	4,665	3,603	4,839	4,977	4,180	3,928	5,639
101-53100-50205	LIFE INSURANCE	1,076	786	647	220	236	228	303
101-53100-50206	POST RETIREMENT BENEFITS	1,299	1,344	1,428	1,431	116	109	146
101-53100-50207	UNEMPLOYMENT		220	48	300			
101-53100-50307	GEAR AND CLOTHING				1,000	1,490	1,565	1,750
101-53100-50309	OTHER SUPPLIES AND EXPENSE	9,612	14,813	15,268	19,000	14,692	14,171	20,000
101-53100-50403	ENGINEERING AND INSPECTION							
101-53100-50405	COMPUTER AND WEBSITE							
101-53100-50501	BUILDINGS AND GROUNDS	36						
101-53100-50504	VEHICLE MAINTENANCE	11,547	10,629	12,530	14,000	14,000	14,700	14,000
101-53100-50505	ROAD MAINTENANCE	62,652	65,809	65,010	70,000	19,709	17,490	85,000
101-53100-50506	DITCHING AND DRAINAGE	219						
101-53100-50601	FUEL - GASOLINE AND DIESEL	19,515	16,226	13,176	15,000	17,445	17,980	15,000
101-53100-50602	ELECTRICITY	10,678	10,885	10,986	9,326	7,589	6,895	11,000
101-53100-50603	GAS	4,123	5,461	3,836	5,734	2,484	2,591	6,021
101-53100-50604	TELEPHONE	336	1,037	660	500	440	416	600
101-53100-50606	WATER AND SEWER	7,726	6,486	13,425	15,000	7,124	4,628	9,120
101-53100-50607	STREET LIGHTS	22,022	20,753	23,246	23,144	17,482	14,058	24,195
101-53100-50701	CONFERENCES & TRAINING	419	1,990	2,657	1,000			2,000
101-53100-50702	TRAVEL			146	250			
101-53100-50902	EQUIPMENT CAPITAL OUTLAY		2,000	1,950	3,000			3,500
Totals for dept 53100 - PUBLIC WORKS		510,302	426,734	509,634	564,489	392,985	358,731	620,818
Dept 53620 - SOLID WASTE								
101-53620-50102	WAGES	7,042	11,913	10,646	9,237	7,277	6,522	9,944
101-53620-50201	FICA	539	855	803	707	546	489	761
101-53620-50202	RETIREMENT	447	830	717	637	502	450	691
101-53620-50203	HEALTH INSURANCE	3,056	4,256	4,383	3,723	3,036	2,861	4,055
101-53620-50204	DENTAL INSURANCE	192	304	299	242	195	184	242
101-53620-50205	LIFE INSURANCE	43	47	30	8	9	8	11
101-53620-50301	OFFICE SUPPLIES				1,000			1,000
101-53620-50408	GARBAGE COLLECTION	458,452	476,998	522,056	590,179	396,645	367,758	413,187
101-53620-50504	VEHICLE MAINTENANCE							
101-53620-50601	FUEL - GASOLINE AND DIESEL							
101-53620-50608	RECYCLING AND TIPPING FEES	221,655	233,753	251,278	306,504	200,267	186,755	217,831
Totals for dept 53620 - SOLID WASTE		691,426	728,956	790,212	912,237	608,477	565,027	647,722
Dept 54100 - ANIMAL CONTROL								
101-54100-50409	OTHER CONTRACTED SERVICES	14,297	13,225	9,790	10,733	9,946	9,493	10,862
Totals for dept 54100 - ANIMAL CONTROL		14,297	13,225	9,790	10,733	9,946	9,493	10,862
Dept 55200 - PARKS								

101-55200-50101	SALARIES				500			500
101-55200-50201	FICA				38			38
101-55200-50309	OTHER SUPPLIES AND EXPENSE	2,639	2,900	5,072	3,000	2,212	2,155	3,000
101-55200-50409	OTHER CONTRACTED SERVICES	158		50	200			
101-55200-50501	BUILDINGS AND GROUNDS					755	793	
101-55200-50902	EQUIPMENT CAPITAL OUTLAY				4,000	3,160	3,318	4,000
Totals for dept 55200 - PARKS		2,797	2,900	5,122	7,738	6,127	6,266	7,538
Dept 55300 - RECREATION								
101-55300-50102	WAGES	5,757	5,990	5,072	5,778	4,521	4,047	5,952
101-55300-50104	OVERTIME							
101-55300-50106	SEASONAL		4,999		1,500			1,500
101-55300-50201	FICA	442	823	385	557	335	299	570
101-55300-50202	RETIREMENT	389	389	345	399	312	279	414
101-55300-50203	HEALTH INSURANCE	993	978	603	1,009			2,704
101-55300-50204	DENTAL INSURANCE	55	56	85	161	133	126	161
101-55300-50205	LIFE INSURANCE	4	5	3	5	3	3	5
101-55300-50309	OTHER SUPPLIES AND EXPENSE	1,000						
101-55300-50902	EQUIPMENT CAPITAL OUTLAY							
Totals for dept 55300 - RECREATION		8,640	13,240	6,493	9,409	5,304	4,754	11,306
Dept 56910 - PLAN COMMISSION								
101-56910-50101	SALARIES	29,862	31,520	36,197	37,772	29,808	26,723	39,600
101-56910-50102	WAGES	1,320	1,120	640	3,360	720	756	3,360
101-56910-50201	FICA	2,386	2,378	2,815	3,147	2,336	2,103	3,287
101-56910-50202	RETIREMENT	2,016	2,035	2,442	2,606	2,054	1,841	2,752
101-56910-50203	HEALTH INSURANCE	8,493	7,383	4,518	6,981	4,137	3,909	5,407
101-56910-50204	DENTAL INSURANCE	349	349	322	433	269	254	645
101-56910-50205	LIFE INSURANCE	32	32	39	57	40	37	75
101-56910-50206	POST RETIREMENT BENEFITS							
101-56910-50305	PRINTING AND PUBLISHING	531	416	289	400	254	190	400
101-56910-50309	OTHER SUPPLIES AND EXPENSE		40	30	30	24	25	30
101-56910-50403	ENGINEERING AND INSPECTION							
101-56910-50409	OTHER CONTRACTED SERVICES	34,925	10,315	8,481	10,000			12,400
Totals for dept 56910 - PLAN COMMISSION		79,914	55,588	55,773	64,786	39,642	35,838	67,956
Dept 56920 - BOARD OF APPEALS								
101-56920-50102	WAGES	440	320		400			400
101-56920-50201	FICA	34	24		31			31
101-56920-50202	RETIREMENT							
101-56920-50309	OTHER SUPPLIES AND EXPENSE							
Totals for dept 56920 - BOARD OF APPEALS		474	344		431			431
Dept 59100 - TRANSFER OUT								
101-59100-50000	TRANSFER TO OTHER FUNDS			1,800,754				
Totals for dept 59100 - TRANSFER OUT				1,800,754				
TOTAL APPROPRIATIONS		5,242,288	5,727,534	8,140,150	6,751,696	4,992,832	4,623,127	7,170,685
NET OF REVENUES/APPROPRIATIONS - FUND 101		74,582	2,015,616	731,930		2,017,970	1,870,807	
BEGINNING FUND BALANCE		3,026,402	3,100,987	5,105,858	5,837,663	5,837,663	5,837,663	7,708,470
FUND BALANCE ADJUSTMENTS			(10,747)	(128)				
ENDING FUND BALANCE		3,100,984	5,105,856	5,837,660	5,837,663	7,855,633	7,708,470	7,708,470

2025 Consolidated CIP Page 1

	2025 total proposed CIP budget	Use of funds										Use of carry over funds from previous borrowings
		2025 proposed General fund	2025 proposed Sewer fund CIP	2025 proposed Water fund CIP	2025 proposed Stormwater fund CIP	2025 proposed TID #1 CIP	Carried over from 2024	2023A borrowing	2025 cash	2026 cash	ARPA funds	
<b>General fund:</b>												
<b>Paving:</b>												
37th Ave. (CTH A to 6th Pl.) Pets Sub., Paser Rating = 2	\$71,634	71,634						71,634				
6th Place (37th Ave. to 39th Ave.) Pets Sub. PAsER Rating = 2	\$31,000	31,000						31,000		-		
39th Ave. (6th Pl. to Cul-De-Sac) Pets Sub. PAsER Rating = 2	\$57,400	57,400						57,400				
39th Ave. (6th Pl. to 5th Pl.) Pets Sub. PAsER Rating =2	\$85,400	85,400						85,400				
5th Place (39th Ave. to Cul-De-Sac) Pets Sub. PAsER Rating = 2	\$61,700	61,700						61,700				
5th Place (39th Ave. to 4204, 6th St.) Pets Sub. PAsER Rating = 2	\$120,000	120,000						120,000				
6th Street (6th Street to Termination) Pets Sub. PAsER Rating = 2	\$30,000	30,000						30,000		-		
	<b>\$457,134</b>	<b>457,134</b>						<b>457,134</b>				
<b>Public works:</b>												
Trench Box	\$15,000	\$15,000					15,000	\$15,000				
GIS	\$15,000	15,000					15,000	\$15,000				
	<b>\$30,000</b>	<b>\$30,000</b>					<b>30,000</b>	<b>\$30,000</b>				
<b>Parks:</b>												
	-	-										
	-	-										
	-	-										
<b>Public Safety:</b>												
Turnout Gear	150,000	150,000							150,000			
Fire Hose Replacement	15,000	15,000							15,000			
CO2 Meters	12,000	12,000							12,000			
Dispacrch Software	12,500	12,500							12,500			
Gym Equipment	15,000	15,000							15,000			
Station #2 Architectue	50,000	50,000							50,000			
	<b>254,500</b>	<b>254,500</b>							<b>254,500</b>			
<b>Administration:</b>												
Website upgrade	5,000	5,000							5,000			
Fire Department Lobby	25,000	25,000							25,000			
Replacement Computer Plan	20,000	20,000							20,000			
Electronic Speed Signs	15,000	15,000							15,000			
Village Buildings HVAC Repairs	10,000	10,000							10,000			
Banners	3,500	3,500							3,500			
	<b>78,500</b>	<b>78,500</b>							<b>78,500</b>			
<b>TOTAL GENERAL FUND</b>	<b>\$820,134</b>	<b>820,134</b>					<b>30,000</b>	<b>487,134</b>	<b>333,000</b>			
<b>Sewer fund:</b>												
I&I Replacement/Repaid Program	250,000		250,000						250,000			
<b>TOTAL SEWER FUND</b>	<b>250,000</b>		<b>250,000</b>						<b>250,000</b>			
<b>Water fund: updated 6/20/24</b>												
Water Meter AMR Program												
<b>TOTAL WATER FUND</b>	<b>-</b>								<b>-</b>			
<b>Storm water fund: updated 6/20/24</b>												
Greenbay Road Mueller parcel	40,000				40,000		40,000		40,000			
Gitzlaff Phase II	31,711				31,711		31,711		31,711			
Neumiller Woods Phase II	30,000				30,000		30,000		30,000			
Davis Culvert	40,000				40,000		40,000		40,000			
4th Street Culvert Replacement	71,600				71,600		81,600		71,600			
<b>TOTAL STORM WATER FUND</b>	<b>213,311</b>				<b>213,311</b>		<b>223,311</b>		<b>213,311</b>			
<b>TID#1:</b>												
<b>TOTAL TID #1 FUND</b>	<b>-</b>								<b>-</b>			
<b>TOTAL</b>	<b>\$1,283,445</b>	<b>820,134</b>	<b>250,000</b>		<b>213,311</b>		<b>253,311</b>	<b>487,134</b>	<b>796,311</b>			



# FY 2025 CIP Request Form

1. Department Administration 2. Date Requested 10/15/2024

3. Project Name Website Update 4. Requested by Kevin Poirier

5. Project Number 1 6. # of Requests Submitted 5 7. Priority of Request 1 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

The Somers website was redesigned five years ago. It is overall still working properly, but some functions have started to break down as it is running on older software, Dooley and Associates suggested to updates and upgrades:  
1. Update menus to make them more user friendly  
2. Change the settings of the site index to make the search bar more efficient  
3. Update plugin to make the website more user friendly on mobile devices  
4. Some minor design to refresh page flow  
5. Setup a graphic to maximize appeal when sharing on social media  
6. Evaluate software for efficiency and get rid of unnecessary ones

9. Included in Prior CIPs No 10. If Yes, Budget Year \_\_\_\_\_ 11. \$\$\$ Budgeted in Prior CIP \_\_\_\_\_

12. Age of Item to be Replaced 5 Years 13. Estimated Useful Life of Replacement 5 Years 14. End of Useful Life Year 2030

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)

If known, please give rationale for suggested funding source)

Cash

16. Amount Requested for FY2023 **\$5,000**



# FY 2025 CIP Request Form

1. Department Administration 2. Date Requested 10/15/2024

3. Project Name Fire Department Lobby 4. Requested by Jason Peters

5. Project Number 2 6. # of Requests Submitted 5 7. Priority of Request 2 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

In 2021, the lobby entrance was remodeled. It was repainted and carpeting was added. The main front counter was also replaced and tempered glass was added as a security feature. In 2024 the auditorium was remodeled. During this project the Fire Department lobby was carpeted and painted to match lobby and auditorium. Administration is requesting that Fire Department front counter be remolded to match the lobby entrance. As was the case with the old main front counter, the Fire Department front counter has no security features. We have key card system but someone can easily jump over the counter to get access to the rest of the Fire Department and Public Works area.

9. Included in Prior CIPs No 10. If Yes, Budget Year N/A 11. \$\$\$ Budgeted in Prior CIP N/A

12. Age of Item to be Replaced 20 years 13. Estimated Useful Life of Replacement 30 years 14. End of Useful Life Year 2055

(Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves

15. Suggested Source of Funding If known, please give rationale for suggested funding source)

Cash

16. Amount Requested for FY2025 **\$25,000**



# FY 2025 CIP Request Form

1. Department Administration 2. Date Requested 10/15/2024

3. Project Name Computer Replacement Program 4. Requested by Jason Peters

5. Project Number 3 6. # of Requests Submitted 5 7. Priority of Request 3 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

In 2021, the Village received COVID related funds from the Federal Government through the state. A large portion of these funds went to buy new laptop for staff so they could work remotely, if needed. The lifespan of a laptop is around 3 to 4 years. In 2024, several computers were replaced, including the Assistant to the Administrator, Building Inspector, and added two more Badger Books. The Village should be allocating dollar each year to replace computers has they age. This is an ongoing request from Administration and will be include in all future CIPs.

9. Included in Prior CIPs Yes 10. If Yes, Budget Year 2024 11. \$\$\$ Budgeted in Prior CIP \$20,000

12. Age of Item to be Replaced 3-5 13. Estimated Useful Life of Replacement 5 14. End of Useful Life Year 2027 - 2030

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)

If known, please give rationale for suggested funding source)

General Fund Reserves

16. Amount Requested for FY2025 **\$20,000**



# FY 2025 CIP Request Form

1. Department Administration 2. Date Requested 10/15/2024

3. Project Name Electronic Speed Limit Signs 4. Requested by Jason Peters

5. Project Number 4 6. # of Requests Submitted 5 7. Priority of Request 4 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

The flashing speed limit signs that were included in our 2023 & 2024 CIP budgets. 8 signs have been purchased with these fund. They are on 100th Ave and 47th Ave. The 2024 signs will be installed on Old Green Bay Road and 4th Stree. They have been well received by our residents. Administration would like to budget \$15,000 to purchase 4 additional signs.

9. Included in Prior CIPs No 10. If Yes, Budget Year 2024 11. \$\$\$ Budgeted in Prior CIP \$15,000

12. Age of Item to be Replaced N/A 13. Estimated Useful Life of Replacement 10 14. End of Useful Life Year 2035

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves) If known, please give rationale for suggested funding source)

General Fund Reserves

16. Amount Requested for FY2024 **\$15,000**





# FY 2025 CIP Request Form

1. Department Administration 2. Date Requested 10/22/2024

3. Project Name Electronic Speed Limit Signs 4. Requested by Jason Peters

5. Project Number 6 6. # of Requests Submitted 6 7. Priority of Request 6 of 6

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

As a part of a branding effort, the Village has had banners designed and placed along CTH E. These were originally purchased through a donation. Trustee Nelson has received a quote to purchase 14 new banners and the hardware to install them. The new banners would be placed at additional sites around the Village.

9. Included in Prior CIPs No 10. If Yes, Budget Year N/A 11. \$\$\$ Budgeted in Prior CIP \$3,500

12. Age of Item to be Replaced N/A 13. Estimated Useful Life of Replacement 5 14. End of Useful Life Year 2029

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)

If known, please give rationale for suggested funding source)

General Fund Reserves

16. Amount Requested for FY2024 **\$3,500**



# FY 2025 CIP Request Form

1. Department Fire Department 2. Date Requested 10/9/2024

3. Project Name Turnout Gear bulk Replacemennt 4. Requested by Chief Andersen

5. Project Number 1 6. # of Requests Submitted 5 7. Priority of Request 1 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

12 years ago we received a grant to replace a large amount of turnout gear at once. Turnout gear is only supposed to be used for 10 years we have a lot that is more then 10 years old and in rough condition while we do purchase a few sets each year since we have been unsuccessful on a new gear grant I am requesting fund to purchase a large order of turnout gear to replace all of the outdated and un safe turnout gear. these funds would allow us to replace all of the bad gear plus purchase some other lighterweight gear that can be worn on car accidents and other none fire calls so that they arnt wearing gear that has been in a fire which can off gas and causes cancer so we try to wear fire gear only when needed.

9. Included in Prior CIPs no 10. If Yes, Budget Year \_\_\_\_\_ 11. \$\$\$ Budgeted in Prior CIP \_\_\_\_\_

12. Age of Item to be Replaced \_\_\_\_\_ 13. Estimated Useful Life of Replacement \_\_\_\_\_ 14. End of Useful Life Year \_\_\_\_\_

(Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves

15. Suggested Source of Funding \_\_\_\_\_ If known, please give rationale for suggested funding source)

\_\_\_\_\_

16. Amount Requested for FY2025 **\$150,000**



# FY 2025 CIP Request Form

1. Department Fire Department 2. Date Requested 10/9/2024

3. Project Name Fire Hose Replacement 4. Requested by Chief Andersen

5. Project Number 1 6. # of Requests Submitted 5 7. Priority of Request 2 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

This year we had a lot of old fire hose not pass safety inspection and we need to replace it. Hose is supposed to be replaced every 10 years but we keep it as long as it will pass inspection. We have some hose from the 1970s with brass couplings still. this money would let us replace a large amount of hose that needs to be replaced.

9. Included in Prior CIPs no 10. If Yes, Budget Year \_\_\_\_\_ 11. \$\$\$ Budgeted in Prior CIP \_\_\_\_\_

12. Age of Item to be Replaced \_\_\_\_\_ 13. Estimated Useful Life of Replacement \_\_\_\_\_ 14. End of Useful Life Year \_\_\_\_\_

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves) If known, please give rationale for suggested funding source)

[Empty box for suggested source of funding rationale]

16. Amount Requested for FY2025 **\$15,000**



# FY 2025 CIP Request Form

1. Department Fire Department 2. Date Requested 10/9/2024

3. Project Name 2 RAD 57 Co meters for ambulances 4. Requested by Chief Andersen

5. Project Number 1 6. # of Requests Submitted 5 7. Priority of Request 3 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

I am requesting \$12,000 to replace 2 meters we use on the ambulances to check a persons oxygen and Carbon Monoxide levels. The units we have now are starting to fail and are held together with tape. The 2 units in need of replacement are more then 10 years old which is longer then most departments get out of them.

9. Included in Prior CIPs no 10. If Yes, Budget Year \_\_\_\_\_ 11. \$\$\$ Budgeted in Prior CIP \_\_\_\_\_

12. Age of Item to be Replaced \_\_\_\_\_ 13. Estimated Useful Life of Replacement \_\_\_\_\_ 14. End of Useful Life Year \_\_\_\_\_

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)

If known, please give rationale for suggested funding source)

[Empty box for suggested funding source rationale]

16. Amount Requested for FY2025 **\$12,000**



# FY 2025 CIP Request Form

1. Department Fire Department 2. Date Requested 10/9/2024

3. Project Name Vehicle to Dispatch software and equipment 4. Requested by Chief Andersen

5. Project Number 1 6. # of Requests Submitted 5 7. Priority of Request 4 of 5

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)

We are requesting \$12,500 so we can add software to the tablets we have in our vehicles so we can communicate with dispatch. This software will not replace radios but will allow for more and improved communication with dispatch without having to talk on the radio. also when multiple calls are going we can call enroute and on scene by the push of a button. we would also need a couple of additional Ipads for vehicles that dont have them yet

9. Included in Prior CIPs no 10. If Yes, Budget Year \_\_\_\_\_ 11. \$\$\$ Budgeted in Prior CIP \_\_\_\_\_

12. Age of Item to be Replaced \_\_\_\_\_ 13. Estimated Useful Life of Replacement \_\_\_\_\_ 14. End of Useful Life Year \_\_\_\_\_

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves) If known, please give rationale for suggested funding source)

16. Amount Requested for FY2025 **\$12,500**







# FY 2025 CIP Request Form

1. Department Public Works 2. Date Requested 10/9/2024

3. Project Name Pets Subdivision Paving Project 4. Requested by Andy Kreye

5. Project Number 1 6. # of Requests Submitted 1 7. Priority of Request 1 of 1

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)  
For the 2025 paving project public works is recommending Pets Subdivision. The paser rating is a 2 (very Poor).

9. Included in Prior CIPs No 10. If Yes, Budget Year N/A 11. \$\$\$ Budgeted in Prior CIP N/A

12. Age of Item to be Replaced \_\_\_\_\_ 13. Estimated Useful Life of Replacement 20 Yrs 14. End of Useful Life Year 2045

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)  
If known, please give rationale for suggested funding source)

2023A Borrowing

16. Amount Requested for FY2025 **\$457,334**



# FY 2025 CIP Request Form

1. Department Public Works 2. Date Requested 10/3/2024

3. Project Name Trench Box and Shoring Pannels 4. Requested by Andy Kreye

5. Project Number 1 6. # of Requests Submitted \_\_\_\_\_ 7. Priority of Request \_\_\_\_\_ of \_\_\_\_\_

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)  
The Public Works Department is requesting to purchase shoring panels and a second trench box for excavation projects. We currently own an aluminum 6ft x 6ft trench box and are requesting to purchase an aluminum 8ft x8ft trench box. In the last year, we have experienced an increase in water service line leaks and subsequent repairs that have required two trench boxes on site. Certain sections of our water/sewer distribution and collection infrastructure are also deeper than what our current box can safely be used in. An 8ft box will allow us to preform repairs in a safe manner, along with the addition of shoring panels, which will help us prevent any cave-ins and keep the integrity of the hole uncompromised.

9. Included in Prior CIPs NO 10. If Yes, Budget Year \_\_\_\_\_ 11. \$\$\$ Budgeted in Prior CIP \_\_\_\_\_

12. Age of Item to be Replaced N/A 13. Estimated Useful Life of Replacement N/A 14. End of Useful Life Year \_\_\_\_\_ N/A

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)  
If known, please give rationale for suggested funding source)

Cash, can be apportioned to utility funds as well.

16. Amount Requested for FY2025 **\$15,000**



# FY 2025 CIP Request Form

1. Department Public Works 2. Date Requested 10/4/2024

3. Project Name GIS 4. Requested by Andy Kreye

5. Project Number 3 6. # of Requests Submitted 3 7. Priority of Request 3 of 3

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)  
The Public Works Department is requesting to start-up a GIS program for Village utilities with assistance from Baxter and Woodman. GIS is essentially a digital mapping software that can be used on a phone, tablet, and/or desktop computer to help identify, locate, and upkeep assets in a utility system. It includes visual components for: system design changes, operations and maintenance, and condition assessment in one central software. GIS also improves efficiencies in locating, tracking, and managing infrastructures hence why it is widely used by water/wastewater utilities. We would also explore purchasing a GPS unit for the Department so that Public Works would be able to add/edit data as we see fit and continue to grow our GIS system.

9. Included in Prior CIPs No 10. If Yes, Budget Year N/A 11. \$\$\$ Budgeted in Prior CIP N/A

12. Age of Item to be Replaced N/A 13. Estimated Useful Life of Replacement N/A 14. End of Useful Life Year N/A

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)  
If known, please give rationale for suggested funding source)

Cash, can be apportioned to utility funds as well.

16. Amount Requested for FY2025 **\$15,000**



# FY 2025 CIP Request Form

1. Department Public Works 2. Date Requested 10/4/2024

3. Project Name Bucket Truck 4. Requested by Andy Kreye

5. Project Number 2 6. # of Requests Submitted 3 7. Priority of Request 2 of 3

8. Item /Description (Rationale for Project, Ex. Why now? Why this budget?)  
The Public Works Department is requesting the purchase of a Bucket Truck. This will be a very beneficial piece of equipment to add to our fleet and will help us to provide safe and timely associated services, both planned and in an emergency situation, to the public. This truck would be used for a variety of tasks such as but not limited to the installation and maintenance of street signs and banners, outdoor lighting and building maintenance, and arborist work such as tree trimming and removal. The use of a bucket truck will also help us to improve employee safety and efficiency in comparison to past practices when performing similar duties. Ultimately, the addition of this truck will allow us to be more proactive and productive when maintaining various facets of Village infrastructure, as well as in response to any storms and or emergency situations that may arise.

9. Included in Prior CIPs No 10. If Yes, Budget Year N/A 11. \$\$\$ Budgeted in Prior CIP N/A

12. Age of Item to be Replaced N/A 13. Estimated Useful Life of Replacement 20 yrs 14. End of Useful Life Year 2045

15. Suggested Source of Funding (Cash, General Obligation Bond, Revenue Bond, Sewer Fund, Water Fund, Storm Water Fund, Reserves)  
If known, please give rationale for suggested funding source)

\$55,000 from 2023A Borrow that was not used to buy 2024 CIP Outdoor Boom Lift  
\$195,000 Cash

16. Amount Requested for FY2025 **\$250,000**



## POSITIONS AUTHORIZED IN THE 2025 BUDGET

### FULL TIME

Village / Town Administrator  
Village / Town Clerk / Treasurer  
Village / Town Deputy Clerk / Treasurer-New  
Village / Town Assistant Administrator -Reclass  
Accounting Manager / Deputy Clerk / Treasurer  
Accountant I  
Clerk of Courts  
Clerk I  
Utility Clerk  
Accounts Payable Clerk  
Building Inspector  
Public Works Superintendent  
Public Works Utilities Manager-New  
Public Works Foreman  
Public Works I Employee (4 employees)  
Public Works II Employee (1 employees)  
Fire Chief  
Fire/EMS Deputy Chief  
Firefighter / EMT-I12 or Firefighter / Paramedic (13 employees)

### PART TIME

Public Works Seasonal Employees  
Paid on Call / Part Time Firefighters  
Chief Pollworkers  
Pollworkers

### ELECTED OFFICIALS

Village President  
Village Trustees (6 elected at large)  
Town Chairman  
Town Supervisors (2 elected at large)  
Municipal Judge

### TOTALS

Full Time Employees: 34  
Elected Officials: 11  
Part Time Employees: Varies

**Village of Somers  
7511 12th Street  
Somers, WI 53171**

**Village Board Meeting  
Tentative Agenda  
Tuesday, November 12th, 2024  
5:30 p.m.**

<b>Village Board Meeting:</b>	
<b>Item #</b>	
1	Call to order
2	Pledge of Allegiance
3	Consent and Approval of Minutes of Regular meetings on October 22 <sup>nd</sup> , 2024, Vouchers dated October 24 <sup>th</sup> , 2024 and October 31 <sup>st</sup> , 2024, [MONTH] Building Report, September 2024 Investment Report
4	Correspondence:
5	Citizens Comments
6	President and Trustee Comments
7	<p>Action on Plan Commission Recommendation (Spaay &amp; Frederick)</p> <ul style="list-style-type: none"> <li>a. Request by John &amp; Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a rezoning from R-2 Suburban Single-Family Residential Dist. &amp; A-2 General Agricultural Dist. to R-2 Suburban Single-Family Residential Dist. on Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E, two parcel east of 52nd Avenue.)</li> <li>b. Request by John &amp; Ruthann Spaay, 6828 Grand Pkwy, Wauwatosa, WI 53213 (Owner), Steven Frederick, 5025 12th St., Kenosha, WI 53144 (Agent), for approval of a Certified Survey Map (dated August 26, 2024 and prepared by Mark A. Bolender of Ambit Land Surveying) to create two (2) 4.82-acre Lots from Tax Parcel #82-4-222-142-0070, located in the NW ¼ of Section 14, T2N, R22E, Village of Somers. (For information only, the property is located on the south side of Highway E,</li> </ul>

	two parcel east of 52nd Avenue.)
8	Action on Operator’s Licenses: Sandra Parise, Denise Maegaard, Dormilee Craig, and Alexis Tennyson
9	Adjourn

I hereby certify that as the designee of the chief elected official of the Village of Somers, I posted this notice of the November 12<sup>th</sup>, 2024 Village Board Meeting & Tentative Agenda in 1 public place & on the Village website.

Dated this 1st day of November 2024

Wendy Burnette, Clerk-Treasurer

Requests from person with disabilities who need assistance to participate in this meeting should be made to the Clerk’s Office at 262-859-2822 with as much notice as possible. **Notice is hereby given that members of the Village Board may participate telephonically. Notice is hereby given that members of the Town Board may be in attendance for the sole purpose of gathering information. A quorum may be present. However, no Board action will be taken.**